

COUNCIL ASSESSMENT PANEL MEETING

9 June 2021

AGENDA – 8.3

Applicant: Joyson Orchards Pty Ltd	Landowner: W J Joyce Nominees Pty Ltd
Agent: Beyond Ink	Originating Officer: Melanie Scott
Development Application:	20/1273/473
Application Description: Staged Consent Construction of light industry (agricultural industry) building & associated two storey offices, carparking, retaining walls (maximum height 3.5m), acoustic fence (maximum height 2.8m), signage, associated access work, landscaping & associated earthworks. Stage 1 earthworks, Stage 2 balance of the works	
Subject Land: Lot:3 Sec: P5047 DP:22713 CT:5440/920 Subject Land: Lot:2 Sec: P5047 DP:22713 CT:5440/810 Subject Land: Lot:21 Sec: P5047 DP:50774 CT:5643/977	General Location: 202 – 204 Onkaparinga Valley Road Woodside General Location: Naughtons Road Woodside Attachment – Locality Plan
Development Plan Consolidated : 8 August 2019 Maps AdHi/18 & 57	Zone/Policy Area: Light Industry Zone - Light Industry (Woodside) Policy Area 72
Form of Development: Merit	Site Area: 3888.60 m ²
Public Notice Category: Category 2 Merit - DPC requested & Private Cert BRC	Representations Received: Nil Representations to be Heard: Nil

1. EXECUTIVE SUMMARY

The proposal seeks to obtain Development Plan Consent for the establishment of light industry comprising a fruit processing building, containing apparatus for receiving, grading, washing, packing, storage and ultimately transportation of produce; associated office and worker facilities, and internal cool rooms, with office staff & worker car parking, retaining walls (on the boundaries) and internal drainage and wastewater infrastructure connected to Council's Common Waste Management Scheme (CWMS).

The subject land is located within the Light Industry Zone - Light Industry (Woodside) Policy Area 72 and the proposal is a Category 2, merit form of development. There were no representations received as a result of Public Notification processes for this proposal.

As per the CAP delegations, the CAP is the relevant authority for commercial and industrial developments where the value of the work is over \$2.5 million. The proposed value of this project is \$4 million.

The main assessment matters relating to the proposal include the scale of the development and interface with nearby and adjoining residential land uses (three such uses exist within the Light Industry Zone), heavy vehicle movements onto Onkaparinga Valley Road and from Naughtons Road, and interface with the Amy Gillet bikeway and stormwater drainage from the site and buildings.

In consideration of all the information presented, and following an assessment against the relevant zone and Council Wide provisions within the Development Plan, staff are recommending that the proposal be **GRANTED** Development Plan Consent, subject to conditions

2. DESCRIPTION OF THE PROPOSAL

The proposal is for the following:

- Multi-facet processing building containing apparatus for receiving, grading, washing, packing, storage and ultimately transportation of produce
- Office and office staff/worker facilities
- Sealed car parking and heavy vehicle access, manoeuvring and loading bays
- Retaining walls upon boundaries, earthworks and acoustic fence
- Landscaping
- Internal drainage and wastewater infrastructure.

The proposed development generally results in a building of a maximum of two storeys and 11.0 metres maximum height above natural ground level, with a dimension of 99.50 metres in length x 61.80 metres in width (overall dimensions), forming a reversed 'L' shape when viewed from Onkaparinga Valley Road.

The proposed building is setback 31 metres (at its closest point) from the Onkaparinga Valley Road primary road frontage and 20 metres from the Naughtons Road secondary road frontage with setbacks of 3.0 metres to the southern side property boundary and 2.0 metres to the eastern side (rear) property boundary.

The proposed building is setback 12 metres to the southern side and 20 metres from the eastern side property boundaries of the adjoining property at 10 Naughtons Road, at the intersection of Onkaparinga Valley and Naughtons Roads on the north-western side of the proposed development (refer to the *Site Plan* for details).

The proposed building is to be finished in a composite of external materials and finishes. These include maxline (vertical expressed ridge, metal sheeting) in 'kalkajaka' (deep slate grey tone) for the forward prominent section comprising the office and worker facilities and load out dock areas, 'tilt-up' precast concrete wall sections with an 'off form' finish (natural concrete grey) generally for the peripheral walls, and Colorbond © 'surfmist' (off-white) metal roof sheeting.

The customer interface/office at the front of the building facing Onkaparinga Valley Road also features substantial glazing and an anodized 'bronze', vertical blade awning.

The proposal incorporates retaining walls, to the southern, eastern and a short section of the northern boundaries to a maximum height of 2.9 metres and the incorporation of a chain link mesh compound fencing generally, with post and rail sheet metal fencing and landscaping to the adjoining residential allotment boundaries.

The balance of the site's open areas are to be sealed surface car parking and manoeuvring areas, including truck un-loading area and two dedicated truck loading docks for exporting produce. The site will operate a one way vehicle access for trucks with the entry being from Naughtons Road and the exit being to Onkaparinga Valley Road. To achieve Australian Standards for heavy vehicle access to Onkaparinga Valley Road, modifications to the existing slip lane are proposed at the applicant's cost as part of this application.

The site also contains a river red gum tree (*Eucalyptus camaldulensis*) within the site and three outside the site within the Council administered Road reserve. Two of the trees within the road reserve are classed as regulated trees. The two smaller ‘non-regulated’ trees are recommended for removal and the two larger regulated trees recommended to be retained and protected.

Consolidation of the three titles is proposed as part of the proposal to assist in meeting the requirements of the Building Code of Australia. This will also include the extinguishment of an easement to Council for drainage purposes which has no infrastructure within it, and with the construction of this proposal will have no purpose.

The proposed plans are included as **Attachment – Proposal Plans** with other information included as **Attachment – Application Information** and **Attachment – Applicant’s Professional Reports**.

3. BACKGROUND AND HISTORY

APPROVAL DATE	APPLICATION NUMBER	DESCRIPTION OF PROPOSAL
4 August 1998	98/D42/473	Land division

4. REFERRAL RESPONSES

The application was subject to a mandatory referral to the Department of Infrastructure & Transport (DIT) Transport Planning Services pursuant to Section 37 of the Act and Schedule 8 of the *Development Regulations 2008*, as the proposed development abuts a secondary arterial road (DIT State Controlled) confirmed in Map AdHi/1, and proposes a change to the nature and intensity of traffic movements relative to the land and the development.

- **DIT Transport Planning**

DIT have raised no objections to the proposal, acknowledging the proposed access/egress and turning proposals are to be undertaken by the applicant, referenced in Beyond Ink site plan and design drawings within the MFY Traffic Consultant Report. All costs of this work to be borne by the applicant and subject to conditions of consent.

The above response is included as **Attachment – Referral Responses**.

- **AHC Engineering**

The proposal has been referred to Council’s Engineering Department in respect of the proposed stormwater management and Council’s easement within the subject land (which contains no infrastructure and is to be extinguished), and existing stormwater infrastructure which exists immediately to the south of the proposal within adjoining Allotment 7 DP22714.

The property is connected to CWMS infrastructure within the Onkaparinga Valley Road Reserve nearby the south-western corner of the allotment.

5. CONSULTATION

The application was categorised as a Category 2 form of development in accordance with Section 38(2)(a) of the *Development Act (1993)* and Schedule 9 Part 2 (19) of the *Development Regulations (2008)*, with no representations received.

6. PLANNING & TECHNICAL CONSIDERATIONS

This application has been evaluated in accordance with the following matters:

i. The Site's Physical Characteristics

The collective area of the subject land (allotments 2 & 3 DP22713 and allotment 21 DP50774, is 7591m² representing approximately two to three times the typical land area of other industrial sites within the Woodside Light industry Zone.

The land is considered to be 'relatively flat', with a rise in elevation of approximately 3 metres at its rear (south-eastern) boundary, equating to approximately 1:50 grade.

The land is generally clear of vegetation with the exception of four river red gum trees situated at the Onkaparinga Valley Road Frontage with one in the site and the other three gum trees outside the site boundaries, within the Council road reserve. Two of the four are regulated trees and the arborist's report suggests that these will not be impacted on by the proposal. The two smaller river red gums are not protected by Development legislation and the Native Vegetation Branch of the Department for Environment, Heritage and Sustainability has confirmed that they can be removed in accordance with their guidelines for Management of Roadside Native Vegetation and Regrowth (Refer to **Attachment DEW Comment**)

The land contains a swale drain and there is an easement to Council in this location for drainage purposes alongside the shared boundary of Allotment 2 and Allotment 21. There is also an excavated vehicular entrance compensating for a change in the levels at the northern boundary of Allotment 21 and Naughtons Road. Council engineering have confirmed that there is no infrastructure in the easement and they are happy to have the easement extinguished. There is a condition regarding a new crossover from Naughtons Road which will be ensure it is suitable for trucks.

ii. The Surrounding Area

The surrounding locality is largely industrial and is at the northern extremity of the industrial zone on the northern side of the Woodside township. The land further north across Naughtons Road and on the western side of Onkaparinga Valley Road is within the Watershed (Primary Production) Zone.

The zone abuts the Township zone to the south and this creates a clear delineation of zones, where land uses transition directly from industrial/commercial use to residential land uses.

The subject land has a direct interface with two residential sites, one of which lies within the Light Industry Zone at the corner of Onkaparinga Valley Road and Naughtons Road. The other is adjacent to the north, within the Watershed (Primary Production) Zone on the opposite side of Naughtons Road.

The frontage of the site to Onkaparinga Valley Road reserve is flared to a broad width at the intersection of Onkaparinga Valley Road and Naughtons Road, and contains part of the Amy Gillett bikeway and a Lions Club picnic setting. The road itself is a DIT Controlled secondary arterial road.

iii. Development Plan Policy considerations

a) *Policy Area/Zone Provisions*

The subject land lies within the Light Industry Zone - Light Industry (Woodside) Policy Area (72) and these provisions seek:

- to become intensively developed with a wide range of low-impact industrial activities,
- to reflect a vigorous local economy and developments with a form that enhances the overall views and rural character,
- to present a high quality, aesthetically pleasing landscaped industrial area with generous set-backs and substantial and effective vegetated buffers to reduce the visual and environmental impact of the development,
- The zone will be protected from the intrusion of residential and other inappropriate uses to avoid land use conflicts. High impact industrial uses will not be located within the zone,
- To develop appropriate scale of buildings exhibiting well-articulated form, using smaller building modules, variation in the facades, roof form and colour and enhance the nature of the area, and
- Will recognise the importance of the Mount Lofty Ranges Watershed Area with an emphasis on prevention of pollution.

The following are considered to be the relevant Policy Area provisions:

<i>Objectives:</i>	<i>1, 2, 3</i>
<i>Desired Character Statement:</i>	<i>Paragraphs 1, 2 & 3</i>
<i>PDCs:</i>	<i>1, 2, 3, 4</i>

The following are considered to be the relevant Zone provisions:

<i>Objectives:</i>	<i>1, 2</i>
<i>Desired Character Statement:</i>	<i>Paragraphs 1, 2, 3 & 4</i>
<i>PDCs:</i>	<i>1, 3, 4</i>

This proposal represents a high standard of light industrial development, which is substantial in scale, several times the scale of surrounding industrial developments. The proposal is relevantly linked to rural industry within the district and on the basis of its linkages to primary production and its scale, is considered to reflect vigour of the local economy as described within the Desired Character Statement.

The building utilises a composite of building elements and materials which far surpass the 'typical appearance' of industrial buildings, presenting a contemporary design and styling which will enhance the visual appearance of the northern entry into

Woodside. The scale of the development, exceeding the scale of existing buildings in the locality is not considered prejudicial to the zone.

The design layout, access through the site, car parking and heavy vehicle manoeuvring and docking (loading bays) creates large setbacks, particularly to Onkaparinga Valley Road, which is in excess of 31 metres from the front boundary at its nearest point, which surpasses the requirements of Zone PDC 4. The setback from Naughtons Road is slightly reduced at approximately 7 metres.

Existing trees retained at the Onkaparinga Valley Road frontage and new landscaping at the Naughtons Road frontage will also enhance the appearance of the land, buildings and activities occurring within the site.

Accordance with Zone

Development for the purpose of processing, packing, storage, transport and associated offices and workers facilities is considered to be appropriate within the Light Industry Zone.

The scale of the building is substantial, however, its built form, articulation and setbacks accord or exceed the minimum requirements of the zone in many respects. The building is not considered likely to overwhelm the zone or its visual characteristics.

The proposed development and the activities associated are not prejudicial to any other development on land within adjacent zones. In particular, it does not threaten the attainment of the Development Plan Objectives in respect of protecting the Watershed (Primary Production) Zone from undesirable impact upon water resources or pollution.

Form of Development

The zone and policy area PDC 1 both indicate envisaged forms of development including *light industry* as appropriate.

Schedule 1 of the Regulations provides detail of the nature, scale and intensity activities within the definition of 'light industry'. The Schedule 1 (interpretation) provides that:

light industry - means an industry where the process carried on, the materials and machinery used, the transport of materials, goods or commodities to and from the land on or in which (wholly or in part) the industry is conducted and the scale of the industry does not—

(a) detrimentally affect the amenity of the locality or the amenity within the vicinity of the locality by reason of the establishment or the bulk of any building or structure, the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit, oil, spilled light, or otherwise howsoever; or

(b) directly or indirectly, cause dangerous or congested traffic conditions in any nearby road;

The form of development being a composite of processing, packing, associated storage (incoming and outgoing cool room storage) and associated office and workers

facilities are all considered to be substantially representative of 'light industry' which is identified as being appropriate within the zone and is unlikely to create unreasonable impact in respect of those highlighted issues.

Desired Character (Woodside Policy Area)

The Desired Character Statement for the *Light Industry (Woodside) Policy Area* provides that the Light Industry Zone forms part of the approach to Woodside from the north, being a highly visible element of the 'gateway' to the town. In this respect, the proposed development contributes a substantially modern industrial building, featuring aesthetics which surpass the 'standard appearance' of light industry and commercial 'shed shop fronts'.

The location of the development, practically, at the intersection of Onkaparinga Valley Road and Naughtons Road is afforded some excellent existing native vegetation forming part of the pleasant rural landscape along Onkaparinga Valley Road transitioning into the northern outskirts of Woodside. Large eucalypts and the widened road reserve minimise the visual impact of the Light Industry Zone and the commercial nature of the entrance into the town.

Heavy vehicle access to the site will be via Naughtons Road, which will minimise the impact of traffic movements on Onkaparinga Valley Road, and traffic movements back onto Onkaparinga Valley Road have been assessed and approved by DPTI (now DIT) to ensure that vehicle manoeuvring onto the road network is satisfactorily in accordance with relevant Australian Standards.

b) Council Wide provisions

The Council Wide provisions of relevance to this proposal seek to reinforce appropriate built form and design, safety of life and property from natural hazards, and for appropriate forms of industrial development to contribute positively to the surrounding natural and built environment. The fine balance of these matters is critical to the achievement of the Development Plan intent.

The following are considered to be the relevant Council Wide provisions:

Design and Appearance

Objective: 1

PDCs: 1, 3, 11, 12 & 27

The appearance of the proposed buildings in this application are considered to be acceptable in the context of the site and zone. The buildings will be set back substantially from the Onkaparinga Valley Road frontage exceeding the standards specified within the Zone Policy Area. The proposed building provides modern architectural styling, and incorporates appropriate muted colours & non-reflective finishes to external surfaces of the buildings.

As mentioned previously, landscaping comprising the retention of substantial eucalypts at the Onkaparinga Valley Road frontage and the incorporation of new landscaping at the Naughtons Road frontage will enhance the appearance of the development.

The site and proposed development *would* be visually prominent if not for the existing vegetation nearby the site frontage and also the substantial setbacks proposed.

In respect of scenic amenity, the subject land, within the Light Industry Zone, is not within the Scenic Route described in PDC 11, the *Onkaparinga Valley Scenic Route* on Figure AdHi(EC)/1 with the scenic route shown as commencing immediately north of the zone.

It is considered that the proposed development will be well concealed from clear and direct view and will not visually overwhelm the entrance into the township.

Hazards

Objectives: 1 & 5

PDCs: 1 & 8

The proposed development and the surrounding zone is situated within a 'Medium' Bushfire Risk Zone as shown in Bushfire Protection Area Map AdHi(BPA)/5.

The development is considered to be reasonably invulnerable to bushfire risk particularly due to the surrounding landscape and sparse distribution of vegetation generally about the site, notwithstanding continuous lines of vegetation to the west (along the opposite side of Onkaparinga Valley Road) and to the north (on the opposite side of Naughtons Road) could potentially increase this risk.

Landscaping, which has been discussed elsewhere for the benefit of visual amenity and appearance of the land and buildings and is unlikely to present an increase in risk for bushfire, due to the majority of the site being developed, or surfaced which should adequately mitigate the bushfire risk.

The Onkaparinga River traverses land on the opposite (western) side of the Onkaparinga Valley Road and receives stormwater run-off from adjacent land in high rainfall events. However, the river is unlikely to create any risk to the development being well outside of its floodplain, and noting that the majority of the building's floor area is raised to facilitate operating at an appropriate level for unloading and loading of trucks.

The proposal is therefore not considered to be at risk of fire or flooding/inundation hazards and is also considered unlikely to create any risk of pollution to the watercourse.

Industrial Development

Objectives: 3, 4, 5, & 6

PDCs: 2, 3, 4, 5, 6, 7 & 8

The proposed development is considered to represent an appropriate development within the Zone and Policy Area in respect of according appropriate levels of aesthetic/visual appearance, which is consistent with Objective 6 and PDC's 4, 5, 7 &

8 and accordingly appropriate levels of landscaping proposed and existing within the road reserve, and the proposed setbacks of the building will provide a soft landscaped, and spacious streetscape.

The proposal is in close proximity to the adjoining residence to the north at the corner of Onkaparinga Valley Road and Naughtons Road, which interfaces with two boundaries of the subject site. The proposed development affords as much spatial separation as reasonably practical from the residence's property boundaries to the proposed building, utilising the areas adjacent to those boundaries for thoroughfare and heavy vehicle movements.

The separation created by the layout of the site creates comparable setback and separation to that shown in PDC 2. This arrangement, although likely to have some noise and vibration impact upon the residence is considered to be appropriate. The application has been subject to Category 2 Public Notification and has not received any representations from the adjoining or nearby adjacent owners or occupiers of residential premises. The proponent has provided a copy of an agreement (Refer *Attachment – Neighbour Comment*) reached with the occupier of 10 Naughtons Road with regard to fencing, noise and landscaping. Those relevant to the planning considerations are included as recommended conditions of consent (refer to *conditions 9 & 10*).

The proposed development is considered to be compatible with the other established developments within the Light Industry Zone, with no representations from any other business owner or occupier.

Interface Between Land Uses

Objectives: 1 & 3

PDCs: 1 & 2

Specifically noting that the zone and Policy Area provide for dedicated low intensity industrial uses without prejudice from other incompatible uses within the zone, the interface between the adjoining residence and the proposed development is considered to be somewhat of a moot point in the assessment.

As discussed above, with respect to setback for industrial development at the zone boundary, the setback and spatial separation of the proposed building from the adjoining residence's boundaries is considered to be appropriate.

The proposal includes new post & rail fencing and associated retaining to a height of 2.8 metres. This indicatively provides for landscaping between the boundary and internal driveway/manoeuvring areas to assist in mitigating impacts.

The nature of the land use is considered to of a relatively low-impact nature, with the movement of trucks delivering produce to the site and exporting from the site presenting the main interface. Noise impacts are limited to plant and refrigeration noise emissions from the building, workers vehicle movements and internal works within the building.

The building's design and construction provides for 'tilt-up' concrete construction which is intended to mitigate sound transmission from within the building to the outside surrounding environment.

The impacts are not considered to be excessive or unreasonable acknowledging the intended land uses to be established within the zone and the protection that the Light Industry Zone is to be afforded in terms of avoidance of conflict with incompatible uses within the zone.

The land use is not expected to produce any undesirable conditions by way of process or materials used or by-product/waste created. The processes employed involve washing of produce, with generally mineral/organic material (soil, dirt and the like) captured with small volumes of the wastewater to be discharged and the majority of the volume of washing water to be recycled. There will be no waste materials disposed of on the site.

Landscaping Fences and Walls

Objectives: 1 & 2

PDCs: 1 & 2

The proposal provides for extensive fencing and retaining around the periphery of the site. The retaining walls are to be comprised of concrete sleepers to a maximum of 2.9 metres (south-eastern corner of the land), to 2.5 metres (north-eastern corner of the site) and at much lower levels (<1 metre) to the adjoining residence's boundaries.

Fences being established between existing light industry development is to be constructed of 1.8 metres chain link mesh atop the sections of concrete sleeper retaining walls.

Fences abutting the residential land to the north of the site (Residential allotment's south and east boundaries) are to be fenced with post and rail type sheet metal fencing and landscaped with appropriate species between manoeuvring areas, car parking and the boundary.

The treatment of boundary walls, fences and landscaping is considered to be of a reasonable standard within the Light Industry zone, appropriate for such uses and consistent with other existing fencing in the zone. The proposal has sought to create a satisfactory outcome for the adjoining residence boundaries, which will be improved and complemented with landscaping for noise attenuation.

The interface with the residential land and its fence treatment will not be readily apparent again due in large part to the existing roadside vegetation at, and nearby the intersection of Onkaparinga Valley Road and Naughtons Road and the relative concealment of that land from clear and direct view.

The subject land is also occupied by one river red gum tree (*Eucalyptus camaldulensis*) within the site with three 'council street trees' outside of the site within the Council administered Road reserve. Two of the trees in the road reserve are regulated trees.

As discussed previously the two regulated trees are not impacted by the development and the two smaller trees have Native Vegetation approval for removal.

The applicant's arboriculture report indicates that the two smaller 'non-regulated' trees are recommended for removal with the two larger *regulated trees* recommended to be retained and protected.

The regulated trees particularly play an important part in the landscape value of the locality and will also contribute substantially to the concealment of the proposed development from clear and direct view from Onkaparinga Valley Road.

The balance of the site's open areas are to be compacted gravel car parking and sealed truck manoeuvring areas, including truck un-loading area and two dedicated truck loading docks for exporting produce. The area generally surrounding the large river red gums is proposed to be left largely unsealed and landscaped. The trees' root protection zone requirements should be observed to ensure that the trees' health and longevity.

Other Matters – Stormwater

The site is changing from unsealed to largely sealed. The site is largely below Naughtons Road level and the swale drainage on Naughtons Road is at capacity. Therefore all stormwater from the site, roof and surface, is to be directed to Onkaparinga Valley Road and existing drainage in that area. Prior to discharge from the site bio-retention swales will treat the water. MUSIC modelling was provided to support this approach. Council engineering is supportive of the design presented in the amended documents presented to Council on 5 May 2021. Conditions 17 & 18 refer to engineering requirements for works to the site and the requirement for a separate Section 221 approval for works in the road reserve.

7. SUMMARY & CONCLUSION

The proposal has been assessed against the relevant provisions of the Adelaide Hills Development Plan, Consolidated 8 August 2019, and is considered to demonstrate adequate merit insofar that it has the potential to considerably improve upon and positively influence the existing built form and appearance industrial activity within the zone.

The proposal represents development of the Zone and Policy Area consistent with the desired character, to support and reflect *a vigorous local economy* and to encourage developments with built form that *enhances the overall views and rural character* of the zone and the locality generally.

The proposal fits well within the existing industrial and commercial activities in the zone and despite its scale, will contribute to an aesthetically pleasing transition from the surrounding rural environment to the Light Industry Zone, and on to the Township areas of Woodside.

The proposed building, site works and landscaping are of a high standard of design and appearance relative to the industrial nature of the development, and is not considered to be aesthetically overwhelming within the locality.

The proposed development is not subject to unreasonable risk or susceptibility to bushfire and will not propagate or perpetuate any risk to natural resources or the environment, including the Onkaparinga River in the adjacent Watershed (Primary Production) Zone.

The proposal is sufficiently consistent with the relevant provisions of the Development Plan, and it is considered the proposal is not seriously at variance with the Development Plan. In the view of staff, the proposal has sufficient merit to warrant consent. Staff therefore recommend that Development Plan Consent be **GRANTED**, subject to conditions.

8. RECOMMENDATION

That the Council Assessment Panel considers that the proposal is not seriously at variance with the relevant provisions of the Adelaide Hills Council Development Plan, and GRANTS Development Plan Consent to Development Application 20/1273/473 by Joyson Orchards Pty Ltd for Staged Consent Construction of light industry (agricultural industry) building & associated two storey offices, carparking, retaining walls (maximum height 3.5m), acoustic fence (maximum height 2.8m), signage, associated access work, landscaping & associated earthworks. Stage 1 earthworks, Stage 2 balance of the works at 202 and 204 Onkaparinga Valley Road and Lot 21 Naughtons Road Woodside subject to the following conditions:

(1) Development In Accordance With the Plans

The development herein approved shall be undertaken in accordance with the following plans, details and written submissions accompanying the application, unless varied by a separate condition:

- Plans from Beyond Ink drawing number PD000 Cover sheet and site plan text, 001 Site plan, 002 External Elevations, 101 Floor Plans, 102 Floor plans office, 103 Equipment Plan Revision A and PP101 Front Façade Carpark, 102 Naughtons Entry and 103 Front elevation all dated 25 November 2020
- Report from The Adelaide Tree Surgery dated 25 November 2020
- Traffic Engineering report from MFY Consultants dated 25 November 2020
- Stormwater Management Report from MLEI Consulting Engineers reference A2020-10798_A dated 18 February 2021
- Siteworks and Stormwater Management Plan drawing number A2020-10798 Sheets C00 Revision C dated 18 February 2021, C01 Revision D dated 9 February 2021, C1 Revision E dated 2 March 2021

(2) Soil Erosion, Drainage and Construction Management Plan – Prior to Stage 1 works

Prior to commence stage 1 works, the applicant shall prepare, submit and have approved a Soil Erosion, Drainage and Construction Management Plan (SEDCMP) for the site. The SEDCMP shall comprise a site plan and design sketches that detail erosion control methods, installation of sediment collection devices and detail vehicle access arrangements that will prevent:

- a) Soil moving off the site during periods of rainfall;
- b) Erosion;
- c) Soil moving into watercourses during periods of rainfall; and
- d) Soil transfer onto roadways by vehicles and machinery.

The works contained in the approved SEDCMP shall be implemented prior to construction commencing and maintained to the reasonable satisfaction of Council during the construction period.

- (3) **Requirement for Retaining Walls to Be Constructed Prior To Works Commencing**
The retaining walls on the southern and south-eastern side of property, as described on the site plan stamped as part of this authorisation, shall be constructed prior to the commencement of the construction of the proposed facility and retaining walls over one (1) metre in height will require Building Consent.
- (4) **Protection of Trees**
The works in relation to the tree(s), outlined in the Arborist's Report prepared by The Adelaide Tree Surgery and submitted as part of this application as a strategy for management of the tree(s) are to be undertaken simultaneously with any building works on the site.
- (5) **Commercial Lighting**
Flood lighting shall be restricted to that necessary for security purposes only and shall be directed and shielded in such a manner as to not cause nuisance to adjacent properties.
- (6) **Forklift Noise**
Any forklift noise arising during the loading and unloading of goods at the loading bays shall comply with the SA EPA Noise Policy at the residence at 10 Naughtons Road Woodside.
- (7) **Hours Of Operation – Truck Movements**
Monday to Friday 7.00am – 6.00pm
Saturday 7.30am – 3.30pm

Staff Parking - Vehicle Movements
Monday to Friday 6.30am – 6.00pm
- (8) **Truck Noise**
Trucks when stationery on the site must have their engines and refrigeration units turned off.
- (9) **Landscaping Timeframe For Landscaping To Be Planted**
Landscaping as detailed in the plan from Beyond Ink PD001 shall be planted in the planting season following occupation and maintained in good health and condition at all times. Any such vegetation shall be replaced in the next planting season if and when it dies or becomes seriously diseased.
- (10) **Stormwater infrastructure**
Stormwater infrastructure shall be installed within one month of roof sheeting being installed on the proposed building.
- (11) **One Way Truck Movements and Sealing of Truck Manoeuvring**
Trucks shall only move through the site in a one way direction with entry from Naughtons Road and exit via Onkaparinga Valley Road. Truck Manoeuvring Areas shall be sealed and directional signage to facilitate truck movement shall be installed prior to operation of the facility.

- (12) **Gravel carparking Designed In Accordance With Australian Standard AS 2890.1:2004.**
All car parking spaces, driveways and manoeuvring areas shall be designed, constructed, and line-marked in accordance with Australian Standard AS 2890.1:2004. Line marking and directional arrows shall be clearly visible and maintained in good condition at all times. Driveways, vehicle manoeuvring and parking areas shall be constructed of compacted gravel prior to occupation and maintained in good condition at all times to the reasonable satisfaction of the Council.
- (13) **Landscaping Protection In Carparks**
All landscaped areas and structures adjacent to driveways and parking areas shall be separated by a wheel stop device prior to the occupation of the development. Such devices shall not impede the free movement of people with disabilities.
- (14) **Unloading And Storage Of Materials And Goods**
All materials and goods shall at all times be loaded and unloaded within the confines of the subject land. Materials and goods shall not be stored on the land in areas delineated for use as car parking.
- (15) **Requirement For Gates Or Doors To Be Within Boundary**
Gates or doors shall not open beyond the boundary of the subject property.
- (16) **Car Parking Stormwater Runoff – Commercial**
All surface water from car parking or hardstand areas shall be directed to a proprietary pollutant treatment device capable of removing oils, silts, greases, and gross pollutants to Council and EPA satisfaction prior to discharge to Council stormwater system or street water table.
- (17) **Prior to Building Consent**
The applicant shall consolidate the titles and extinguish the easement in favour of Council for drainage purposes.
- (18) **Prior to Ruiling Rules**
A separate application (Section 221 of the Local Government Act) with detailed design of the proposed works to Onkaparinga Valley Road and the access point on Naughtons Road is required to be lodged and approved.
- (19) **DIT conditions**
1. The access points to Onkaparinga Valley Road service road and Naughtons Road shall be constructed as shown on Beyond Ink, Site Plan, Sheet No. PD001, Revision A dated 24 November 2020.
 2. The Onkaparinga Valley Road service road shall be modified to accommodate the swept path of a 19 metre Articulated Vehicle in order to permit a suitably angled approach to Onkaparinga Valley Road. The modifications shall be generally consistent with MFY Figure 5 with all costs borne by the applicant.

3. Large commercial vehicles accessing the site shall be restricted to right turn entry only movements to Naughtons Road and left turn exit only movements into the service road and Onkaparinga Valley Road. The movements and traffic flow through the site shall be reinforced with suitable signage and line marking as well as the traffic management plan initiated by the applicant/operator.
4. All vehicles shall enter and exit the site in a forward direction.
5. Any infrastructure within the road reserve that is demolished, altered, removed or damaged during the construction of the project shall be reinstated to the satisfaction of the relevant asset owner, with all costs being borne by the applicant.
6. The car park design shall be designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009*. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
7. Commercial vehicle facilities shall be designed in accordance with *AS2890.2:2018*.
8. Any stormwater run-off shall be collected on-site and disposed of safely without jeopardising the safety of the adjacent roads. Any alterations to the existing road drainage infrastructure as a result of this development shall be at the expense of the applicant.

NOTES

(1) **Development Plan Consent Expiry**

This Development Application is valid for a period of twenty four (24) months commencing from the date of the decision.

Building Consent must be applied for prior to the expiry of the DPC and lodged through the PLANSA portal unless a private certifier was engaged prior to 19 March 2021.

(2) **EPA Environmental Duty**

The applicant is reminded of his/her general environmental duty, as required by Section 25 of the Environment Protection Act 1993, to take all reasonable and practical measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes, or may cause, environmental harm.

(3) **Works On Boundary**

The development herein approved involves work on the boundary. The onus of ensuring development is in the approved position on the correct allotment is the responsibility of the land owner/applicant. This may necessitate a survey being carried out by a licensed land surveyor prior to the work commencing.

(4) **Responsibility In Relation To Flooding**

The applicant is reminded that Adelaide Hills Council accepts no responsibility for damage to, or loss of property, as a result of flooding. It is the applicant's responsibility to ensure that all appropriate steps are undertaken to minimise the potential damage to property as a result of flooding.

(5) Native Vegetation Council Requirements

The applicant is advised that any proposal to clear, remove limbs or trim native vegetation on the land, unless the proposed clearance is subject to an exemption under the Regulations of the Native Vegetation Act 1991, requires the approval of the Native Vegetation Council. For further information visit:

www.environment.sa.gov.au/Conservation/Native_Vegetation/Managing_native_vegetation

Any queries regarding the clearance of native vegetation should be directed to the Native Vegetation Council Secretariat on 8303 9777. This must be sought prior to Full Development Approval being granted by Council.

9. ATTACHMENTS

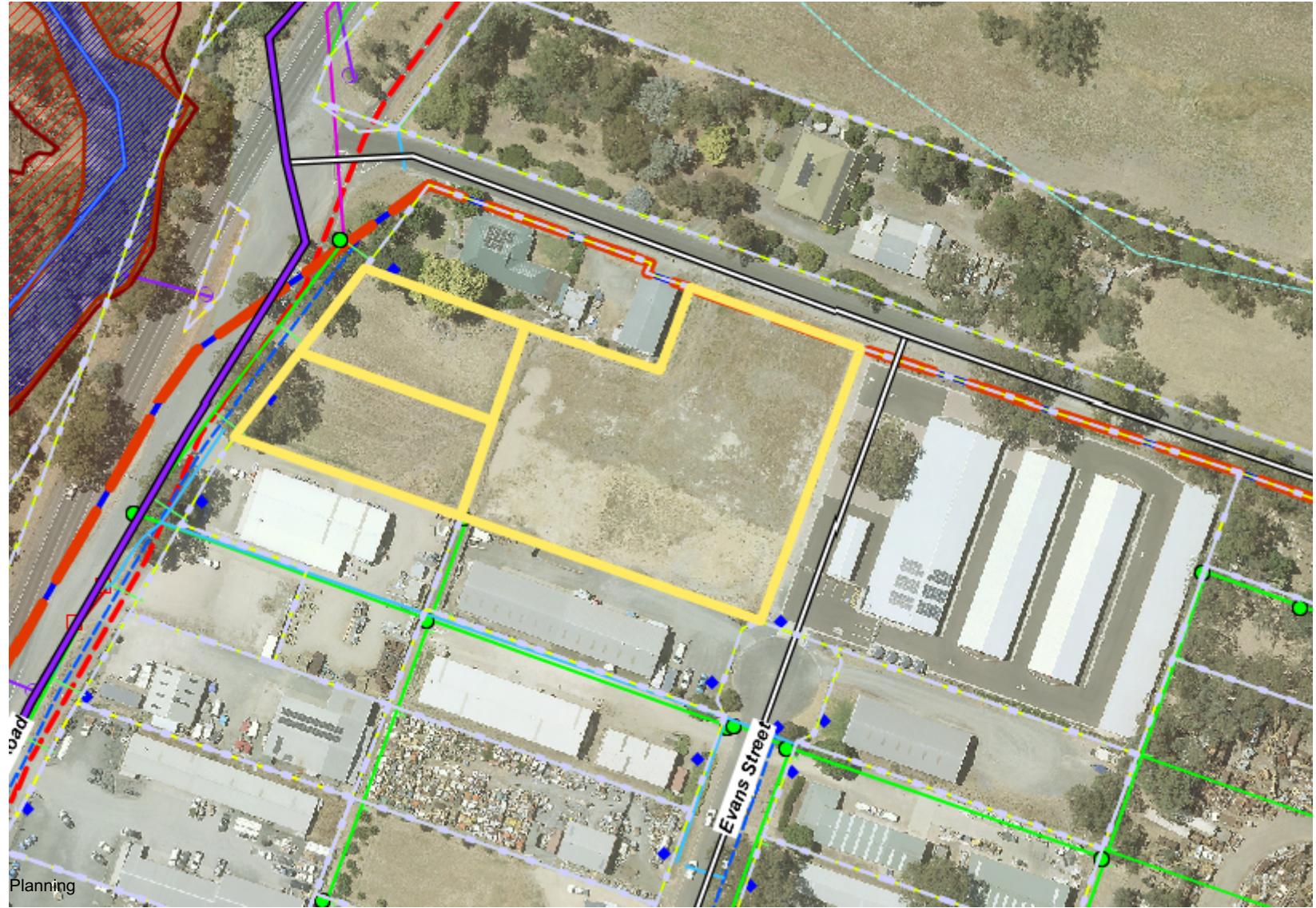
Locality Plan
Proposal Plans
Application Information
Applicant's Professional Reports
Referral Responses
Representation
Applicant's response to representations
Publically Notified Plans

Respectfully submitted

Concurrence

Melanie Scott
Senior Statutory Planner

Deryn Atkinson
Assessment Manager



Annotations

- Feature3
- Feature2
- Subject Land

Planners Summary

- PlanningSummary

AHC Core

- Parks
- Townships
- RoadsStreetView
 - ADJOINING LGA RD
 - AHC & PRIVATE
 - AHC RD
 - DPTI RD
 - PRIVATE RD
 - SHARED RD

- PropertyOwner
- Parcels
- Roads
- Suburbs
- Rivers
 - River
 - Creeks
 - Streams

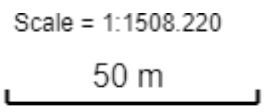
Flood Study Data

- TorrensFloodZones_20Yr
- TorrensFloodZones_100Yr
- OnkaFloodPlain10Yr

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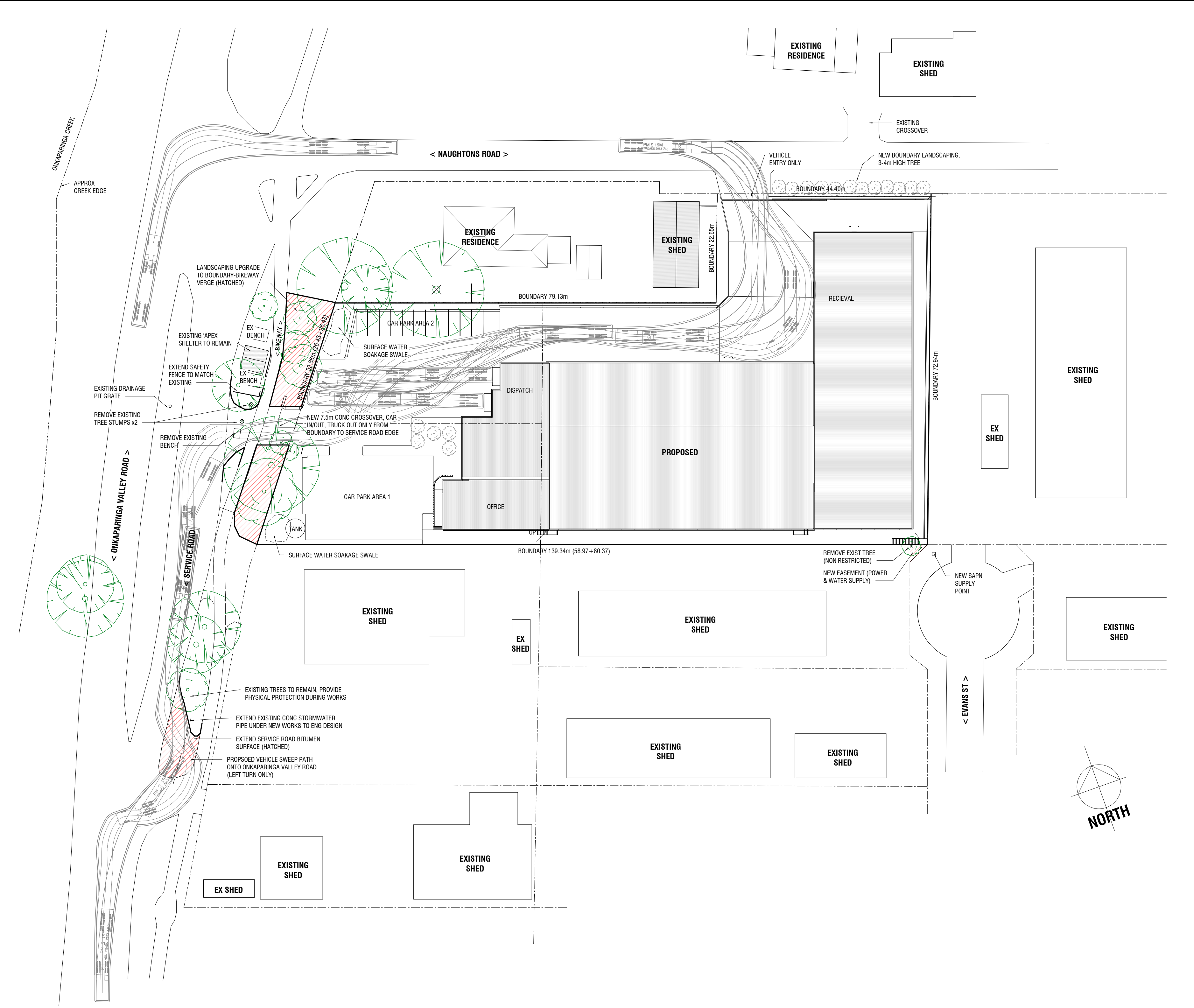
representations regarding the use, or results of use of the information contained herein as to its correctness, accuracy, currency or otherwise. In particular, it should be noted that the accuracy of property boundaries when displayed over aerial photography cannot be considered to be accurate, and that the only certain method of determining boundary locations is to use the services of a licensed Surveyor. The Adelaide Hills Council, its

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Planning

Evans Street



SITE PLAN EXTENDED
1 : 500

JOYSON ORCHARDS ADELAIDE HILLS FRESH FRUIT PACKING FACILITY

198-200 ONKAPARKINGA VALLEY ROAD, WOODSIDE SA 5244



ADELAIDE HILLS COUNCIL
RECEIVED 26/11/2020

Rev No	Rev Date	Revision Description
A	24/11/20	Issued for Council planning discussion.

BEYONDINK
DESIGN | COUNCIL | BUILD
52A Main Street, Hahndorf SA 5245 T 8388 1170
www.beyondink.com.au ABN 36 960 210 275

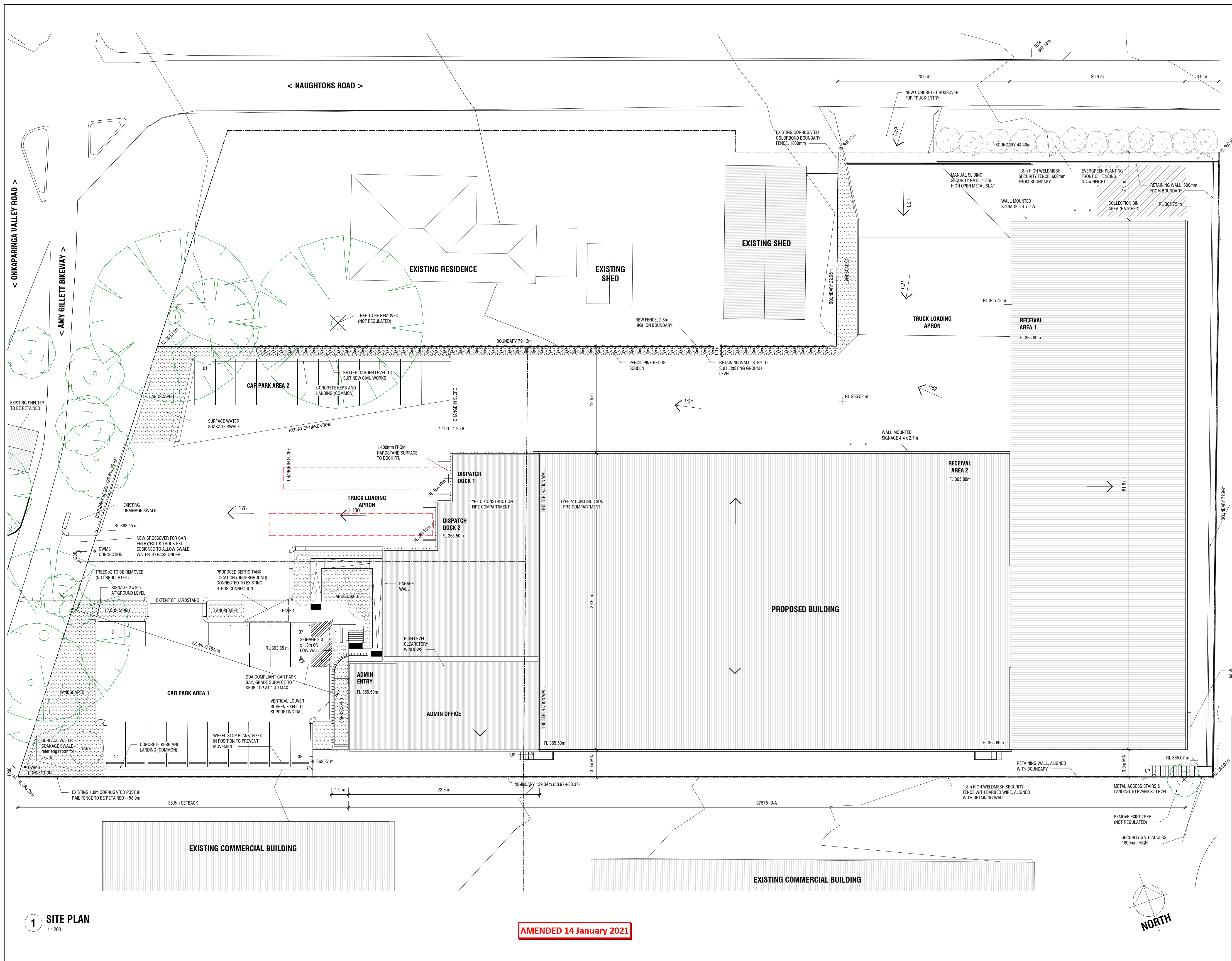
3D AND PERSPECTIVE DRAWINGS MAY BE INACCURATE OR INCOMPLETE. THEY ARE CONSIDERED TO BE INDICATIVE.

JOYSON ORCHARDS, ADELAIDE HILLS
FRESH, FRUIT PACKING FACILITY
198-200 Onkaparinga Valley Road, WOODSIDE SA

COVER SHEET & SITE PLAN EXT

PRELIMINARY Revision **A**

Drawn AH Sheet Number
Date 25/11/2020 **PD000**
Scale @A1 size : 1 : 500



ABBREVIATIONS	
ABBREV.	DESCRIPTION

Areas Schedule		
Name	Area	Level
ALLOTMENT (All Titles)	3888.60 m ²	FL-0 (N)
OFFICE (1)	233.51 m ²	FL-1 (N)
OFFICE (G)	239.05 m ²	FL-0 (N)
PROCESSING	3488.72 m ²	FL-0 (N)

Rev No	Rev Date	Revision Description
B	11/01/20	Council Response Letter: Added coolroom plant area (x2), bin area, CWMS points x2.
A	24/11/20	Issued for Council planning discussion.

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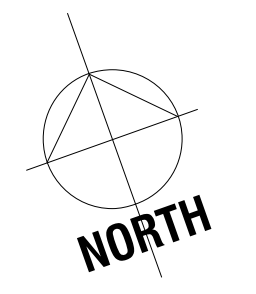
3D AND PERSPECTIVE DRAWINGS MAY BE INACCURATE OR INCOMPLETE. THEY ARE CONSIDERED TO BE INDICATIVE.

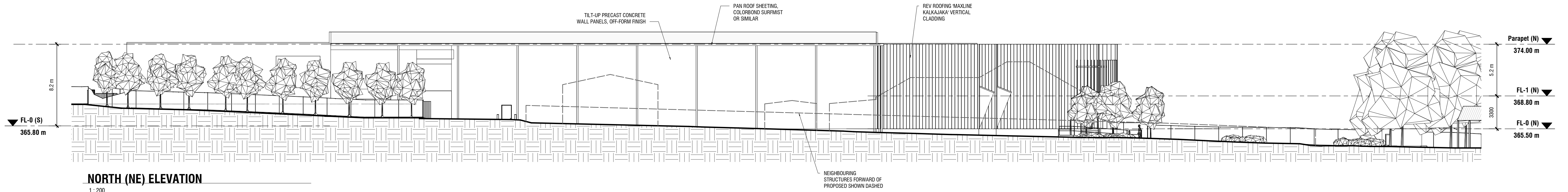
JOYSON ORCHARDS, ADELAIDE HILLS FRESH, FRUIT PACKING FACILITY
 198-200 Onkaparinga Valley Road, WOODSIDE SA

SITE PLAN
 PRELIMINARY
 Revision **B**
 Drawn AH
 Date 25/11/2020
 Scale @A1 size : 1:200
 Sheet Number **PD001**

AMENDED 14 January 2021

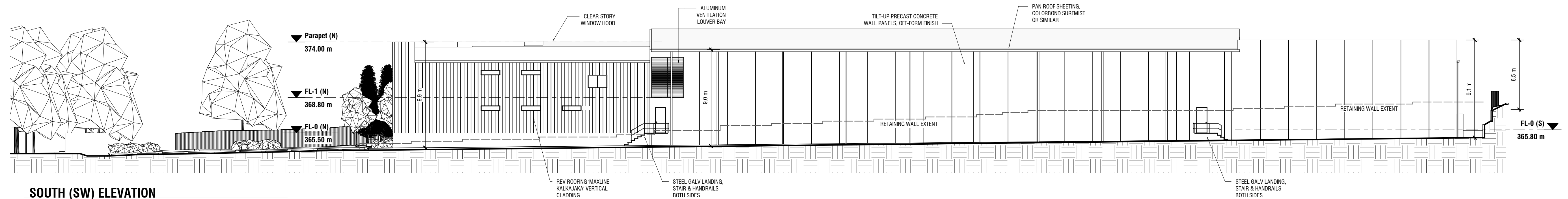
1 SITE PLAN
 1:200





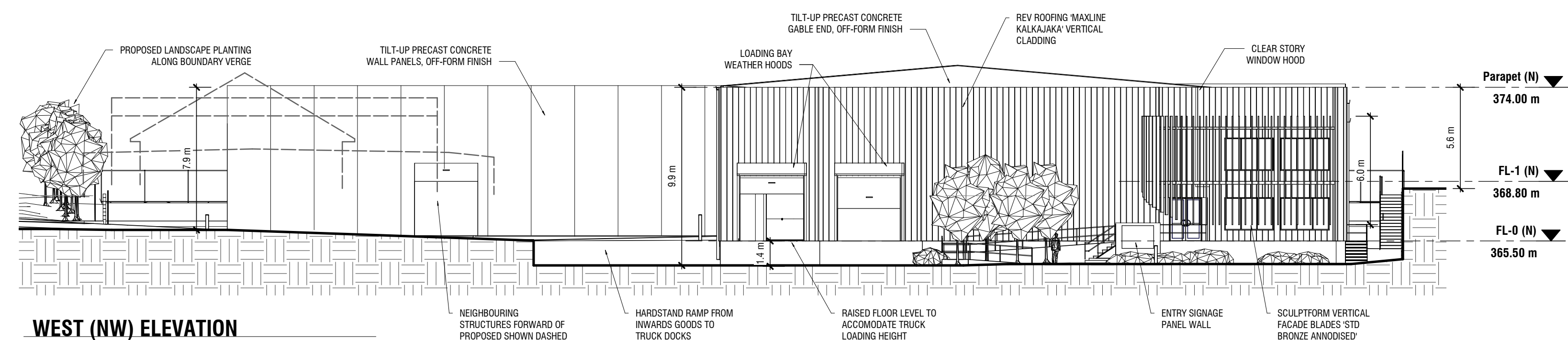
NORTH (NE) ELEVATION

1 : 200



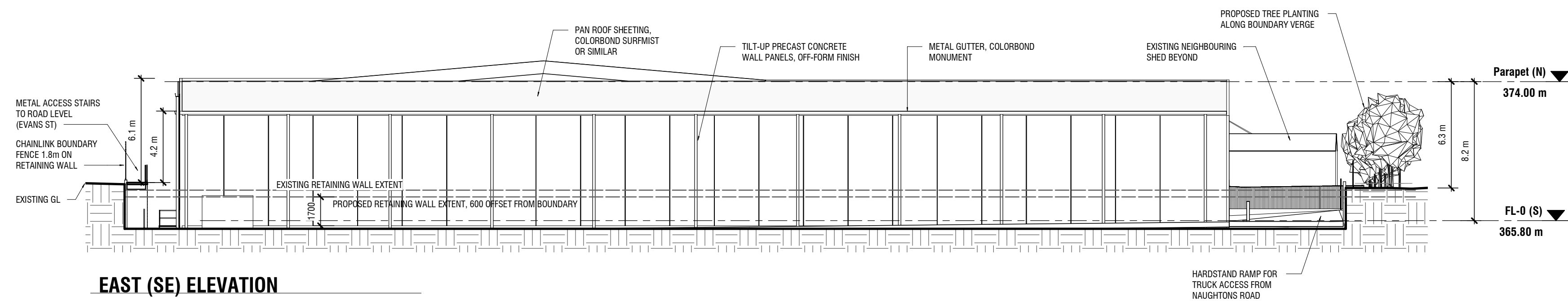
SOUTH (SW) ELEVATION

1 : 200



WEST (NW) ELEVATION

1 : 200



EAST (SE) ELEVATION

1 : 200

Rev No	Rev Date	Revision Description
A	24/11/20	Issued for Council planning discussion.

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JOYSON ORCHARDS, ADELAIDE HILLS FRESH, FRUIT PACKING FACILITY

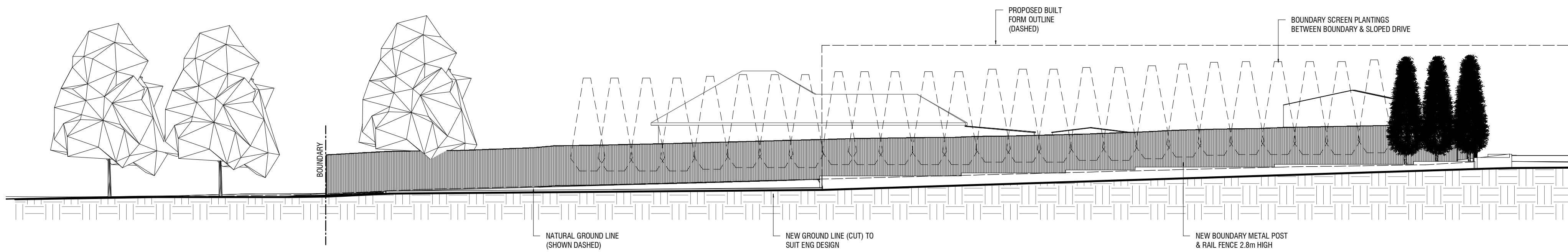
198-200 Onkaparinga Valley Road, WOODSIDE SA

EXTERNAL ELEVATIONS

PRELIMINARY Revision **A**

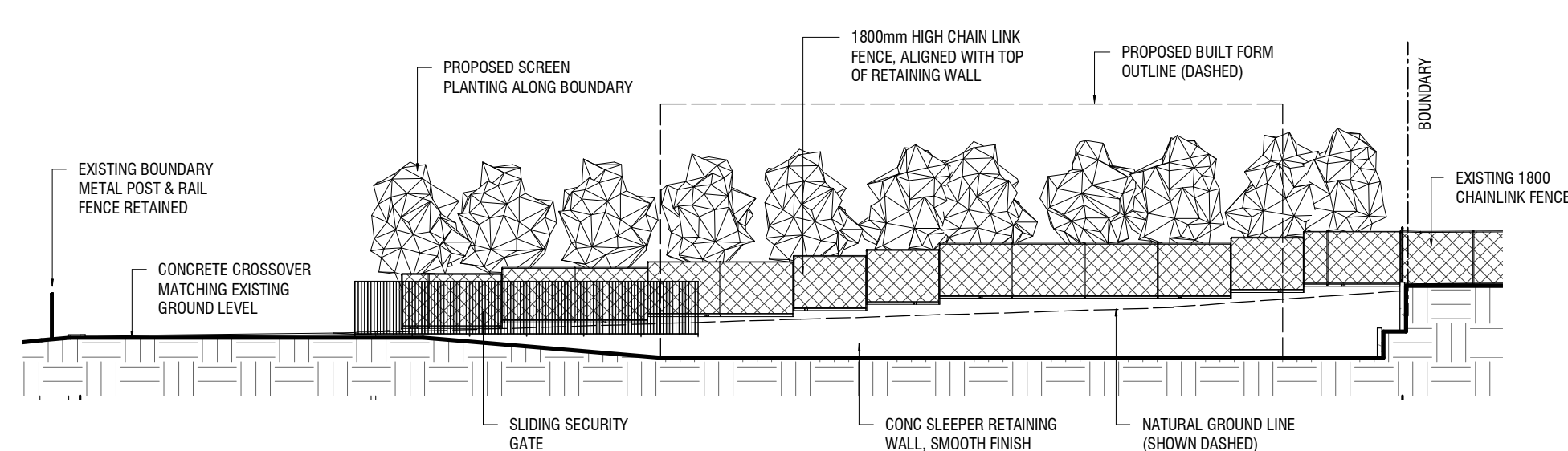
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Sheet Number **PD002**



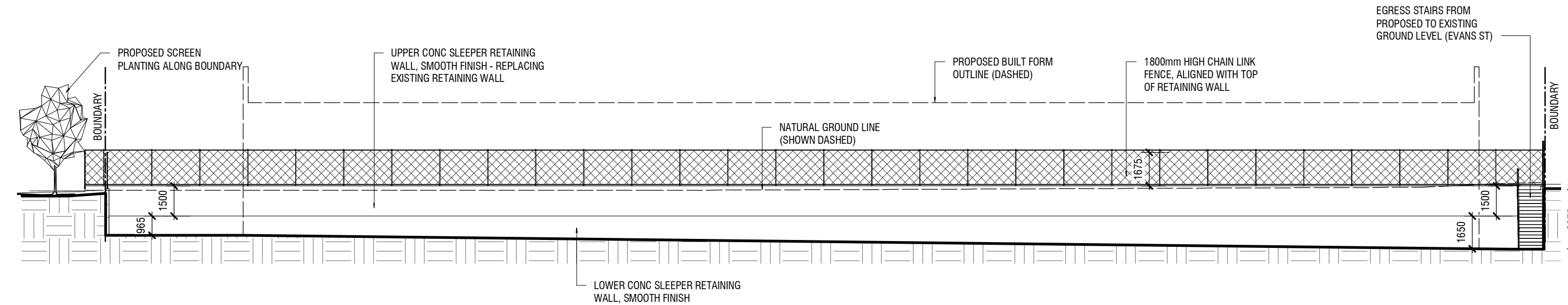
BOUNDARY ELEVATION NORTH 2 (NE)

1 : 200



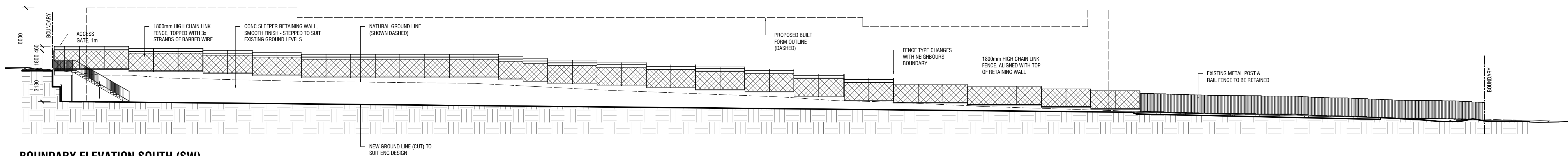
BOUNDARY ELEVATION NORTH (NE)

1 : 200



BOUNDARY ELEVATION EAST (SE)

1 : 200



BOUNDARY ELEVATION SOUTH (SW)

1 : 200

ADELAIDE HILLS COUNCIL
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Rev No	Rev Date	Revision Description
A	22/01/21	Update boundary retaining walls to match engineering update

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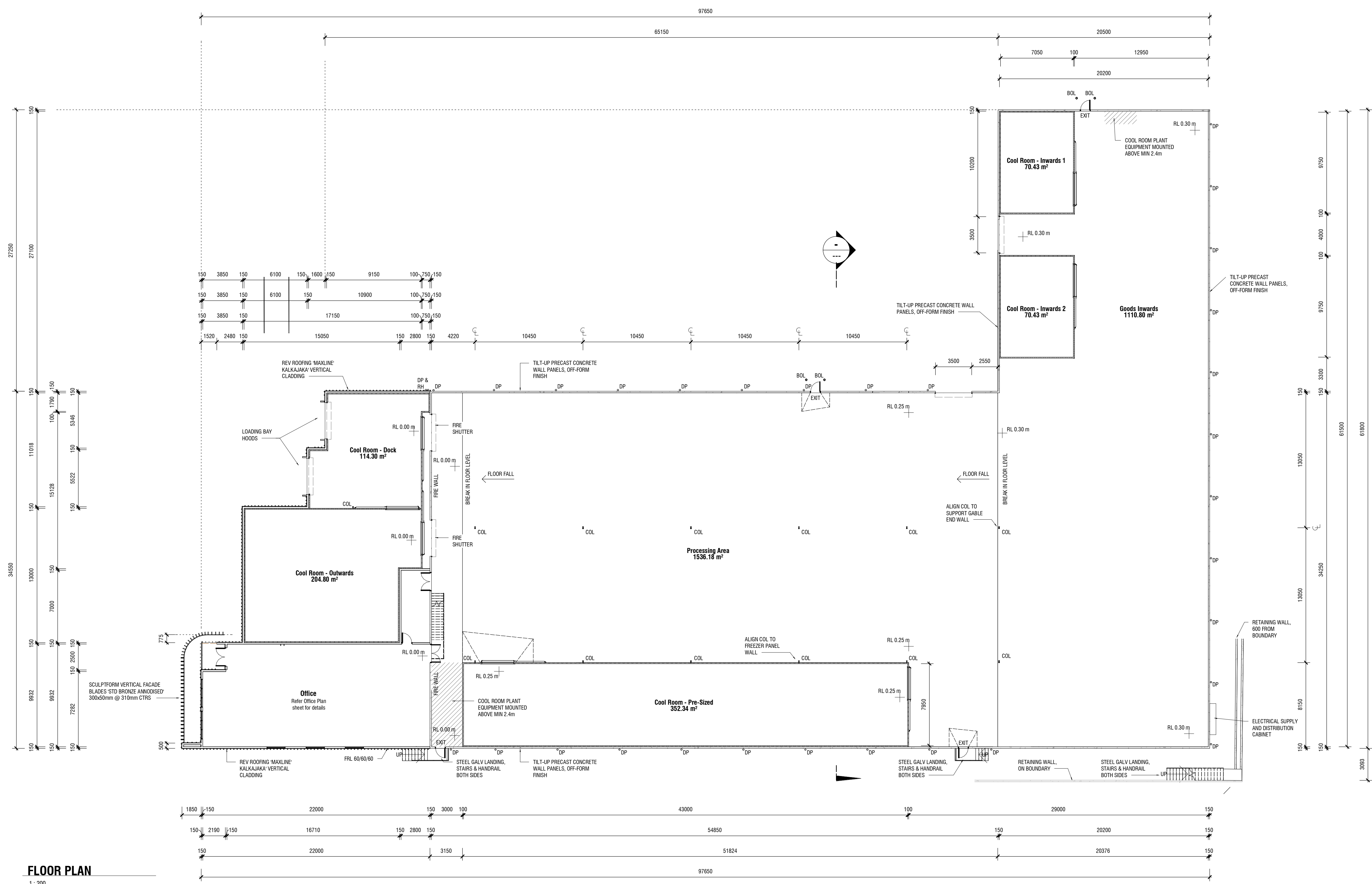
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198-200 Onkaparinga Valley Road, WOODSIDE SA

ELEVATIONS - BOUNDARY

PRELIMINARY Revision **A**

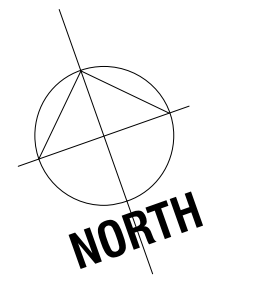
Drawn AH
Date 11/01/21
Scale @A1 size : 1 : 200
Sheet Number **PD003**

ABBREVIATIONS	
ABBREV.	DESCRIPTION



FLOOR PLAN
1 : 200

AMENDED 14 January 2021



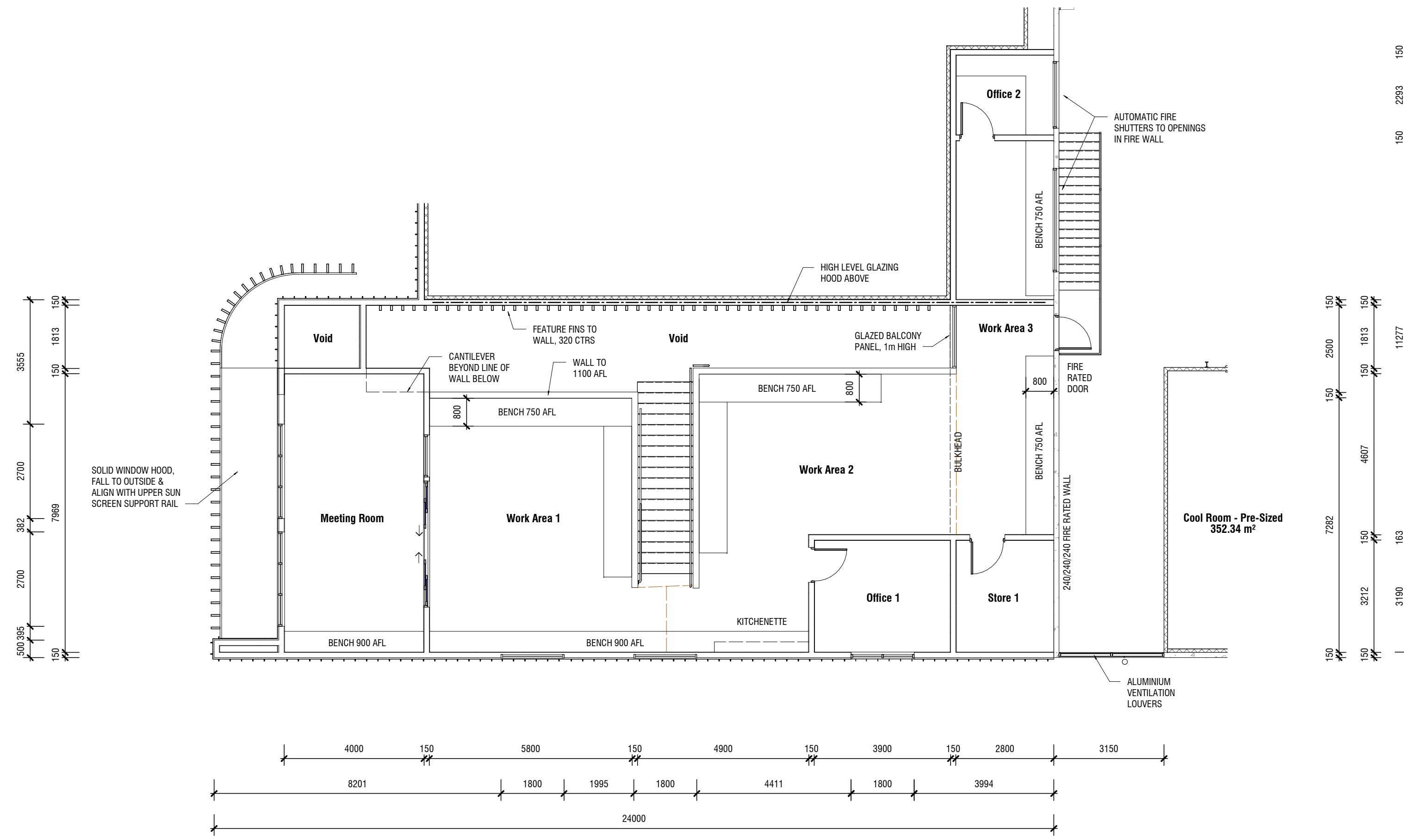
Rev No	Rev Date	Revision Description
B	11/01/20	Added coolroom plant areas (x2)
A	24/11/20	Issued for Council planning discussion.

BEYONDINK
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3D AND PERSPECTIVE DRAWINGS MAY BE INACCURATE OR INCOMPLETE. THEY ARE CONSIDERED TO BE INDICATIVE.

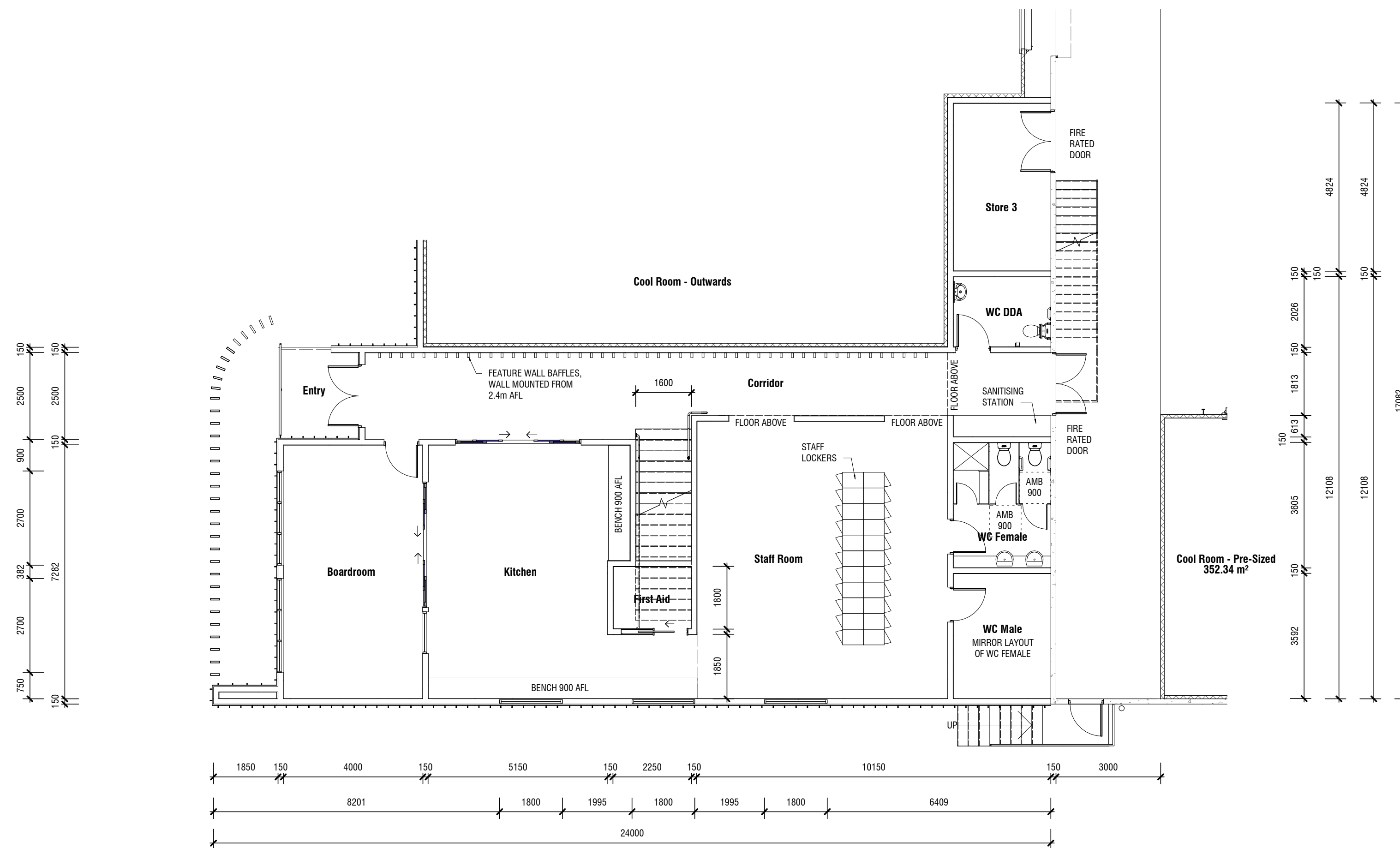
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198-200 Onkaparinga Valley Road, WOODSIDE SA

FLOOR PLAN	
PRELIMINARY	Revision B
Drawn AH	Sheet Number
Date 25/11/2020	PD101
Scale @A1 size : 1 : 200	



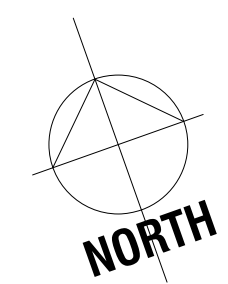
FLOOR PLAN - OFFICE UPPER

1 : 100



FLOOR PLAN - OFFICE LOWER

1 : 100



Rev No	Rev Date	Revision Description
A	24/11/20	Issued for Council planning discussion.

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**JOYSON ORCHARDS, ADELAIDE HILLS
 FRESH, FRUIT PACKING FACILITY**

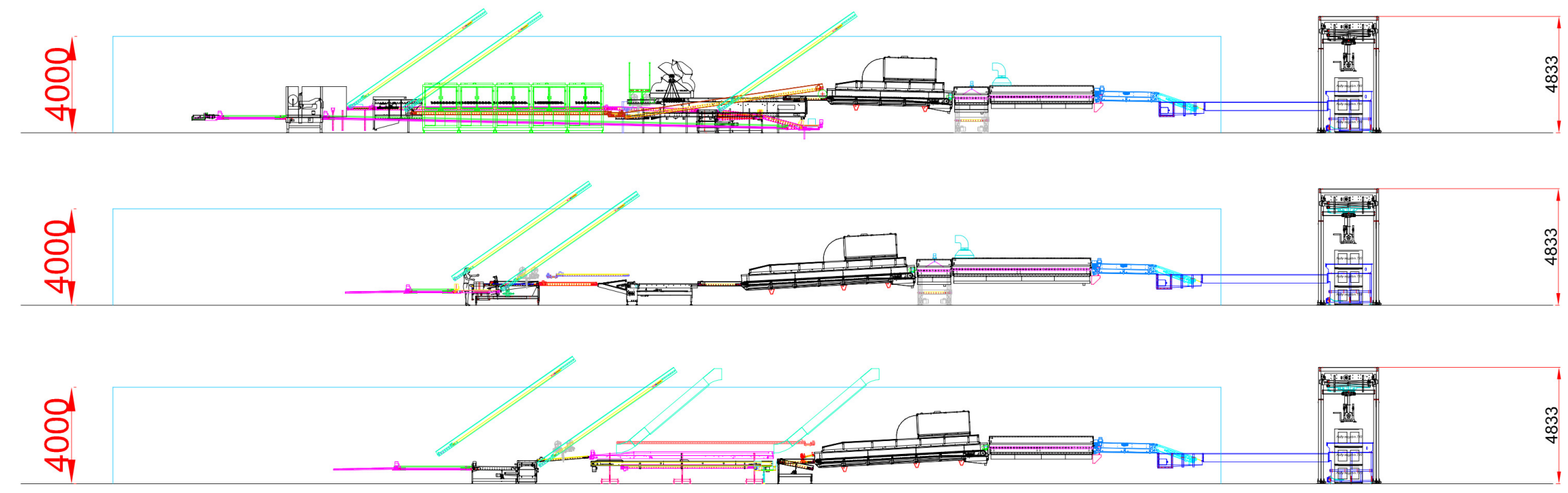
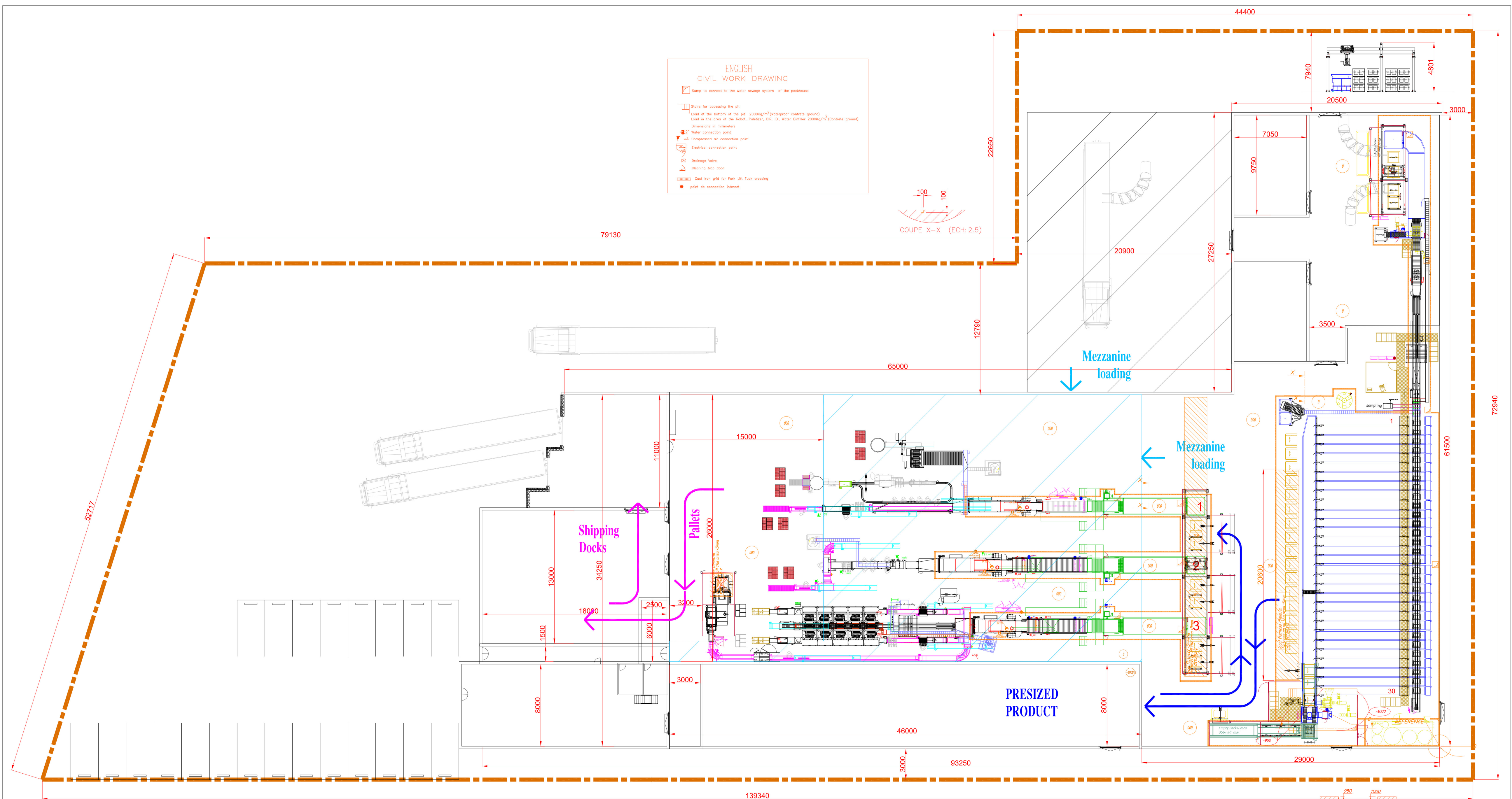
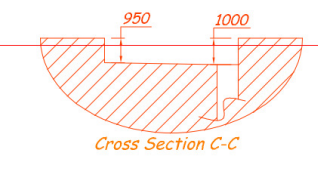
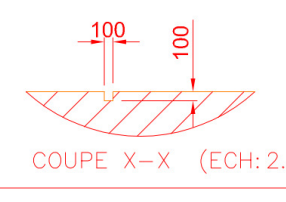
198-200 Onkaparinga Valley Road, WOODSIDE SA

FLOOR PLANS - OFFICE

PRELIMINARY Revision **A**

Drawn AH
 Date 24/11/2020
 Scale @A1 size : As indicated
 Sheet Number **PD102**

- ENGLISH
CIVIL WORK DRAWING
- Same to connect to the water sewage system of the building
 - Same for accessing the pit
 - Load at the bottom of the pit 200kg/m² (external concrete ground)
 - Load in the area of the floor, Pavement, G/H, G/L, Water Surface 200kg/m² (concrete ground)
 - Dimensions in millimeters
 - Water connection point
 - Air-Compressed air connection point
 - Electrical connection point
 - Drainage lines
 - Clearing trap door
 - Cast iron grid for Fork Lift Truck crossing
 - point de connection internet



31 APPROVED CUSTOMER: ADELAIDE HILLS FRESH (REVISED) 2018 (Landscape Hills Fresh 43 A200625)

Approved: _____

Product: APPLE

This layout remains the exclusive property of MAF Agrobotic until finalisation of the contract and the final agreement. Any breach may result in legal proceedings.

CUSTOMER: ADELAIDE HILLS FRESH
Address: WOODSIDE SA
Country: AUSTRALIA Phone: _____

Drawn by: IS/VR
Salesman: ALEXANDER

Scale: 1:100
Date: 27/06/2020

MAF Agrobotic
Rue de France - 30, Route 8
38811 St - 82001 Montebon
FRANCE
www.beyondink.com.au

DRAWING N°: 31 A200625

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A	24/11/20	Issued for Council planning discussion.

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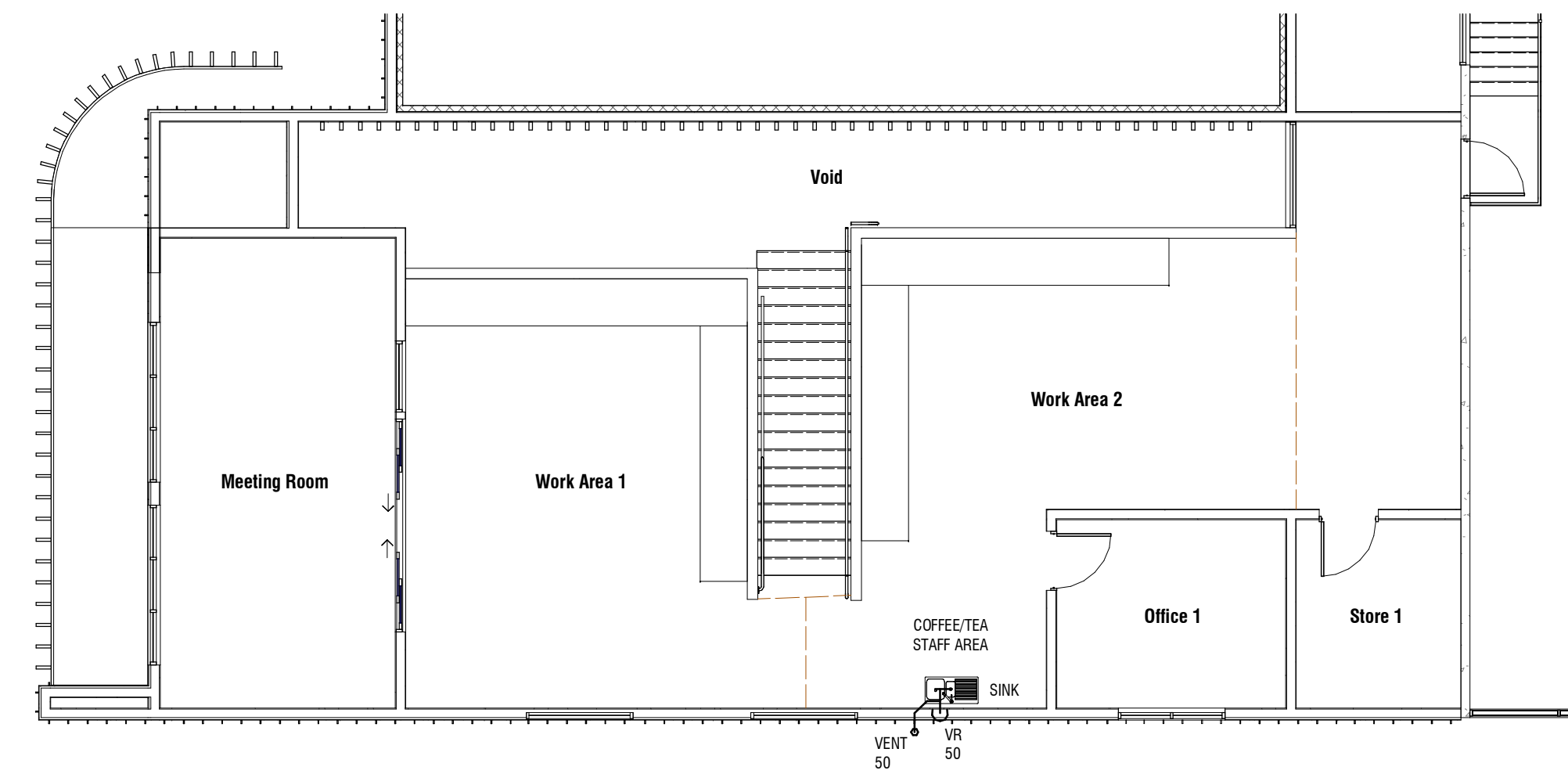
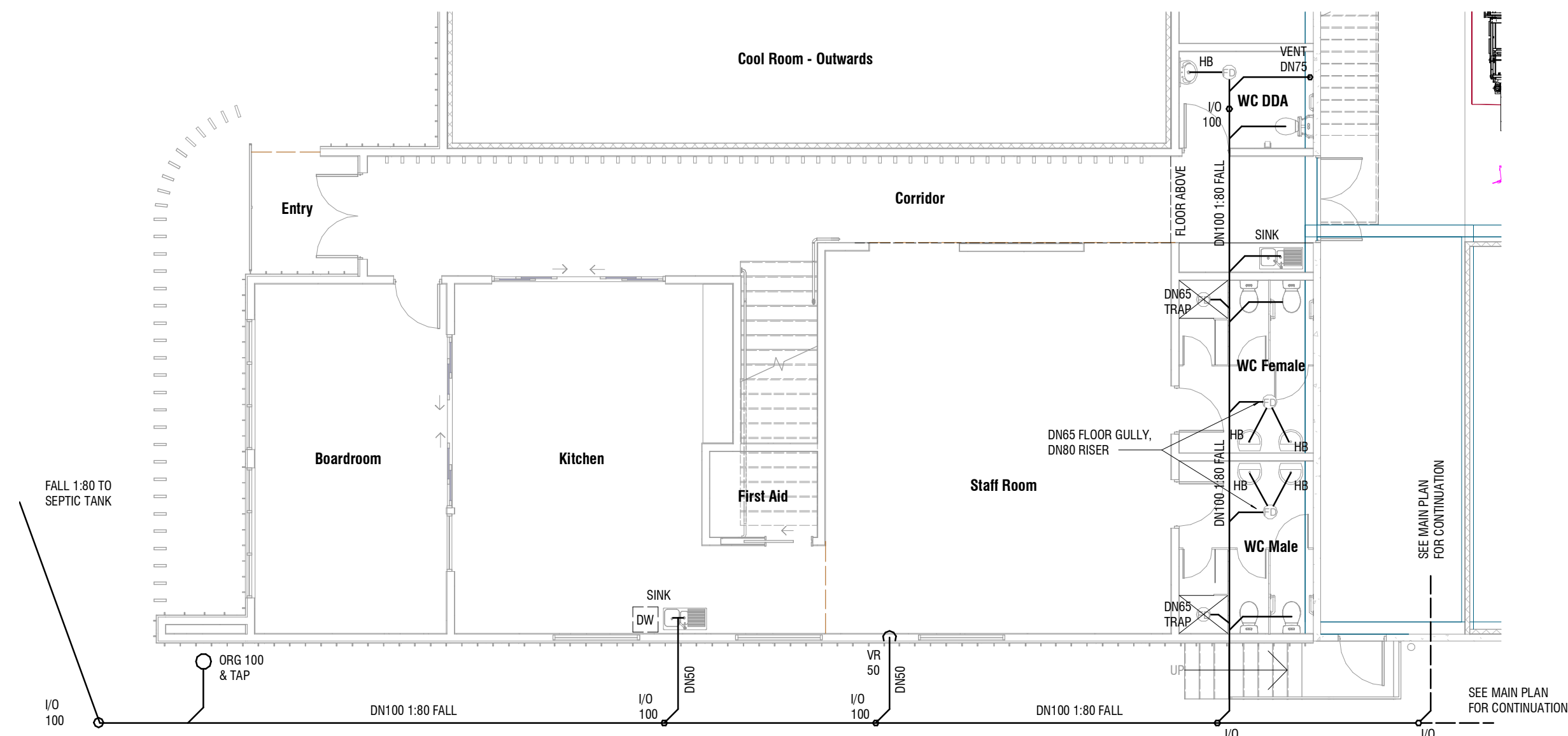
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JOYSON ORCHARDS, ADELAIDE HILLS
FRESH, FRUIT PACKING FACILITY

198-200 Onkaparinga Valley Road, WOODSIDE SA

EQUIPMENT PLAN

PRELIMINARY	Revision	A
Drawn: AH	Sheet Number	PD103
Date: 25/11/2020		
Scale: @A1 size:		



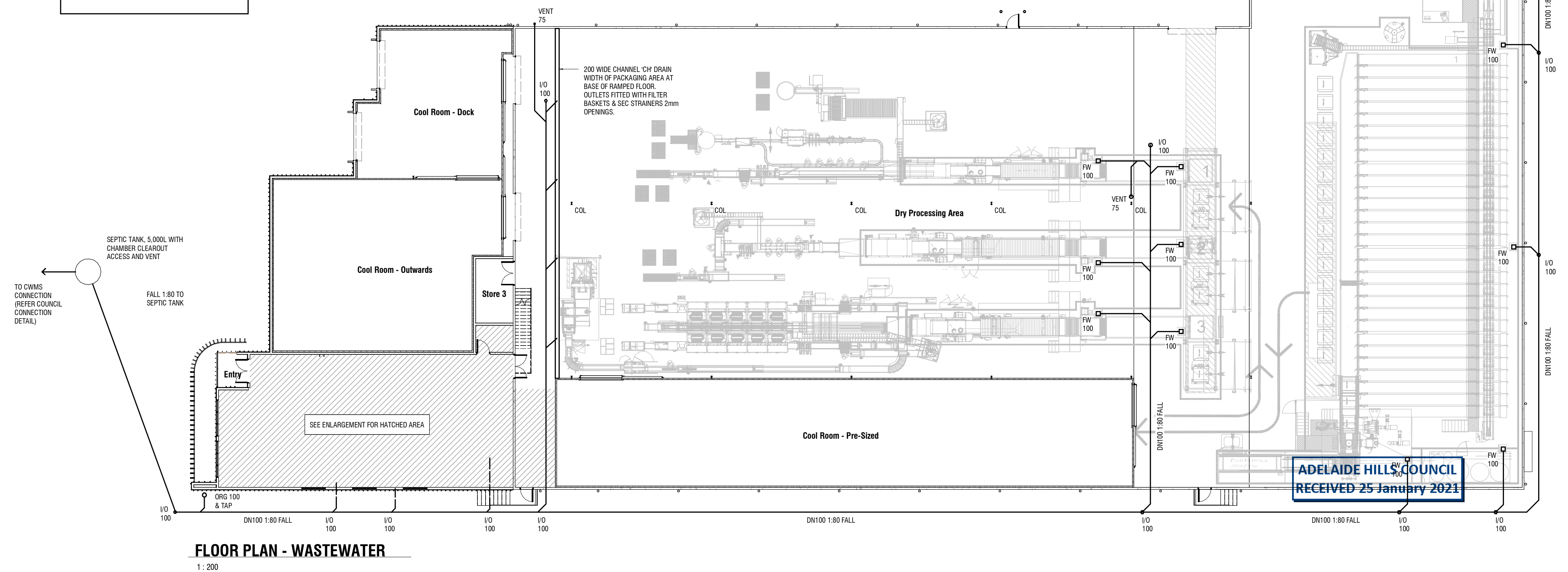
NOTE: ALL WASTEWATER WORKS ARE TO BE INSTALLED TO AS 3500. REPORT CONFLICTS OR ISSUES TO PROJECT SUPERINTENDENT PRIOR TO COMMENCING WORKS.

FLOOR WASTE (FW): 200SQ FLOOR WASTE, TRAPPED (P-TRAP OR REMOVABLE TRAP) DN100 OUTLET, FORKLIFT RATED GRATE WITH INTEGRATED FILTER BASKET & SECONDARY STRAINER (BOTH WITH 2mm HOLES AS PER SA WATER REQUIREMENTS), WATERMARK APPROVED.

CHANNEL (CH): 200mm WIDE CHANNEL WITH FALL TO OUTLET(S), TRAPPED (P-TRAP OR REMOVABLE TRAP) DN100 OUTLET, FORKLIFT RATED GRATE WITH INTEGRATED FILTER BASKET & SECONDARY STRAINER (BOTH WITH 2mm HOLES AS PER SA WATER REQUIREMENTS), WATERMARK APPROVED.

INSPECTION OPENING (IO): BRING POINT TO SERVICEABLE POSITION, SEALED CAP AND TRAFFICABLE WHERE FLUSHED WITH TRAFFICABLE AREA.

OVERFLOW RELIEF GULLY (ORG): POSITION WITH TAP OVER TO CHARGE GULLY.



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Rev No	Rev Date	Revision Description
BEYONDINK DESIGN COUNCIL BUILD 52A Main Street, Hahndorf SA 5245 T 8388 1179 www.beyondink.com.au		
3D AND PERSPECTIVE DRAWINGS MAY BE INACCURATE OR INCOMPLETE. THEY ARE CONSIDERED TO BE INDICATIVE.		
JOYSON ORCHARDS, ADELAIDE HILLS FRESH, FRUIT PACKING FACILITY 198-200 Onkaparinga Valley Road, WOODSIDE SA		
WASTEWATER PLANS		
PRELIMINARY		Revision
Drawn	AH	Sheet Number
Date	22/01/2021	PD104
Scale	@A1 size : As indicated	



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 198-200 Onkaparinga Valley Road,
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Sheet Name	Front Facade Carpark	
Drawn	AH	Sheet Number
Date	25/11/2020	PP101
Scale	@A3 size	

Revision Number	Revision Date	Revision Description



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198-200 Onkaparinga Valley Road,
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Sheet Name	NAUGHTONS ENTRY	
Drawn	AH	Sheet Number
Date	25/11/2020	PP102
Scale	@A3 size	

Revision Number	Revision Date	Revision Description



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PRELIMINARY

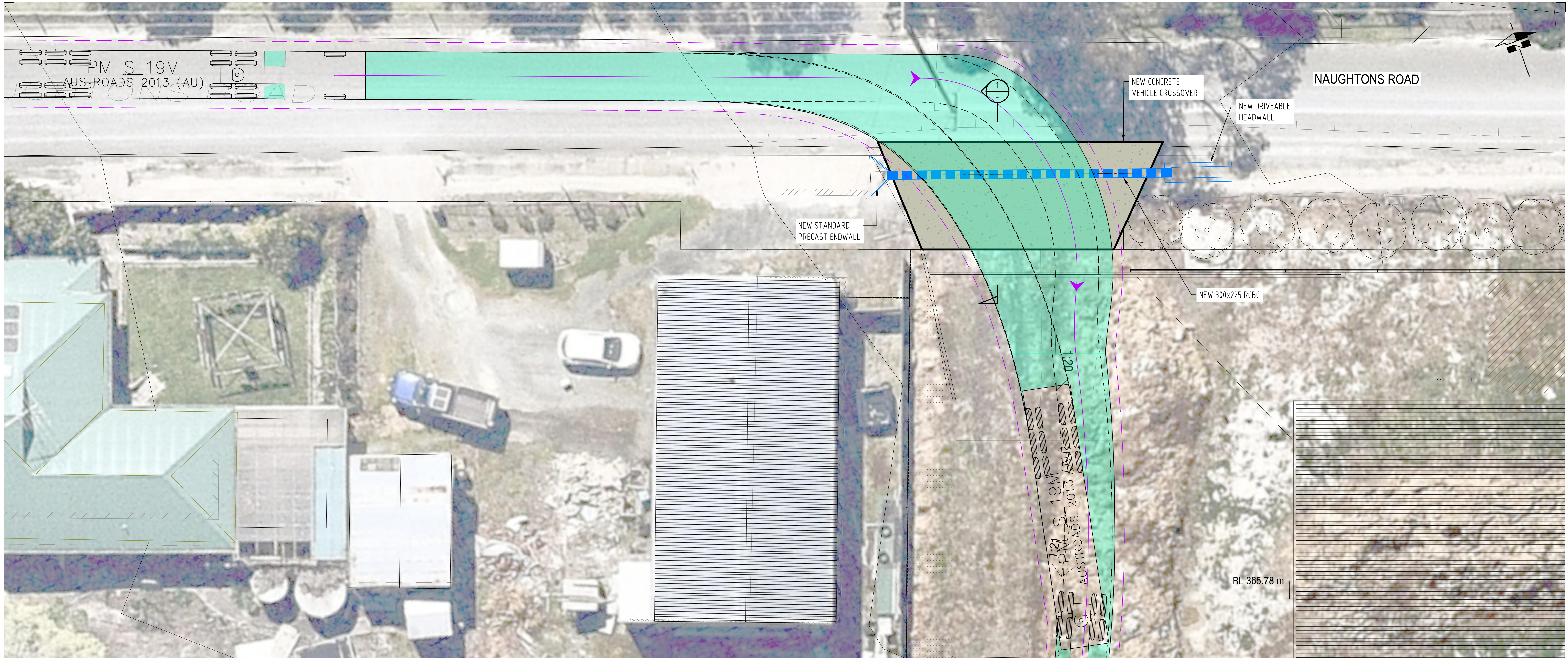
BEYONDINK
 DESIGN | COUNCIL | BUILD
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www.beyondink.com.au
 ABN 36 960 210 275

JOYSON ORCHARDS, ADELAIDE HILLS
 FRESH, FRUIT PACKING FACILITY
 198-200 Onkaparinga Valley Road,
 WOODSIDE SA

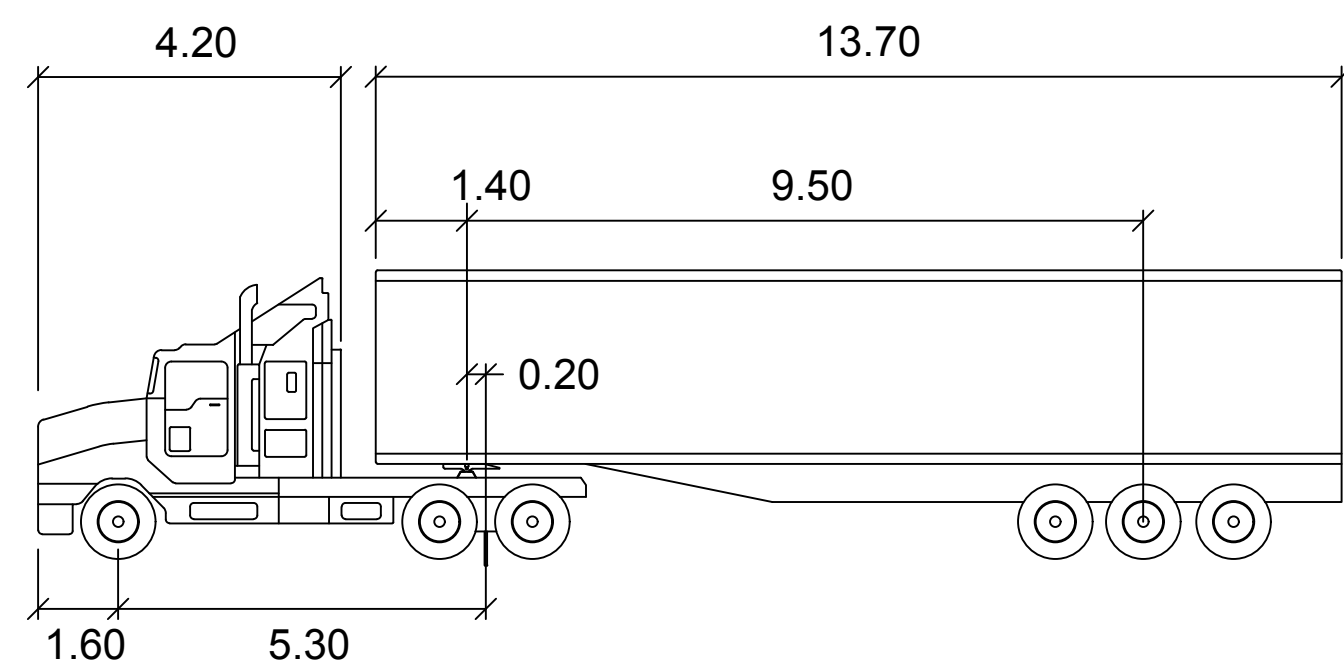
Sheet Name	FRONT ELEVATED	
Drawn	AH	Sheet Number
Date	25/11/2020	PP103
Scale	@A3 size	

Revision Number	Revision Date	Revision Description

ALL DIMENSIONS, LEVELS AND BOUNDARY LOCATIONS SHALL BE VERIFIED ON SITE PRIOR TO CONSTRUCTION. ALL DIMENSIONS TO TAKE PRECEDENCE OVER SCALED DRAWINGS. DO NOT SCALE THIS DRAWING. NO WORK CAN COMMENCE ON SITE UNTIL FULL DEVELOPMENT APPROVAL IS GRANTED BY THE LOCAL APPROVAL AUTHORITY.

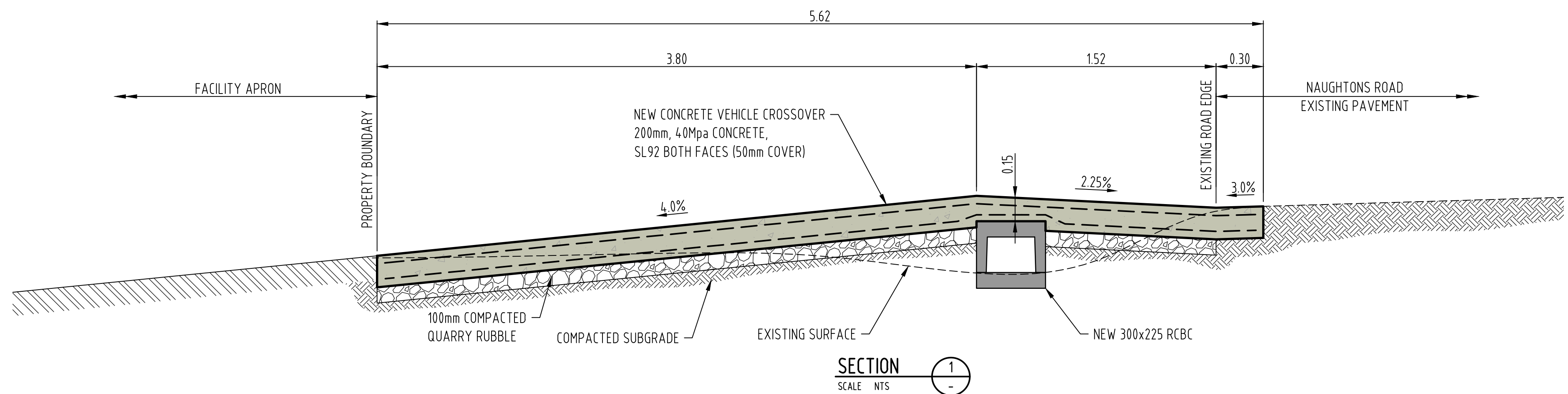


CONCEPT PLAN
SCALE BAR (m)



PM S 19M

meters	
Tractor Width	: 2.50
Trailer Width	: 2.50
Tractor Track	: 2.50
Trailer Track	: 2.50
Lock to Lock Time	: 6.0
Steering Angle	: 27.8
Articulating Angle	: 70.0



FOR APPROVAL
NOT FOR CONSTRUCTION

REV.	DATE	DESCRIPTION	DRAFT	ENG.	CHKD
A	28/04/21	FOR INFORMATION			
B	04/05/21	FOR APPROVAL			

WGA
WALLBRIDGE GILBERT
AZTEC
60 Wyatt Street, Adelaide
South Australia 5000
Telephone 08 8223 7433
Email adelaide@wga.com.au

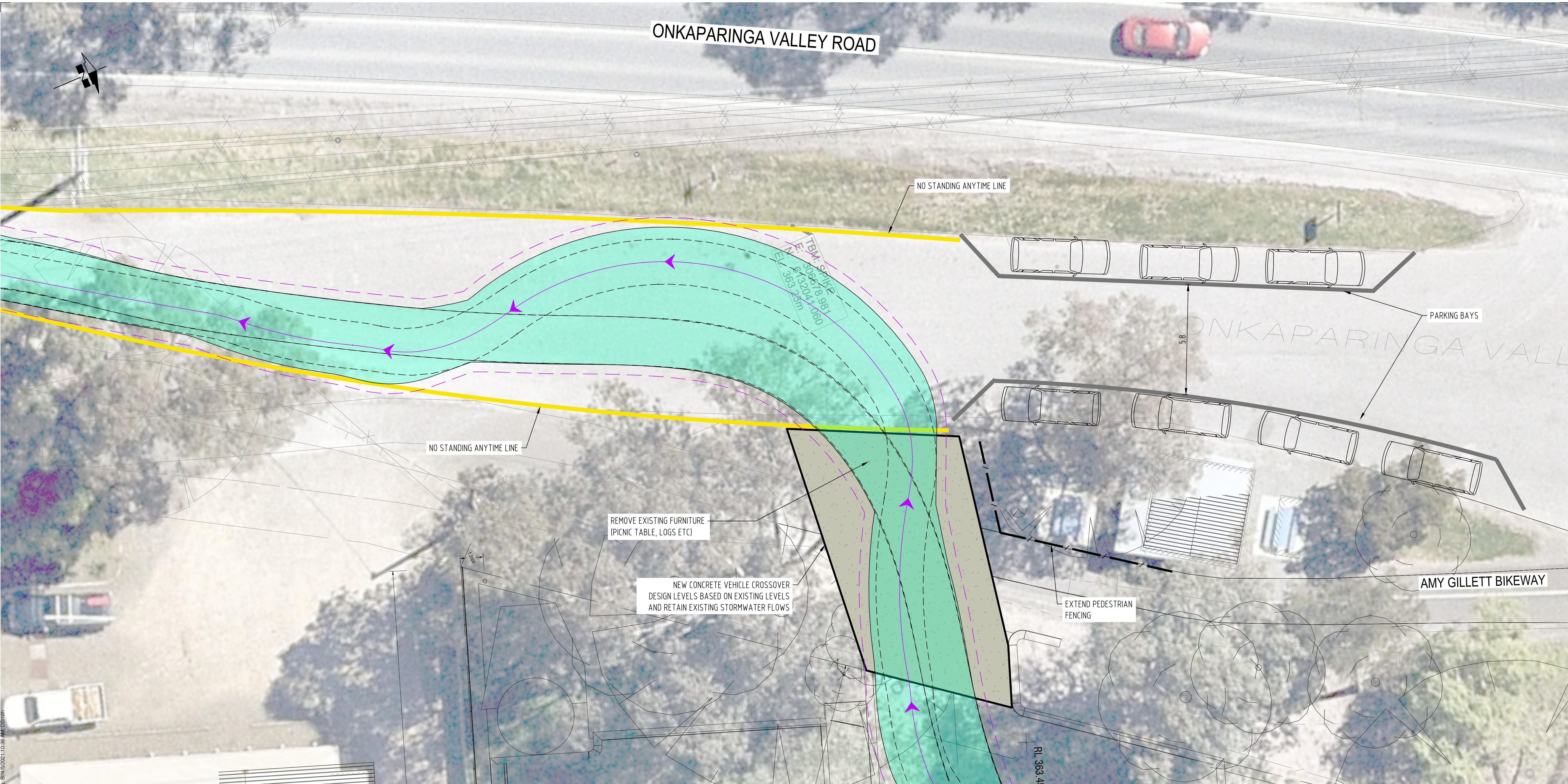
JOYSON FRUIT PACKING FACILITY
WOODSIDE
NAUGHTONS ROAD ACCESS
CONCEPT PLAN - SHEET 1

A1	DOCUMENT NUMBER
Design DS	Drawn DS WGA210420-SK-CC-002
	Sheet No. B

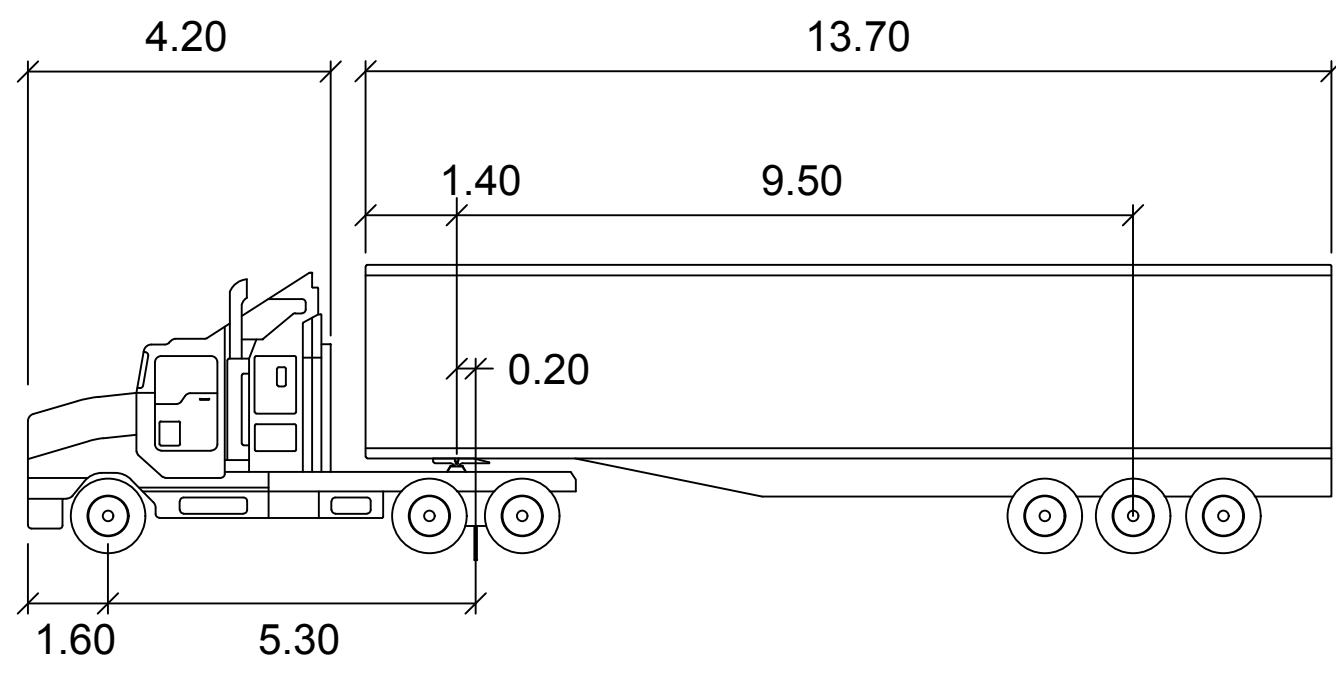
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When sheet printed full size, the scale bar is 100mm.

ADELAIDE HILLS COUNCIL
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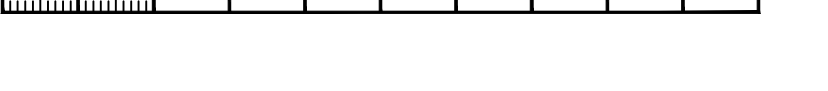
CONCEPT PLAN



PM S 19M

Tractor Width	: 2.50	Lock to Lock Time	: 6.0
Trailer Width	: 2.50	Steering Angle	: 27.8
Tractor Track	: 2.50	Articulating Angle	: 70.0
Trailer Track	: 2.50		

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FOR APPROVAL
NOT FOR CONSTRUCTION

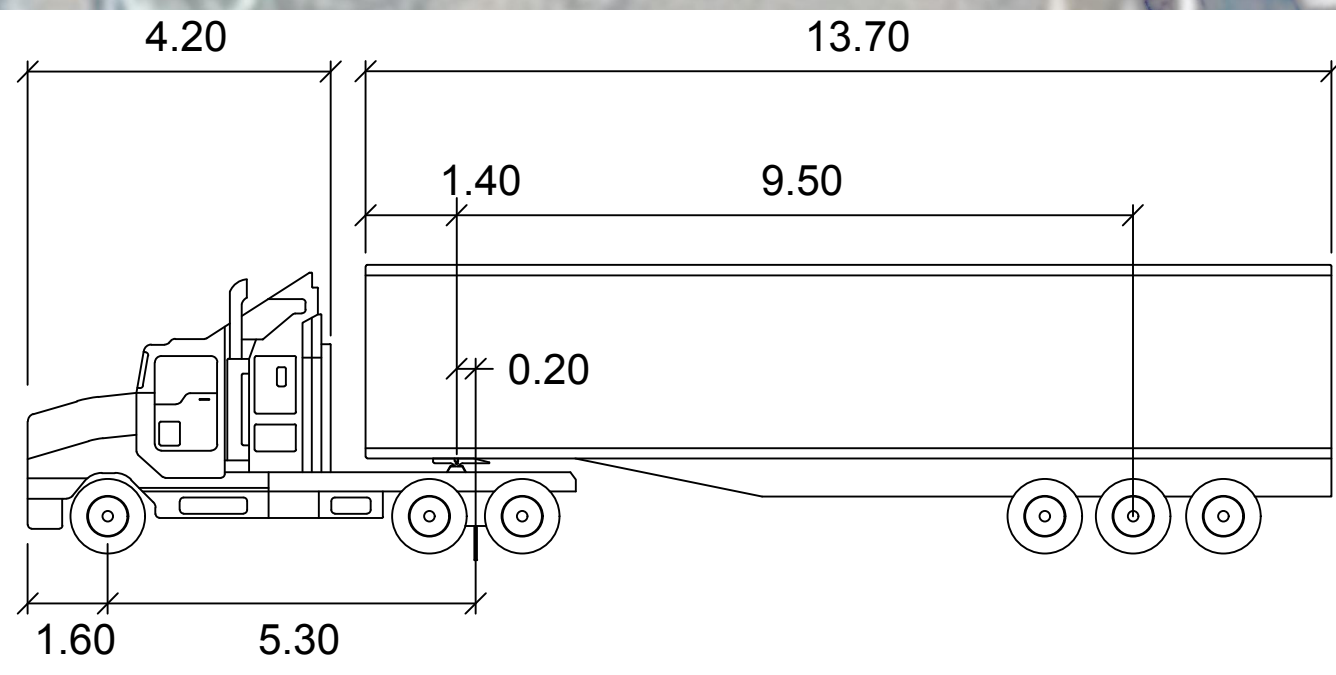
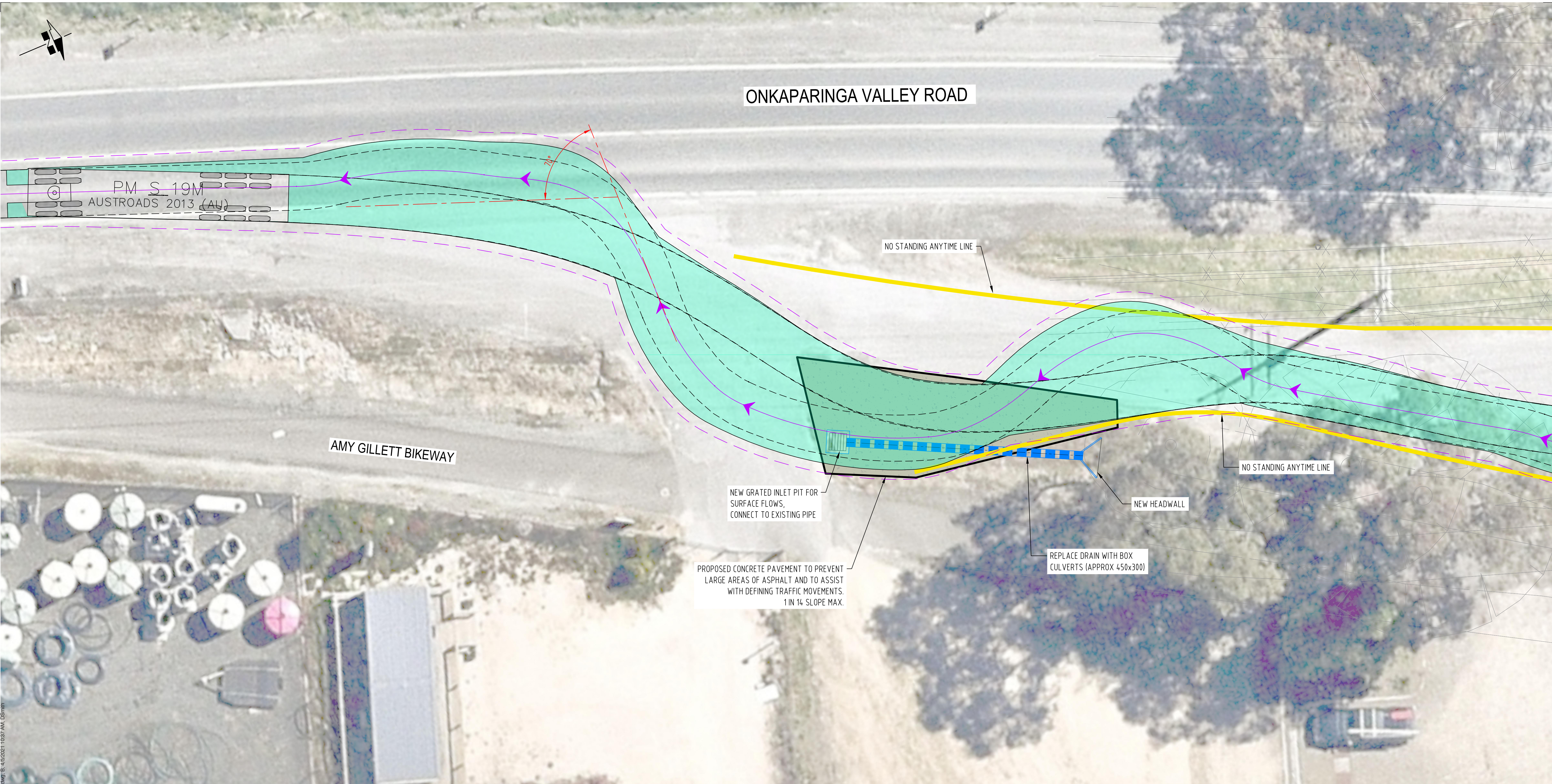
REV.	DATE	DESCRIPTION	DRAFT	ENG.	CHKD
A	28/04/21	FOR INFORMATION			
B	04/05/21	FOR APPROVAL			

WGA
WALLBRIDGE GILBERT
AZTEC
60 Wyatt Street, Adelaide
South Australia 5000
Telephone 08 8223 7433
Email adelaide@wga.com.au

JOYSON FRUIT PACKING FACILITY
WOODSIDE
ONKAPARINGA VALLEY ROAD ACCESS
CONCEPT PLAN - SHEET 2

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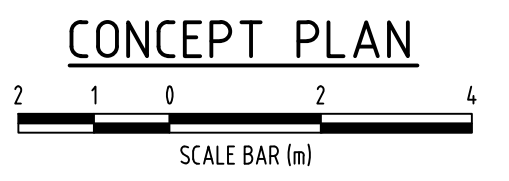
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Trailer Track	: 2.50						

When sheet printed full size, the scale bar is 100mm.



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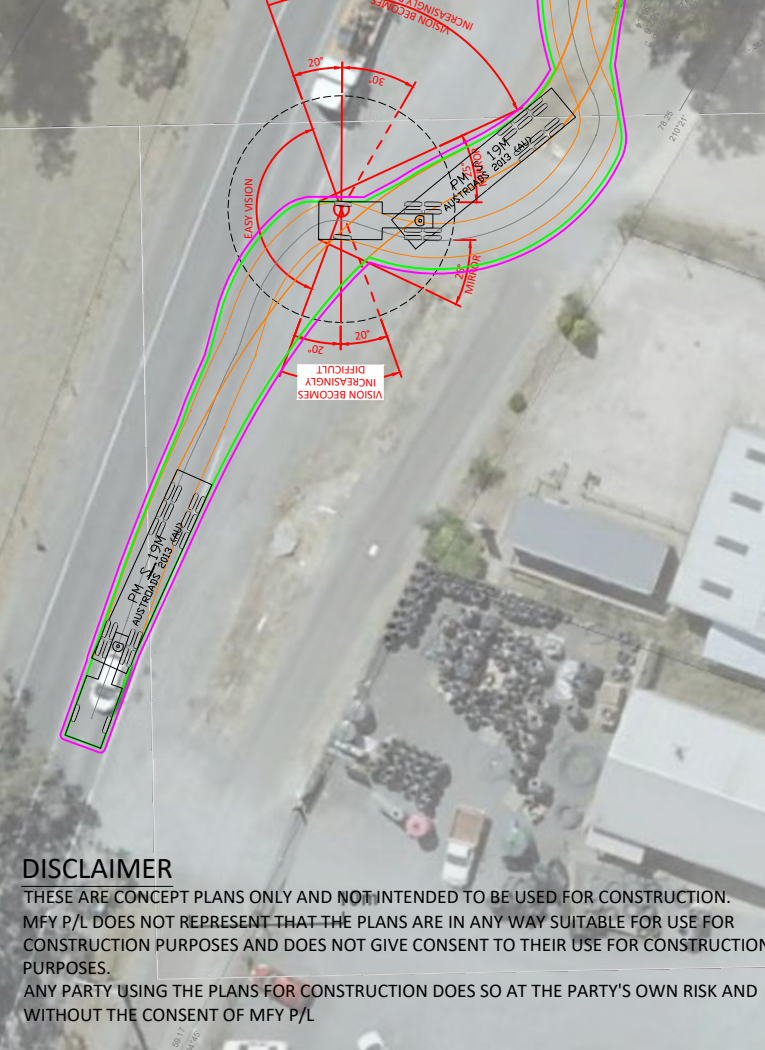
60 Wyatt Street, Adelaide
South Australia 5000
Telephone 08 8223 7433
Email adelaide@wga.com.au

JOYSON FRUIT PACKING FACILITY
WOODSIDE
ONKAPARINGA VALLEY ROAD ACCESS
CONCEPT PLAN - SHEET 3

A1	DOCUMENT NUMBER	Project Number	Sheet No.	Rev.
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198-200 Onkaparinga Valley Road
Woodside
Swept Paths

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Drawing: MFY 200156_02_SH02
 Project Name: 198-200 Onkaparinga Valley Road
 Client: Royce Chads

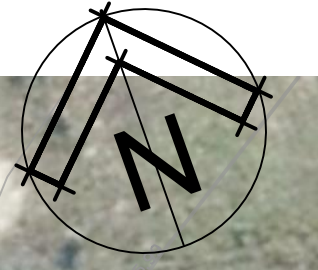
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 Drawn: BH
 Date: 01.09.2020

Revision: B
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Unit 6, 224 Glen Osmond Road
 FULLARTON SA 5063
 T: +61 8 8338 8888
 E: mfy@mfy.com.au

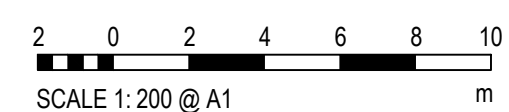
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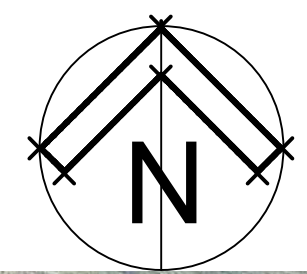
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WOODSIDE APPLE FACILITY
WOODSIDE
TRUCK TURNAROUND

DRAWN RD DESIGNER MR APPROVED HP DWG No. **A2020-10798** SHEET **C02** REV **B**

ISSUE	DATE	DESCRIPTION	DRAWN	DESIGNED	APPROVED
B	16/12/20	ISSUED FOR APPROVAL	RD	MR	HP
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 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - ALL DIMENSIONS SHALL BE VERIFIED ON SITE.
 - ALL LEVELS ARE EXPRESSED IN METRES.
 - THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE RELEVANT SPECIFICATIONS.
 - REFER TO DETAIL DRAWINGS FOR ALL UNDERGROUND PIPEWORK AND DETAILS.
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 - THESE DRAWINGS ARE A SCHEMATIC REPRESENTATION OF SERVICES INFORMATION CONTAINED IN DRAWINGS ISSUED BY THE RELEVANT AUTHORITIES.
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- WHERE INFORMATION IS AVAILABLE, THE SUB-SURFACE SERVICES INSTALLED BY CONTRACTORS OTHER THAN THE AUTHORITIES HAVE BEEN SHOWN, BUT ADDITIONAL UNDOCUMENTED SERVICES MAY BE PRESENT. SHOULD THE CONTRACTOR BELIEVE THAT SUB-SURFACE SERVICES ARE AT RISK OF DAMAGE DURING CONSTRUCTION, THE CONTRACTOR SHOULD NOTIFY THE RELEVANT AUTHORITIES AND ESTABLISH THE EXACT LOCATION OF THE SERVICES.
- THE FINISHED SURFACE SHALL BE EVENLY GRADED BETWEEN DESIGN SURFACE LEVELS.
 - DEMOLISH AND REMOVE ALL EXISTING INSTALLATIONS WHICH ARE TO BE AFFECTED BY NEW WORKS. EXTENT OF DEMOLITION TO BE CONFIRMED ON SITE WITH THE SUPERINTENDENT PRIOR TO WORKS.
 - CONTRACTOR TO ADJUST LIDS OF EXISTING SERVICE PITS TO MATCH FINISHED SURFACE LEVEL. PROVIDE HEAVY DUTY COVER IF IN PAVED AREA TO THE REQUIREMENTS OF THE RELEVANT AUTHORITY, IF APPLICABLE. RELOCATE SERVICE AS REQUIRED.
 - WORKMANSHIP AND MATERIALS ARE TO BE IN ACCORDANCE WITH THE RELEVANT CURRENT S.A.A. CODES INCLUDING ALL AMENDMENTS, AND THE LOCAL STATUTORY AUTHORITIES, EXCEPT WHERE VARIED BY THE CONTRACT DOCUMENTS.

COMPACTION NOTES:

- PRIOR TO THE COMMENCEMENT OF ANY FILLING OPERATION, THE ENTIRE SITE AREA IS TO BE COMPACTED AND TESTED IN ACCORDANCE WITH AS1289 TO PRODUCE 98.0% STANDARD COMPACTION AT THE FINAL EXCAVATED NATURAL SURFACE LEVEL AND AT 250mm BELOW THE EXCAVATED NATURAL SURFACE LEVEL.
- TESTING SHALL BE EVENLY SPACED OVER THE ENTIRE SITE, AND AT RANDOM LOCATIONS. TEST RESULTS SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER.

PAVEMENT NOTES

- ALL SET OUT DIMENSIONS AND LEVELS TO BE CONFIRMED ON SITE PRIOR TO COMMENCEMENT OF THE WORKS.
- REFER TO RELEVANT CIVIL DRAWINGS FOR GRADING AND SERVICES.

COMPACTION NOTES (ALLOTMENTS):

- PRIOR TO THE COMMENCEMENT OF ANY FILLING OPERATION, THE ENTIRE SITE AREA IS TO BE COMPACTED AND TESTED IN ACCORDANCE WITH AS1289 TO PRODUCE 98.0% STANDARD COMPACTION AT THE FINAL EXCAVATED NATURAL SURFACE LEVEL AND AT 250mm BELOW THE EXCAVATED NATURAL SURFACE LEVEL.
- EACH 200mm FILL LAYER SHALL BE COMPACTED AND TESTED IN ACCORDANCE WITH AS1289 TO PRODUCE 98.0% STANDARD.
- THE FINAL FILL LAYER SHALL BE COMPACTED & TESTED IN ACCORDANCE WITH AS1289. 5.1.1 TO PRODUCE 100.0% STANDARD
- TESTING SHALL BE CARRIED AT A RATE TO SATISFY THE REQUIREMENTS OF AS 3798-LEVEL.1 OVERVIEW.
- TESTING SHALL BE EVENLY SPACED OVER THE ENTIRE SITE, AND AT RANDOM LOCATIONS. TEST RESULTS SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER.

EXISTING SERVICES NOTES:

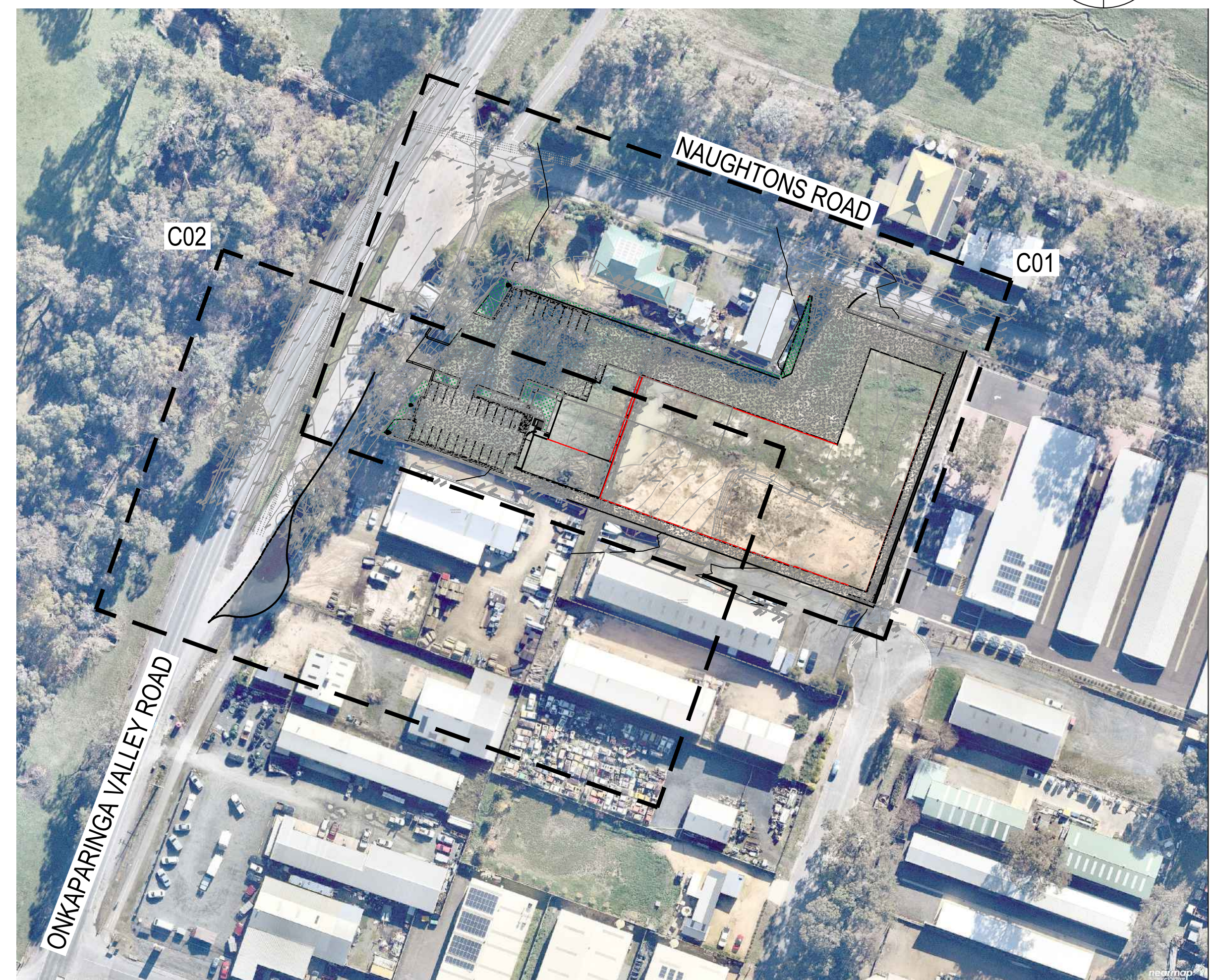
- ALL DRAWINGS AND DOCUMENTS CONTAINED WITHIN THIS PROJECT HAVE LIMITED EXISTING SERVICES SHOWN. OTHER SERVICES MAY EXIST, WHICH WERE NOT KNOWN OR IDENTIFIED AT THE TIME OF DETAIL DOCUMENTATION. THESE UNKNOWN SERVICES MAY POSSIBLY INTERFERE WITH THE PROPOSED WORKS AS SET OUT WITHIN THESE DESIGN DOCUMENTS.
- ALL IDENTIFIED EXISTING SERVICES ARE A SCHEMATIC REPRESENTATION OF THE INFORMATION PROVIDED BY THE VARIOUS SERVICE AUTHORITIES.
- AS SUCH, THE LIMITS OF LIABILITY, ACCURACY OF THE LOCATION OF THE SERVICES, DEPTHS, LEVELS, SIZES, TYPES AS STIPULATED BY THE SERVICE AUTHORITIES IS PASSED ON FROM MLEI TO THE RESPECTIVE CONTRACTOR.
- THE LIMITS REFER TO THE ACCURACY OF THE INFORMATION, AND NO LIABILITY WILL BE ACCEPTED BY THE SERVICE AUTHORITIES, INCLUDING MLEI.
- THE CONTRACTOR MUST FULLY INFORM HIMSELF AS THE NATURE AND EXTENT OF ALL UNDERGROUND SERVICES THAT MAY IMPACT ON THE PROPOSED WORKS.
- ALL SERVICES MUST BE FULLY VERIFIED, AND COMPARED AGAINST THE PROPOSED DESIGN WORKS.
- UNDER NO CIRCUMSTANCES SHALL ANY FIXTURE OR FITTING BE ORDERED AND INSTALLED THAT HAS THE POTENTIAL TO REQUIRE ANY REWORK AS A DIRECT OR INDIRECT RESULT OF FAILURE TO VERIFY EXISTING SERVICES. SHOULD REWORK BE REQUIRED OF ANY NEW FIXTURE OR FITTING AS A RESULT OF THE ABOVE, NO CLAIM AGAINST MLEI OR ITS AGENTS WILL BE CONSIDERED.
- UPON VERIFICATION OF ALL EXISTING UNDERGROUND SERVICES, THE CONTRACTOR SHALL ADVISE THE SUPERINTENDENT OR THEIR NOMINATED REPRESENTATIVE AS SOON AS POSSIBLE, IN THE EVENT OF ANY POTENTIAL CLASH OR INTERFERENCE WITH THE PROPOSED WORKS.
- ALL WORKS DIRECTLY OR INDIRECTLY RELATED TO THE POTENTIAL CLASH / INTERFERENCE SHALL CEASE IMMEDIATELY, AND SHALL NOT RESUME UNTIL SUCH TIME AS INSTRUCTED TO DO SO BY MLEI OR ITS NOMINATED AGENT.
- NO FINANCIAL CLAIMS ARISING FROM THE SUBCONTRACTOR FOR DELAYS WILL BE CONSIDERED BY MLEI OR ITS AGENTS.
- IN THE EVENT THAT ANY CLASH / INTERFERENCE IS BY A SERVICE THAT CAN ONLY BE POTENTIALLY MODIFIED BY THE SERVICE PROVIDER, E.G. SA WATER SERVICES, SAPN TELSTRA OR GAS SUPPLY, THIS WORK SHALL BE COORDINATED BY MLEI OR ITS NOMINATED AGENT. IN THIS CIRCUMSTANCE, CHARGES LEVIED BY THE SERVICE PROVIDER FOR THE MODIFICATION / ALTERATION WILL NOT BE THE RESPONSIBILITY OF THE CONTRACTOR. THIS RELATES ONLY TO THE MODIFICATION WORKS UNDERTAKEN BY THE SERVICE PROVIDER.

STORMWATER NOTES

- SET OUT CHAINAGES AT SIDE ENTRY PITS AND JUNCTION BOXES ADJACENT TO KERBS, REFER TO CENTRE OF PIT AT KERB TOP LINE.
- SET OUT POINTS AT PITS NOT ADJACENT TO KERBS, REFER TO CENTRE OF PIT.
- PIT INVERT LEVELS REFER TO CENTRE OF PITS WITH PIPE GRADES CALCULATED BETWEEN THESE POINTS.
- PIT DESIGN SURFACE LEVELS REFER TO
 - SEP - TOP OF CENTRE OF PIT COVER
 - JB - TOP OF CENTRE OF PIT COVER
 - GIP - TOP OF CENTRE OF GRATE
- THE CONTRACTOR SHALL ENSURE ALL SEP AND JB COVERS AND FRAMES MATCH FINISHED SURFACE GRADE/SLOPE AND LEVEL.
- ALL PIT COVERS ARE TO BE CLASS D TO AS3996 UNLESS OTHERWISE NOTED.
- CONCRETE PIPES TO BE CLASS 2 UNLESS OTHERWISE SHOWN.
- ALL STORMWATER PIPES ARE TO BE RUBBER RING JOINTED IF USING RCP UNLESS NOTED OTHERWISE.
- BANDAGE JOINTS TO BE PROVIDED ON PIPES WHICH CHANGE HORIZONTAL DIRECTION IF USING RCP.

LEGEND

- SURFACE GRADE AND DRAINAGE DIRECTION. MIN. 1.0% UNO.
- DESIGN SPOT LEVEL
- EXISTING SPOT LEVEL
- FINISHED FLOOR LEVEL
- INDICATIVE 150mmØ DOWN PIPE (REFER ARCHITECTURAL DRAWING FOR SPECIFICATIONS)
- INSPECTION POINT.
- 450 SQ (UNO) GRATED INLET PIT
- 600 SQ JUNCTION BOX
- EXISTING SWALE
- BIORETENTION SWALE - REFER DETAIL
- 375mmØ REINFORCED CONCRETE PIPE.
- 150mmØ (UNO) uPVC STORMWATER PIPE.
- UPPER FLOOR OR ROOF OUTLINE
- DRIVEWAY PAVING IN ACCORDANCE WITH AUTHORITY REQUIREMENTS (REFER ARCHITECTURAL DRAWING FOR SPECIFICATIONS)
- CONCRETE PAVING (REFER ARCHITECTURAL DRAWING FOR SPECIFICATIONS)
- LANDSCAPE/GARDEN AREA
- RETAINING WALL: TW: TOP OF RETAINING WALL, BW: BOTTOM OF RETAINING WALL, H: HEIGHT (m)
- RETAINING WALL
- 300 WIDE SPOON DRAIN
- HEADWALL OUTLET
- BATTER SLOPE
- PROPOSED 450X300 REINFORCED CONCRETE BOXED CULVERT CROSSING
- CHECKER PLATE DRAIN



LAYOUT PLAN
SCALE: 1:1000

INDEX

- C00 COVER SHEET, INDEX AND GENERAL NOTES
- C01 SITE WORKS AND STORMWATER MANAGEMENT PLAN
- C02 TRUCK TURNAROUND

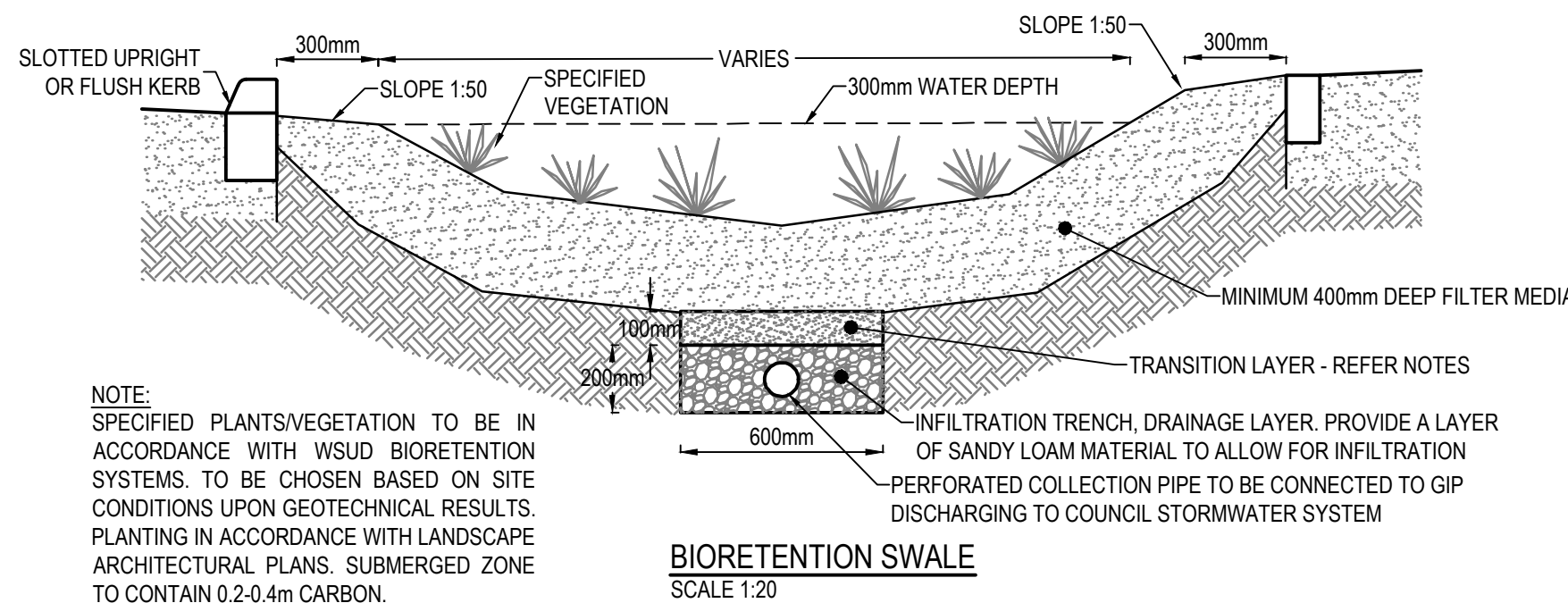
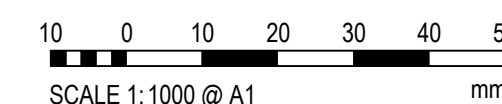


Table 1: Specification for Filter Media

Soil component type	Particle size (mm)	Component (%)
Clay and Silt	< 0.05	< 3
Very fine Sand	0.05 - 0.15	5 - 30
Fine Sand	0.15 - 0.25	10 - 30
Medium to Coarse Sand	0.25 - 1.0	40 - 60
Coarse Sand	1.0 - 2.0	7 - 10
Fine Gravel	2.0 - 3.4	< 3
Soil Properties AS4419 - 2003 Soils for Landscaping and Garden Use		
Organic Matter Content	< 5%	
pH	As specified for natural soils and soil blends. 5.5 - 7.5 (pH 1.5 in water)	
Electrical Conductivity (EC)	As specified for natural soils and soil blends < 2.2 dS/m.	
Phosphorus	< 100 mg/kg. (Note: should be < 20 mg/kg for phosphorus sensitive plants).	

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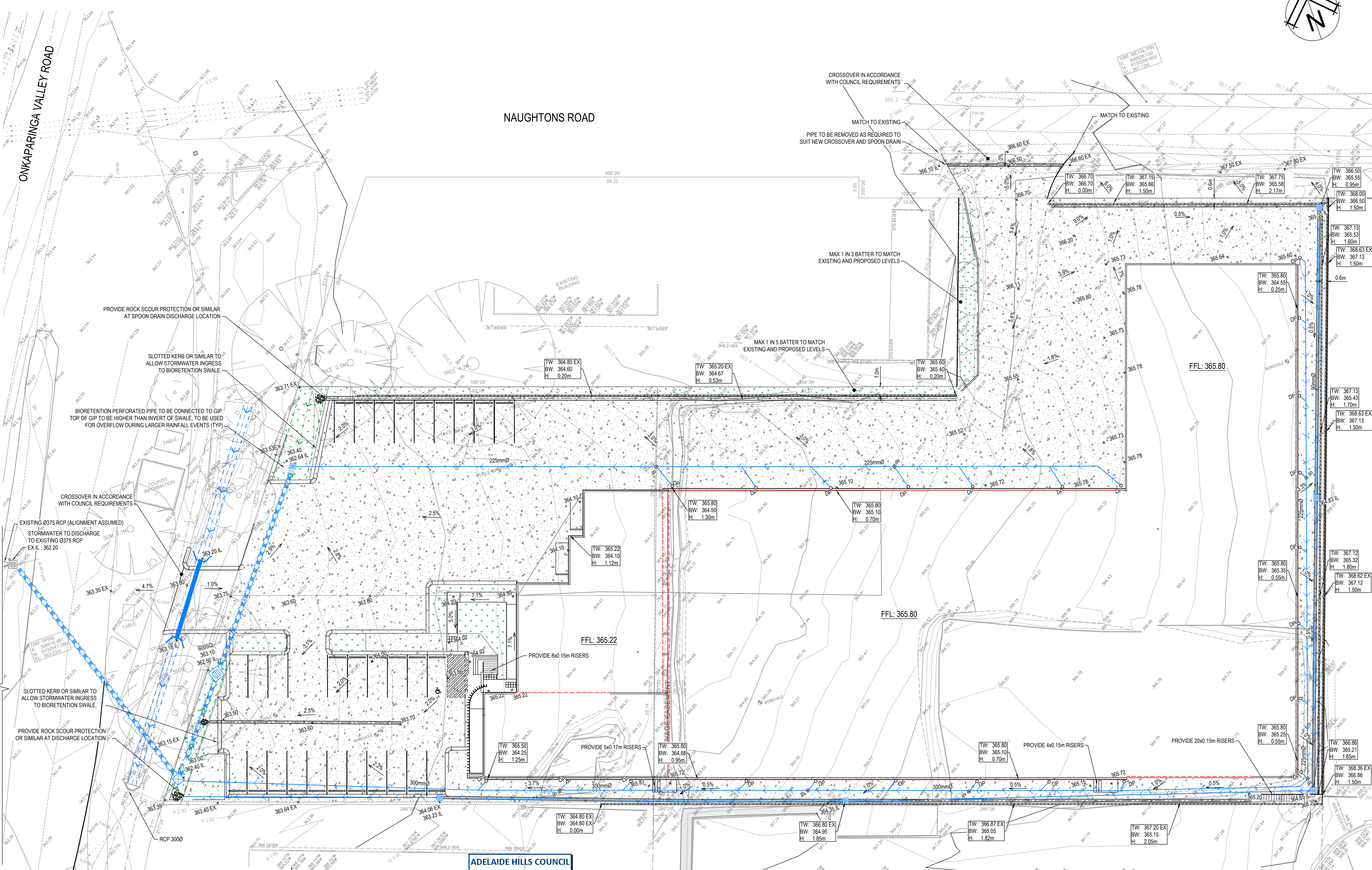
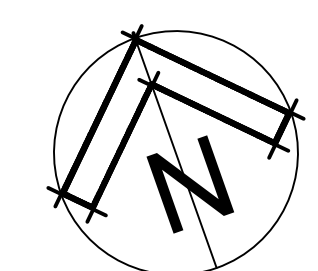
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WOODSIDE APPLE FACILITY
COVER SHEET, INDEX AND GENERAL NOTES
SITE WORKS & STORMWATER MANAGEMENT PLAN

DRAWN RD DESIGNER MR APPROVED HP DWG No. A2020-10798 SHEET C00 REV D

ISSUE	DATE	DESCRIPTION	DRAWN	DESIGNED	APPROVED
D	02/03/21	ISSUED FOR APPROVAL	RD	MR	HP
C	18/02/21	ISSUED FOR APPROVAL	RD	MR	HP
B	16/12/20	ISSUED FOR APPROVAL	RD	MR	HP
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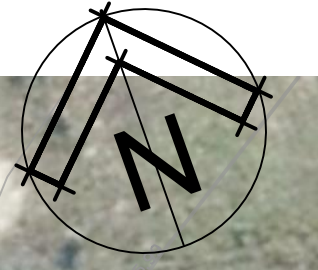
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B	16/12/20	ISSUED FOR APPROVAL	RD	MR	HP
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SITE WORKS & STORMWATER MANAGEMENT PLAN			
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mlei
CONSULTING ENGINEERS
432 Pathway Street, Adelaide SA 5000 Ph (08) 4231 2632 www.mlei.com.au

WOODSIDE APPLE FACILITY
WOODSIDE
TRUCK TURNAROUND

DRAWN RD	DESIGNER MR	APPROVED HP	DWG No. A2020-10798	SHEET C02	REV B
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BUILDINGS

LAND DIVISION

INFRASTRUCTURE

RESOURCES &
INDUSTRIAL

DEFENCE

Stormwater Management Report

Woodside Apple Facility

Reference A2020 – 10798_A

18 February 2021

ABN 15 638 302 913

Head Office 452 Pulteney Street
Adelaide, South Australia 5000
08) 8231 2832
mlei@mlei.com.au

Contents

Contents	0
1. Introduction	1
2. Site Description.....	1
2.1 Site Characteristics.....	1
2.2 Existing Stormwater Infrastructure	2
2.3 Existing Site Drainage	3
3. Proposed Development.....	4
4. Stormwater Management Strategy.....	4
5. Stormwater Capture and Disposal	5
6. Proposed Stormwater Drainage Approach.....	6
6.1 Overall management Approach	6
6.2 Flood Protection	6
6.5 Stormwater Quality – MUSIC modelling.....	6
7. Summary	8
8. Appendix A – Existing Site Plan	9
9. Appendix B – Concept Plan of the Proposed Development.....	10
10. Appendix C – Stormwater Calculations.....	11
11. Appendix D – Concept Stormwater Management Plan	12
12. Appendix E – MUSIC Audit Report.....	13
13. Appendix F – Catchment Analysis	14

1. Introduction

MLEI have been engaged to prepare this report and develop a preliminary stormwater management plan (SMP) for the proposed Woodside Apple Facility development at Naughtons Road, Woodside. This report gives recommendations for the control and management of stormwater runoff from the proposed development and provides a preliminary SMP produced for planning purposes.

The site in its current state is quite steep, with a depression in the centre of the allotment, the overall fall of the land is to the West, toward Onkaparinga Valley Road. The key criterion is the maintenance of stormwater discharge off site to Council's drainage infrastructure. The report encompasses conceptual ideas on the management of stormwater on the development. The design of individual elements on site such as the detailed stormwater collection conveyance systems are not within the scope of this management plan and will be covered within the detailed design in the development stage of the project.

The final stormwater drainage layout for the proposed development will be subject to final design and the site/architectural layout. Hence this report provides an overall strategy and demonstrates that the fundamental stormwater management requirements can be met. This service has been provided on an independent and professional basis. You can be assured that it seeks to present a factual, unbiased and balanced assessment.

2. Site Description

2.1 Site Characteristics

The development site is bounded by the Naughtons Road to the North, Onkaparinga Valley Road to the West, with two entrances proposed from both roads. An aerial site plan is shown in Figure 1.

The overall development site consists of an existing allotment on Naughtons Road merging with 2 existing allotments facing Onkaparinga Valley Road, to combine for a total of approximately 7,600 square metres. The site is undeveloped in its current state and is located within a Light Industry zone.



Figure 1 - Site aerial view

2.2 Existing Stormwater Infrastructure

Currently the catchment north-east of Naughtons Road travels along a shallow formed swale on the southern side of Naughtons Road, then crossing to the northern side of the road and entering a stormwater crossing under Onkaparinga Valley Road to discharge to Onkaparinga River. Council has indicated that this system is over capacity and the proposed development is not to discharge stormwater to this network.

Along Onkaparinga Valley Road in front of the site exists an unformed drainage channel with several driveway culvert crossings, discharging to a stormwater crossing approximately 100m south of the development.

An existing grated inlet pit with a 375mm diameter outlet is located in the road reserve/parking area in front of the proposed development, refer figure 2 for the approximate location. The upstream pipe is unknown but the outlet appears to cross Onkaparinga Valley Road.



Figure 2 - Stormwater Pit Location

2.3 Existing Site Drainage

As shown in Appendix A, the existing topography is overall quite steep and falls toward Onkaparinga Valley Road. No formed on-site stormwater detention devices or rainwater tanks have been observed on site within the proposed development, an existing depression appears to hold water on the site.

3. Proposed Development

A concept plan of the proposed development can be found in Appendix B. The development consists of a large warehouse for commercial purposes with parking and road layout to suit the requirements.

4. Stormwater Management Strategy

The objectives of the stormwater management plan is to determine the runoff generated from the site and the capacity of the downstream network to discharge stormwater from the site without increasing any flooding issues. The proposed stormwater system aims to achieve stormwater management practices to the satisfaction of Adelaide Hills Council's stormwater management objectives.

The SMP will follow the below points:

- Stormwater runoff from site to discharge directly to the existing stormwater infrastructure with consideration of the direct catchment contributing to the grated inlet pit for a minor 5% Annual Exceedance Probability (AEP) storm event.
- Stormwater Management Plan (SMP) will demonstrate the above and to ensure discharge points do not have any adverse impact on adjoining properties, Council infrastructure or the environment.
- Stormwater quality treatment following best practice stormwater quality reduction targets

The volumes are computed by using DRAINS modelling. The calculations are based on the Horton/ILSAX method using the 1 in 20 year (5% AEP) peak storm to determine the runoff of stormwater from the site.

Within the development the primary focus for the management of stormwater is the application of standard stormwater management practices to the satisfaction of Council. For the purposes of this report we have adopted the following standard practices:

- Design of surface and roof drainage systems to a 5% AEP capacity.
- The provision of onsite treatment systems to treat surface runoff.

The SMP endeavours to address these parameters.

5. Stormwater Capture and Disposal

The calculations are based on the preliminary modelling, further design and analysis will be necessary to determine the exact location and sizes of the pits and pipes in the network, to be undertaken in detailed design.

A catchment analysis was undertaken to determine the change in peak flow as a result of the development, found in Appendix F, the results found a negligible increase in change of the peak flow hydrograph when compared to pre-development conditions. Discussions with Council have deemed it appropriate to discharge to the existing outlet pipe in the reserve to the full capacity of the pipe with no flow restriction, providing nearby directly contributing catchments are factored in the flow.

Calculations for the site discharge and nearby catchment contributing to the outlet system can be found in Appendix C. Peak flow from the site for the minor (5% AEP) event is approximately 152L/s, with 61L/s expected from the nearby road reserve catchment. As shown in the calculations, results indicate no overland flow is expected and hence the expected capacity of the drainage network is sufficient to handle these flows.

6. Proposed Stormwater Drainage Approach

6.1 Overall management Approach

Runoff of impervious pavement surface is proposed to be collected using a network of pits and pipes which are designed to a 1 in 20 year storm event (5% AEP) design standard. Provision to accommodate the major storm event up to the 1 in 100 year (1% AEP) peak flow through the site will be facilitated through overland flow paths and roadways that will direct flow away from the site. Where there are trapped low points within the site, such systems are designed to cater for the 1 in 100 year ARI event with an allowance for 50% blocking factor to the pits and pipes.

It is proposed that the runoff from the roof discharges to outlet location via downpipes. Surface runoff from the site is proposed to ultimately discharge to the bio-retention swales for treatment of frequent rainfall events, in larger storm events the bio-retention swales will fill to the height of the sump inlet to then discharge to the provided outlet.

Refer to Appendix D for concept stormwater management plan.

6.2 Flood Protection

Council flood mapping has not been available at the area, correspondence with Council indicates there are existing ponding and flooding issues around the site, finished floor levels have been selected to avoid inundation of the building, 1.80m higher than the existing front property boundary levels.

6.5 Stormwater Quality – MUSIC modelling

Prior to entering Council's drainage network bio-filtration areas have been sized to cater to ensure surface runoff is treated to meet stormwater quality targets.

MLEI are providing MUSIC data with target requirements identified by best practice stormwater quality reduction targets (BPSQRT's) and the satisfaction of Council. MLEI have used industry best practices in the assessment. It is industry practice that the 3-month ARI storm event be used for stormwater quality modelling, this is widely accepted as the catchment is considered sufficiently clean after a rain event of this nature.

The following target reduction requirements were outlined by the BPSQRT and Council:

- 90% gross pollutants
- 80% total suspended solids (TSS)
- 60% total phosphorus (TP)
- 45% total nitrogen (TN)

For the purpose of this planning submission, the following catchment impervious fractions were adopted:

- 90% fraction impervious for hardstand areas.
- 10% fraction impervious for landscape area.

The following reduction in pollutants were achieved with the inclusion of the proposed treatment train as shown in Figure 3.

Pollutant type	BPSQRT & Council Reduction Requirement	Treatment train effectiveness
Gross pollutants	90%	100%
Total suspended solids	80%	98.2%
Total phosphorus	60%	67.5%
Total nitrogen	45%	75.4%

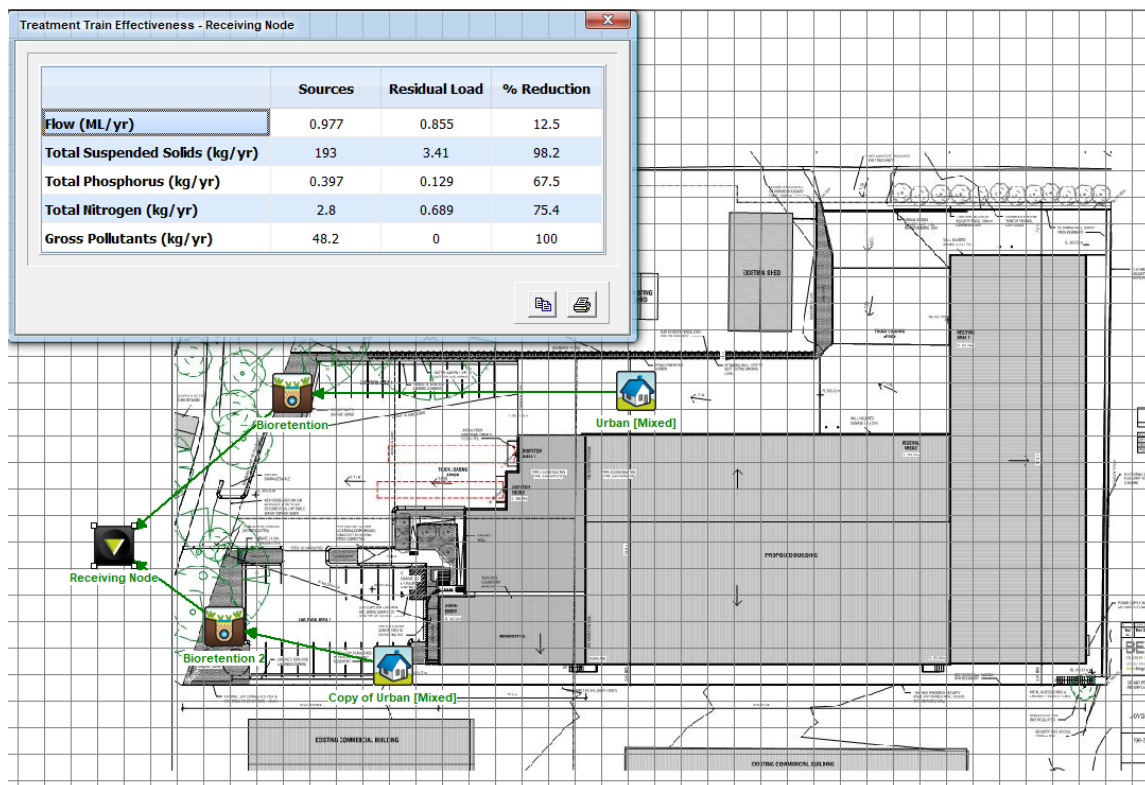


Figure 3 – MUSIC Treatment train

The achieved reductions are a betterment than the requirements outlined by the by best practice stormwater quality reduction targets and in the opinion of MLEI is an acceptable solution for this development. Music Audit Summary Report can be found in Appendix E, at the time of writing this report MLEI did not have access to recent catchment analysis but reviewed the audit and noted the rainfall discrepancies appeared minor.

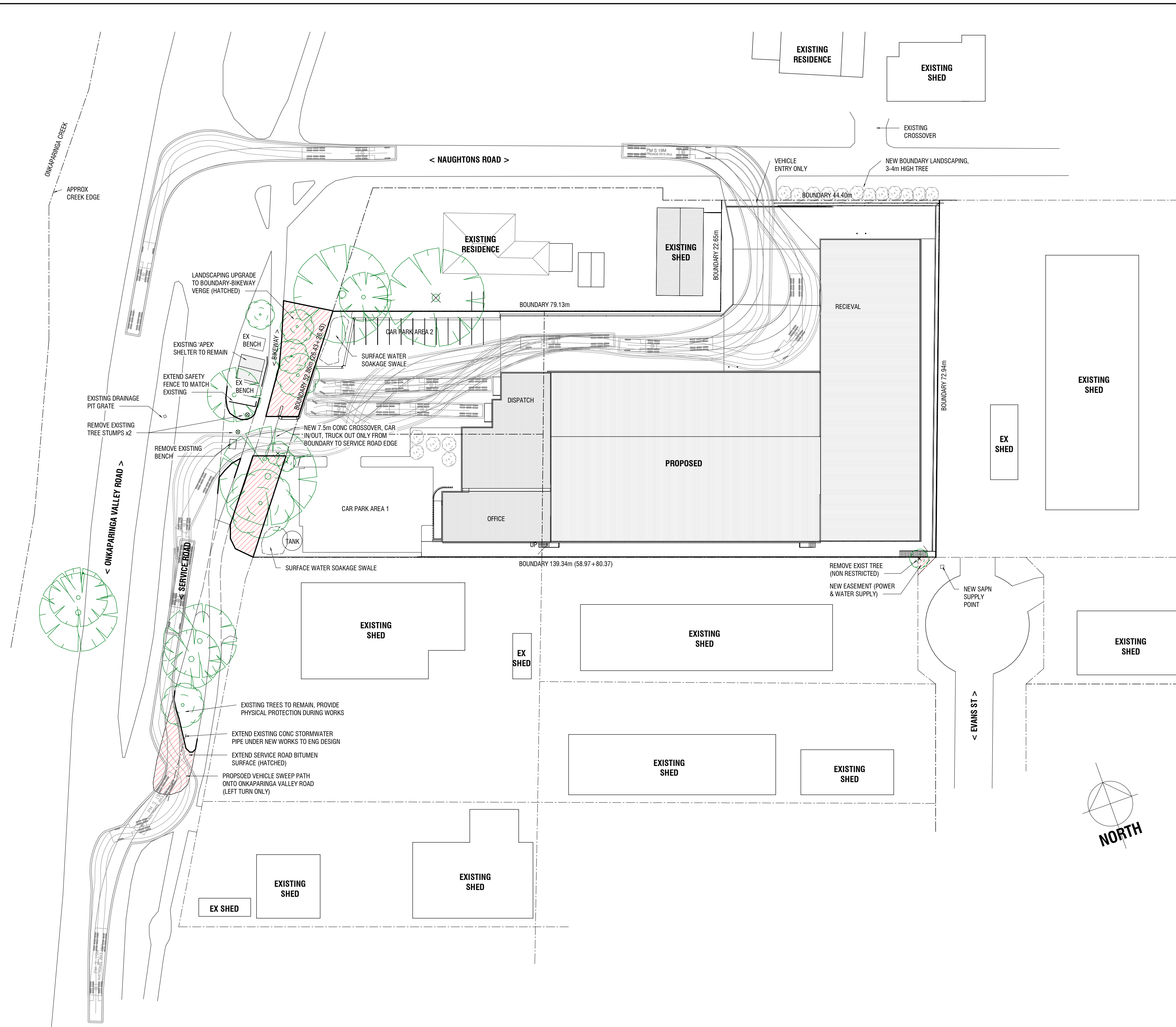
7. Summary

The post-development flow of the site is intended to directly discharge to the existing drainage network as it was found that the full capacity of the pipe is sufficient to manage the flows of events up to and including the 5% AEP storm event. The design requirements outlined in sections 4 and 6 have been adhered to. Stormwater quality treatment targets have been shown to be achieved.

8. Appendix A – Existing Site Plan



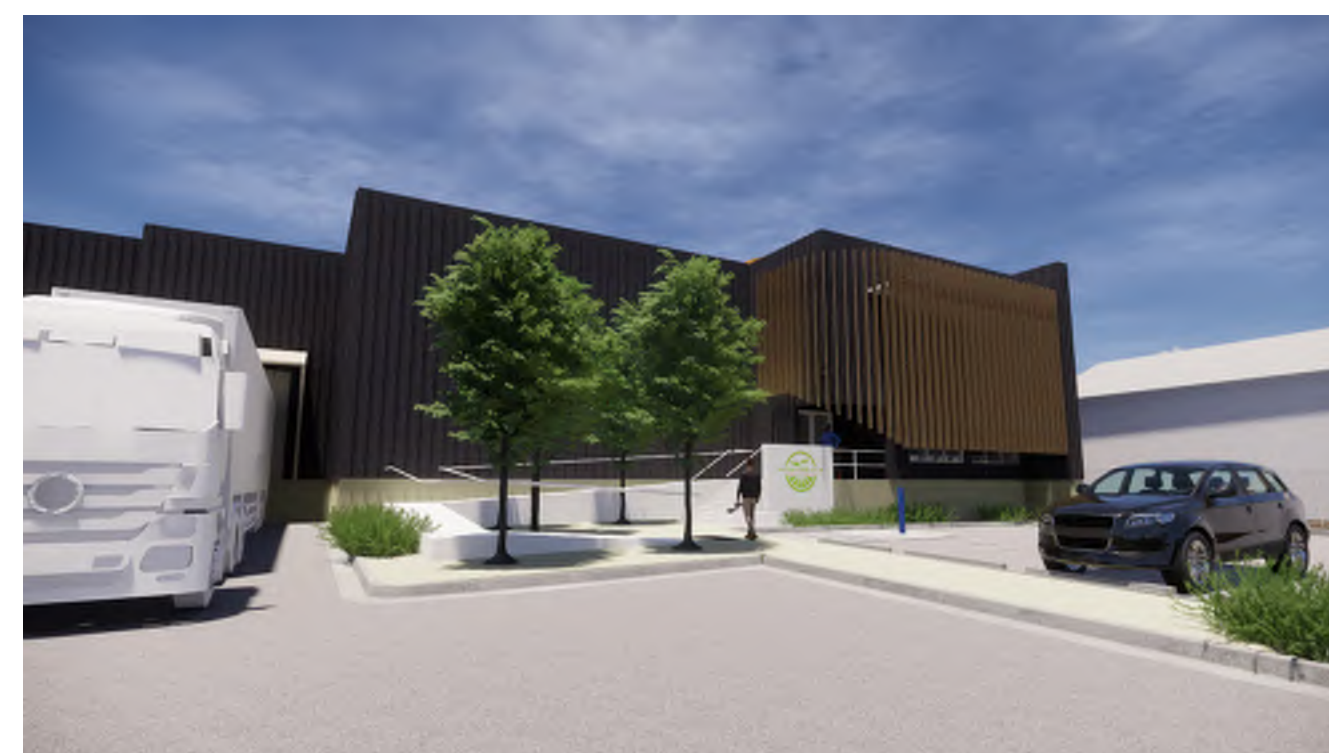
9. Appendix B – Concept Plan of the Proposed Development



SITE PLAN EXTENDED
1 : 500

JOYSON ORCHARDS ADELAIDE HILLS FRESH FRUIT PACKING FACILITY

198-200 ONKAPARKING VALLEY ROAD, WOODSIDE SA 5244



Rev No	Rev Date	Revision Description
A	24/11/20	Issued for Council planning discussion.

BEYONDINK
DESIGN | COUNCIL | BUILD
52A Main Street, Woodside SA 5244 T 8388 1173
www.beyondink.com.au ABN 36 960 210 275

3D AND PERSPECTIVE DRAWINGS MAY BE INACCURATE OR INCOMPLETE. THEY ARE CONSIDERED TO BE INDICATIVE.

**JOYSON ORCHARDS, ADELAIDE HILLS
FRESH, FRUIT PACKING FACILITY**

198-200 Onkaparinga Valley Road, WOODSIDE SA

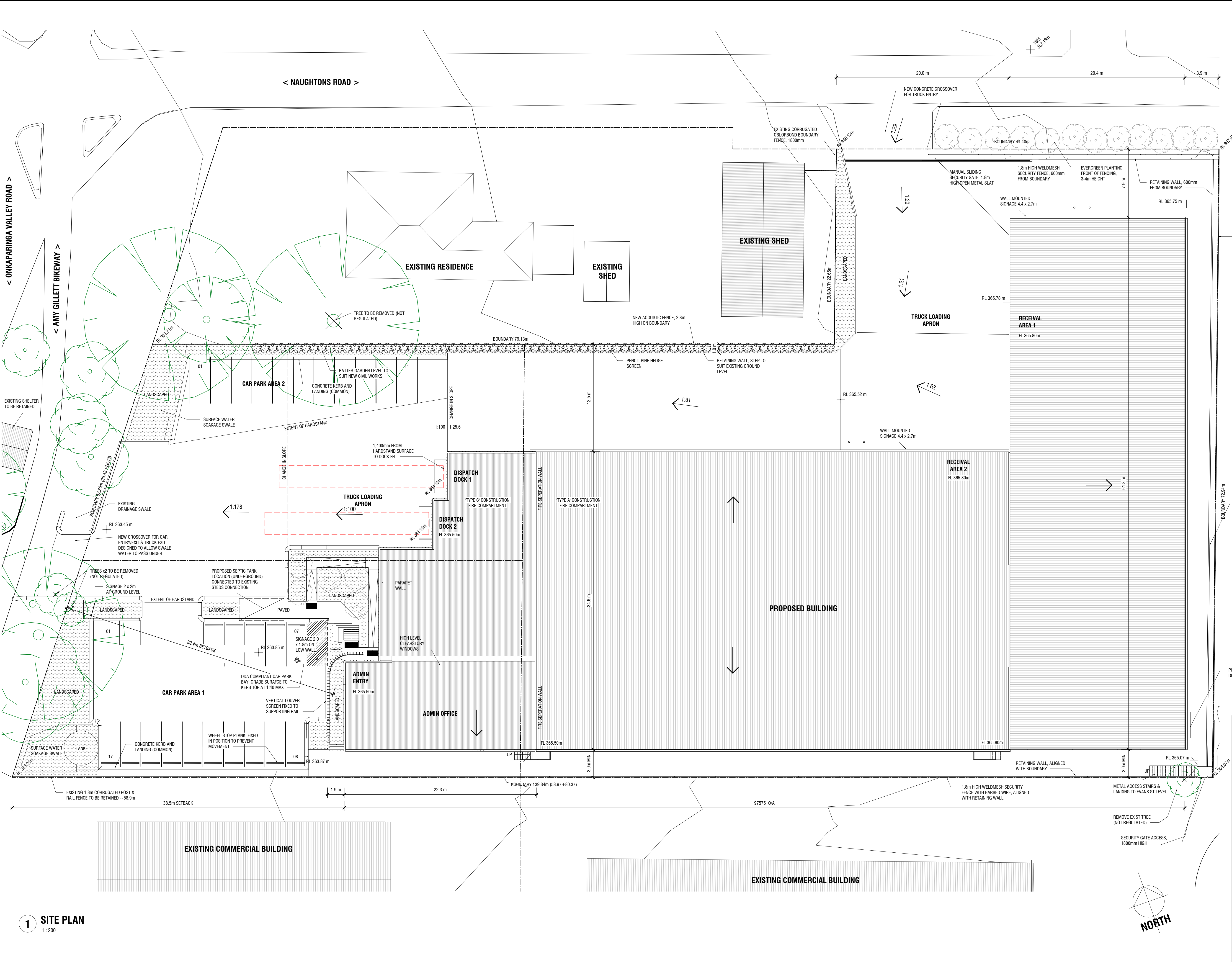
COVER SHEET & SITE PLAN EXT

PRELIMINARY	Revision A
--------------------	-------------------

Drawn AH
Date 25/11/2020
Scale @A1 size : 1 : 500

Sheet Number
PD000

ABBREVIATIONS	
ABBREV.	DESCRIPTION



Areas Schedule		
Name	Area	Level
ALLOTMENT (All Titles)	3888.60 m ²	FL-0 (N)
OFFICE (1)	233.51 m ²	FL-1 (N)
OFFICE (G)	239.05 m ²	FL-0 (N)
PROCESSING	3488.72 m ²	FL-0 (N)

Rev No	Rev Date	Revision Description
A	24/11/20	Issued for Council planning discussion.

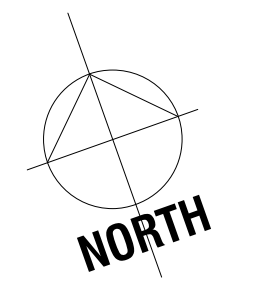
BEYONDINK
 DESIGN | COUNCIL | BUILD
 52A Main Street, Hawthorn SA 5245 T 8388 1179
 www.beyondink.com.au ABN 36 960 210 275

3D AND PERSPECTIVE DRAWINGS MAY BE INACCURATE OR INCOMPLETE. THEY ARE CONSIDERED TO BE INDICATIVE.

JOYSON ORCHARDS, ADELAIDE HILLS FRESH, FRUIT PACKING FACILITY
 198-200 Onkaparinga Valley Road, WOODSIDE SA

SITE PLAN
PRELIMINARY Revision **A**
 Drawn AH Sheet Number
 Date 25/11/2020 **PD001**
 Scale @A1 size : 1 : 200

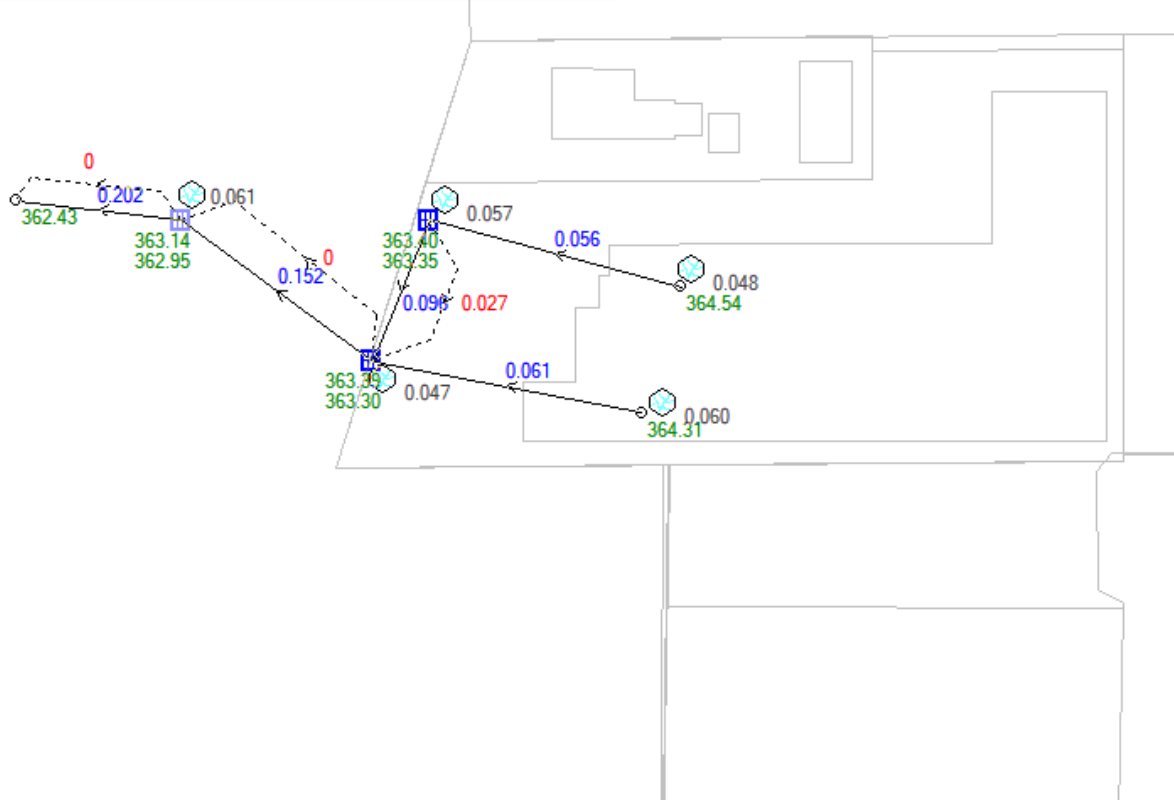
1 SITE PLAN
 1 : 200



10. Appendix C – Stormwater Calculations

Minor Result

Results for median storm in critical 5% AEP ensembles using Lite hydraulic model.



CATCHMENT PLAN

Post-development

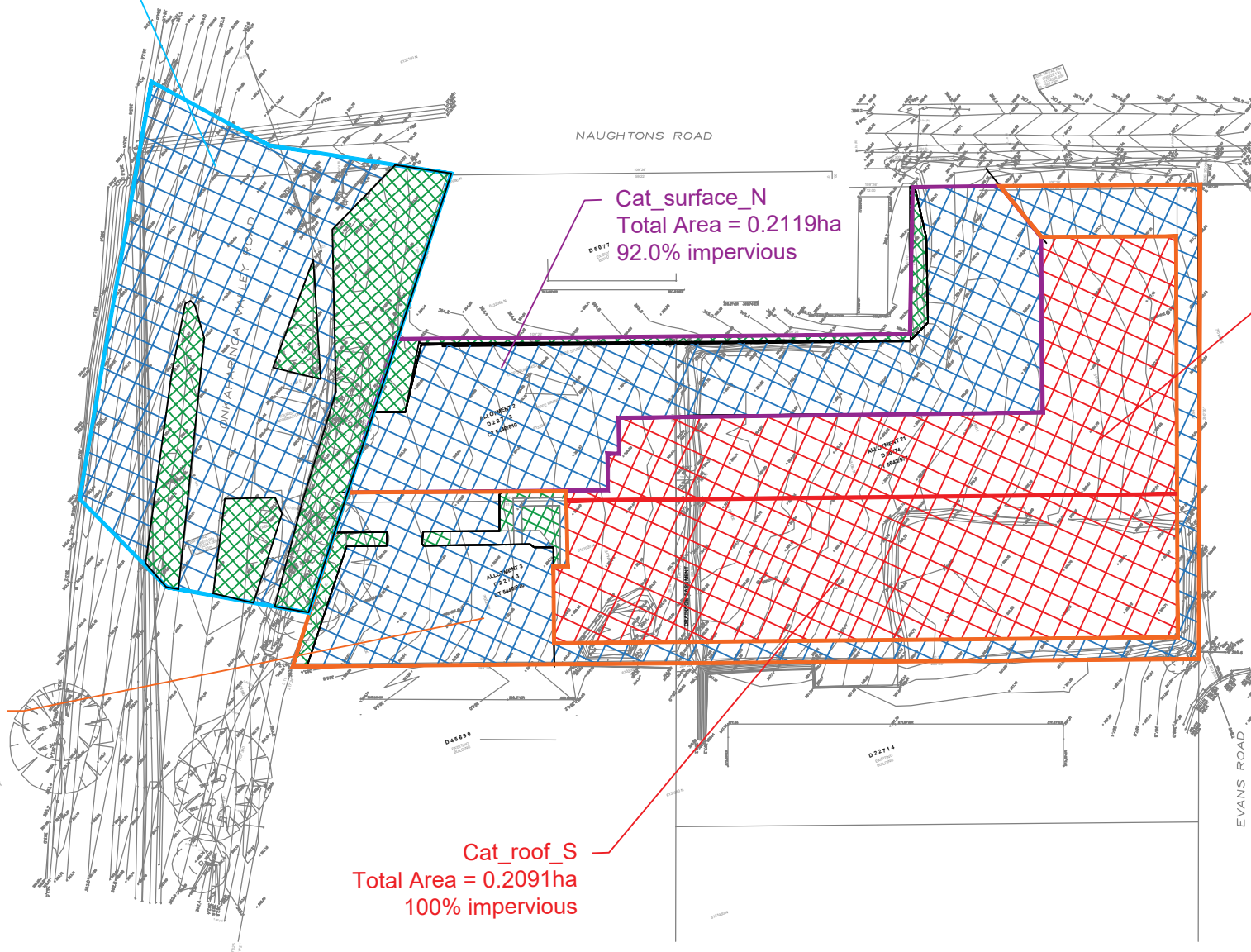
Cat_surface_S
Total Area = 0.2965ha
66.5% impervious

Cat_surface_N
Total Area = 0.2119ha
92.0% impervious

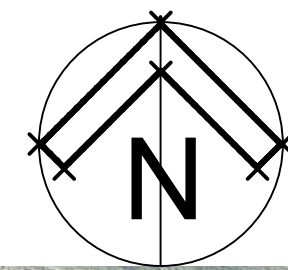
Cat_roof_N
Total Area = 0.1678ha
100% impervious

Cat_surface_S
Total Area = 0.1739ha
92.5% impervious

Cat_roof_S
Total Area = 0.2091ha
100% impervious



11. Appendix D – Concept Stormwater Management Plan



GENERAL NOTES

- THESE DRAWINGS ARE NOT CADASTRAL PLANS AND MUST NOT BE USED IN DETERMINING PRECISE DETAILS WITH RESPECT TO BOUNDARIES.
 - ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
 - ALL DIMENSIONS SHALL BE VERIFIED ON SITE.
 - ALL LEVELS ARE EXPRESSED IN METRES.
 - THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE RELEVANT SPECIFICATIONS.
 - REFER TO DETAIL DRAWINGS FOR ALL UNDERGROUND PIPEWORK AND DETAILS.
 - SPOIL TO BE STOCKPILED AS DIRECTED BY THE SUPERINTENDENT AND EXCESS NOT USED IS TO BE REMOVED FROM SITE BY CONTRACTOR.
 - THESE DRAWINGS ARE A SCHEMATIC REPRESENTATION OF SERVICES INFORMATION CONTAINED IN DRAWINGS ISSUED BY THE RELEVANT AUTHORITIES.
- THE INFORMATION CONTAINED IN THESE DRAWINGS IS INDICATIVE ONLY, AND REFERENCE SHOULD BE MADE TO THE RELEVANT AUTHORITIES DOCUMENTATION TO CONFIRM ACCURACY AND COMPLETENESS.
- WHERE INFORMATION IS AVAILABLE, THE SUB-SURFACE SERVICES INSTALLED BY CONTRACTORS OTHER THAN THE AUTHORITIES HAVE BEEN SHOWN, BUT ADDITIONAL UNDOCUMENTED SERVICES MAY BE PRESENT. SHOULD THE CONTRACTOR BELIEVE THAT SUB-SURFACE SERVICES ARE AT RISK OF DAMAGE DURING CONSTRUCTION, THE CONTRACTOR SHOULD NOTIFY THE RELEVANT AUTHORITIES AND ESTABLISH THE EXACT LOCATION OF THE SERVICES.
- THE FINISHED SURFACE SHALL BE EVENLY GRADED BETWEEN DESIGN SURFACE LEVELS.
 - DEMOLISH AND REMOVE ALL EXISTING INSTALLATIONS WHICH ARE TO BE AFFECTED BY NEW WORKS. EXTENT OF DEMOLITION TO BE CONFIRMED ON SITE WITH THE SUPERINTENDENT PRIOR TO WORKS.
 - CONTRACTOR TO ADJUST LIDS OF EXISTING SERVICE PITS TO MATCH FINISHED SURFACE LEVEL. PROVIDE HEAVY DUTY COVER IF IN PAVED AREA TO THE REQUIREMENTS OF THE RELEVANT AUTHORITY, IF APPLICABLE. RELOCATE SERVICE AS REQUIRED.
 - WORKMANSHIP AND MATERIALS ARE TO BE IN ACCORDANCE WITH THE RELEVANT CURRENT S.A.A. CODES INCLUDING ALL AMENDMENTS, AND THE LOCAL STATUTORY AUTHORITIES, EXCEPT WHERE VARIED BY THE CONTRACT DOCUMENTS.

COMPACTION NOTES:

- PRIOR TO THE COMMENCEMENT OF ANY FILLING OPERATION, THE ENTIRE SITE AREA IS TO BE COMPACTED AND TESTED IN ACCORDANCE WITH AS1289 TO PRODUCE 98.0% STANDARD COMPACTION AT THE FINAL EXCAVATED NATURAL SURFACE LEVEL AND AT 250mm BELOW THE EXCAVATED NATURAL SURFACE LEVEL.
- TESTING SHALL BE EVENLY SPACED OVER THE ENTIRE SITE, AND AT RANDOM LOCATIONS. TEST RESULTS SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER.

PAVEMENT NOTES

- ALL SET OUT DIMENSIONS AND LEVELS TO BE CONFIRMED ON SITE PRIOR TO COMMENCEMENT OF THE WORKS.
- REFER TO RELEVANT CIVIL DRAWINGS FOR GRADING AND SERVICES.

COMPACTION NOTES (ALLOTMENTS):

- PRIOR TO THE COMMENCEMENT OF ANY FILLING OPERATION, THE ENTIRE SITE AREA IS TO BE COMPACTED AND TESTED IN ACCORDANCE WITH AS1289 TO PRODUCE 98.0% STANDARD COMPACTION AT THE FINAL EXCAVATED NATURAL SURFACE LEVEL AND AT 250mm BELOW THE EXCAVATED NATURAL SURFACE LEVEL.
- EACH 200mm FILL LAYER SHALL BE COMPACTED AND TESTED IN ACCORDANCE WITH AS1289 TO PRODUCE 98.0% STANDARD.
- THE FINAL FILL LAYER SHALL BE COMPACTED & TESTED IN ACCORDANCE WITH AS1289. 5.1.1 TO PRODUCE 100.0% STANDARD
- TESTING SHALL BE CARRIED AT A RATE TO SATISFY THE REQUIREMENTS OF AS 3798-LEVEL.1 OVERVIEW.
- TESTING SHALL BE EVENLY SPACED OVER THE ENTIRE SITE, AND AT RANDOM LOCATIONS. TEST RESULTS SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER.

EXISTING SERVICES NOTES:

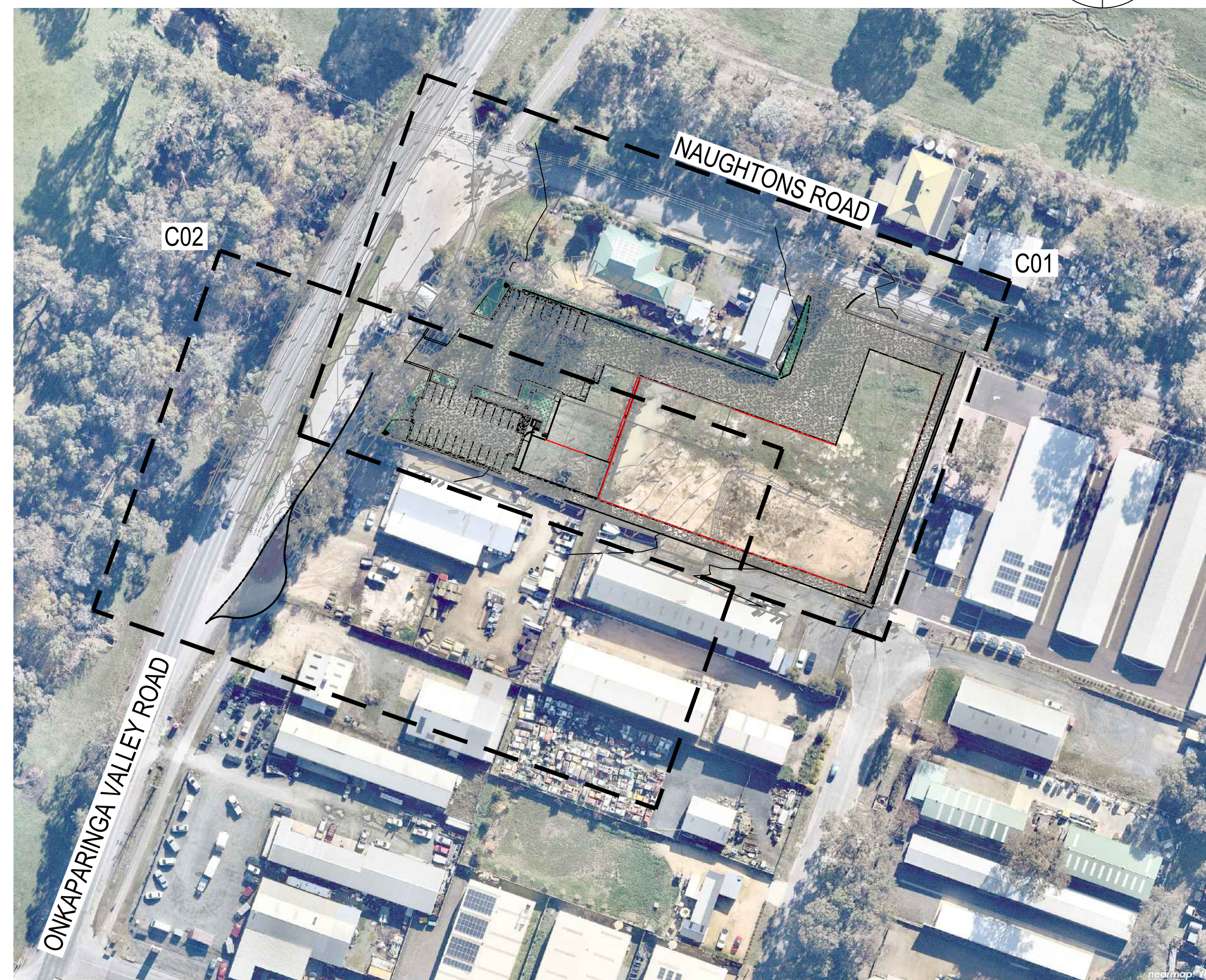
- ALL DRAWINGS AND DOCUMENTS CONTAINED WITHIN THIS PROJECT HAVE LIMITED EXISTING SERVICES SHOWN. OTHER SERVICES MAY EXIST, WHICH WERE NOT KNOWN OR IDENTIFIED AT THE TIME OF DETAIL DOCUMENTATION. THESE UNKNOWN SERVICES MAY POSSIBLY INTERFERE WITH THE PROPOSED WORKS AS SET OUT WITHIN THESE DESIGN DOCUMENTS.
- ALL IDENTIFIED EXISTING SERVICES ARE A SCHEMATIC REPRESENTATION OF THE INFORMATION PROVIDED BY THE VARIOUS SERVICE AUTHORITIES.
- AS SUCH, THE LIMITS OF LIABILITY, ACCURACY OF THE LOCATION OF THE SERVICES, DEPTHS, LEVELS, SIZES, TYPES AS STIPULATED BY THE SERVICE AUTHORITIES IS PASSED ON FROM MLEI TO THE RESPECTIVE CONTRACTOR.
- THE LIMITS REFER TO THE ACCURACY OF THE INFORMATION, AND NO LIABILITY WILL BE ACCEPTED BY THE SERVICE AUTHORITIES, INCLUDING MLEI.
- THE CONTRACTOR MUST FULLY INFORM HIMSELF AS THE NATURE AND EXTENT OF ALL UNDERGROUND SERVICES THAT MAY IMPACT ON THE PROPOSED WORKS.
- ALL SERVICES MUST BE FULLY VERIFIED, AND COMPARED AGAINST THE PROPOSED DESIGN WORKS.
- UNDER NO CIRCUMSTANCES SHALL ANY FIXTURE OR FITTING BE ORDERED AND INSTALLED THAT HAS THE POTENTIAL TO REQUIRE ANY REWORK AS A DIRECT OR INDIRECT RESULT OF FAILURE TO VERIFY EXISTING SERVICES. SHOULD REWORK BE REQUIRED OF ANY NEW FIXTURE OR FITTING AS A RESULT OF THE ABOVE, NO CLAIM AGAINST MLEI OR ITS AGENTS WILL BE CONSIDERED.
- UPON VERIFICATION OF ALL EXISTING UNDERGROUND SERVICES, THE CONTRACTOR SHALL ADVISE THE SUPERINTENDENT OR THEIR NOMINATED REPRESENTATIVE AS SOON AS POSSIBLE, IN THE EVENT OF ANY POTENTIAL CLASH OR INTERFERENCE WITH THE PROPOSED WORKS.
- ALL WORKS DIRECTLY OR INDIRECTLY RELATED TO THE POTENTIAL CLASH / INTERFERENCE SHALL CEASE IMMEDIATELY, AND SHALL NOT RESUME UNTIL SUCH TIME AS INSTRUCTED TO DO SO BY MLEI OR ITS NOMINATED AGENT.
- NO FINANCIAL CLAIMS ARISING FROM THE SUBCONTRACTOR FOR DELAYS WILL BE CONSIDERED BY MLEI OR ITS AGENTS.
- IN THE EVENT THAT ANY CLASH / INTERFERENCE IS BY A SERVICE THAT CAN ONLY BE POTENTIALLY MODIFIED BY THE SERVICE PROVIDER, E.G. SA WATER SERVICES, SAPN TELSTRA OR GAS SUPPLY, THIS WORK SHALL BE COORDINATED BY MLEI OR ITS NOMINATED AGENT. IN THIS CIRCUMSTANCE, CHARGES LEVIED BY THE SERVICE PROVIDER FOR THE MODIFICATION / ALTERATION WILL NOT BE THE RESPONSIBILITY OF THE CONTRACTOR. THIS RELATES ONLY TO THE MODIFICATION WORKS UNDERTAKEN BY THE SERVICE PROVIDER.

STORMWATER NOTES

- SET OUT CHAINAGES AT SIDE ENTRY PITS AND JUNCTION BOXES ADJACENT TO KERBS, REFER TO CENTRE OF PIT AT KERB TOP LINE.
- SET OUT POINTS AT PITS NOT ADJACENT TO KERBS, REFER TO CENTRE OF PIT.
- PIT INVERT LEVELS REFER TO CENTRE OF PITS WITH PIPE GRADES CALCULATED BETWEEN THESE POINTS.
- PIT DESIGN SURFACE LEVELS REFER TO
 - SEP - TOP OF CENTRE OF PIT COVER
 - JB - TOP OF CENTRE OF PIT COVER
 - GIP - TOP OF CENTRE OF GRATE
- THE CONTRACTOR SHALL ENSURE ALL SEP AND JB COVERS AND FRAMES MATCH FINISHED SURFACE GRADE/SLOPE AND LEVEL.
- ALL PIT COVERS ARE TO BE CLASS D TO AS3996 UNLESS OTHERWISE NOTED.
- CONCRETE PIPES TO BE CLASS 2 UNLESS OTHERWISE SHOWN.
- ALL STORMWATER PIPES ARE TO BE RUBBER RING JOINTED IF USING RCP UNLESS NOTED OTHERWISE.
- BANDAGE JOINTS TO BE PROVIDED ON PIPES WHICH CHANGE HORIZONTAL DIRECTION IF USING RCP.

LEGEND

	SURFACE GRADE AND DRAINAGE DIRECTION. MIN. 1.0% UNO.
	DESIGN SPOT LEVEL
	EXISTING SPOT LEVEL
	FINISHED FLOOR LEVEL
	INDICATIVE 150mmØ DOWN PIPE (REFER ARCHITECTURAL DRAWING FOR SPECIFICATIONS)
	INSPECTION POINT.
	450 SQ (UNO) GRATED INLET PIT
	EXISTING SWALE
	BIORETENTION SWALE - REFER DETAIL
	375mmØ REINFORCED CONCRETE PIPE.
	150mmØ (UNO) uPVC STORMWATER PIPE.
	UPPER FLOOR OR ROOF OUTLINE
	DRIVEWAY PAVING IN ACCORDANCE WITH AUTHORITY REQUIREMENTS (REFER ARCHITECTURAL DRAWING FOR SPECIFICATIONS)
	CONCRETE PAVING (REFER ARCHITECTURAL DRAWING FOR SPECIFICATIONS)
	LANDSCAPE/GARDEN AREA
	RETAINING WALL: TW: TOP OF RETAINING WALL BW: BOTTOM OF RETAINING WALL H: HEIGHT (m)
	RETAINING WALL
	300 WIDE SPOON DRAIN
	HEADWALL OUTLET
	BATTER SLOPE
	PROPOSED 450X300 REINFORCED CONCRETE BOXED CULVERT CROSSING
	CHECKER PLATE DRAIN



LAYOUT PLAN
SCALE: 1:1000

INDEX

- | | |
|-----|---|
| C00 | COVER SHEET, INDEX AND GENERAL NOTES |
| C01 | SITE WORKS AND STORMWATER MANAGEMENT PLAN |
| C02 | TRUCK TURNAROUND |

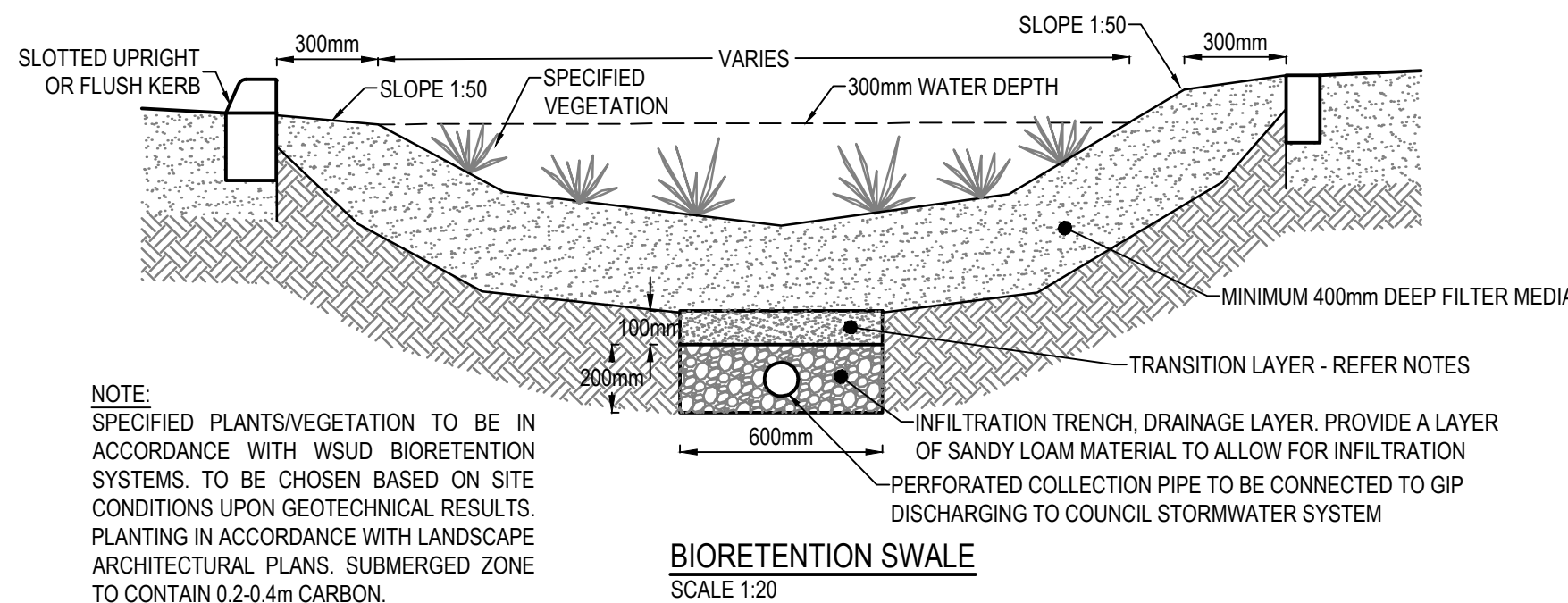
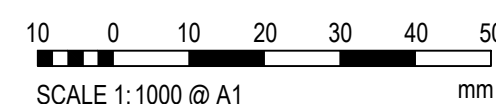


Table 1: Specification for Filter Media

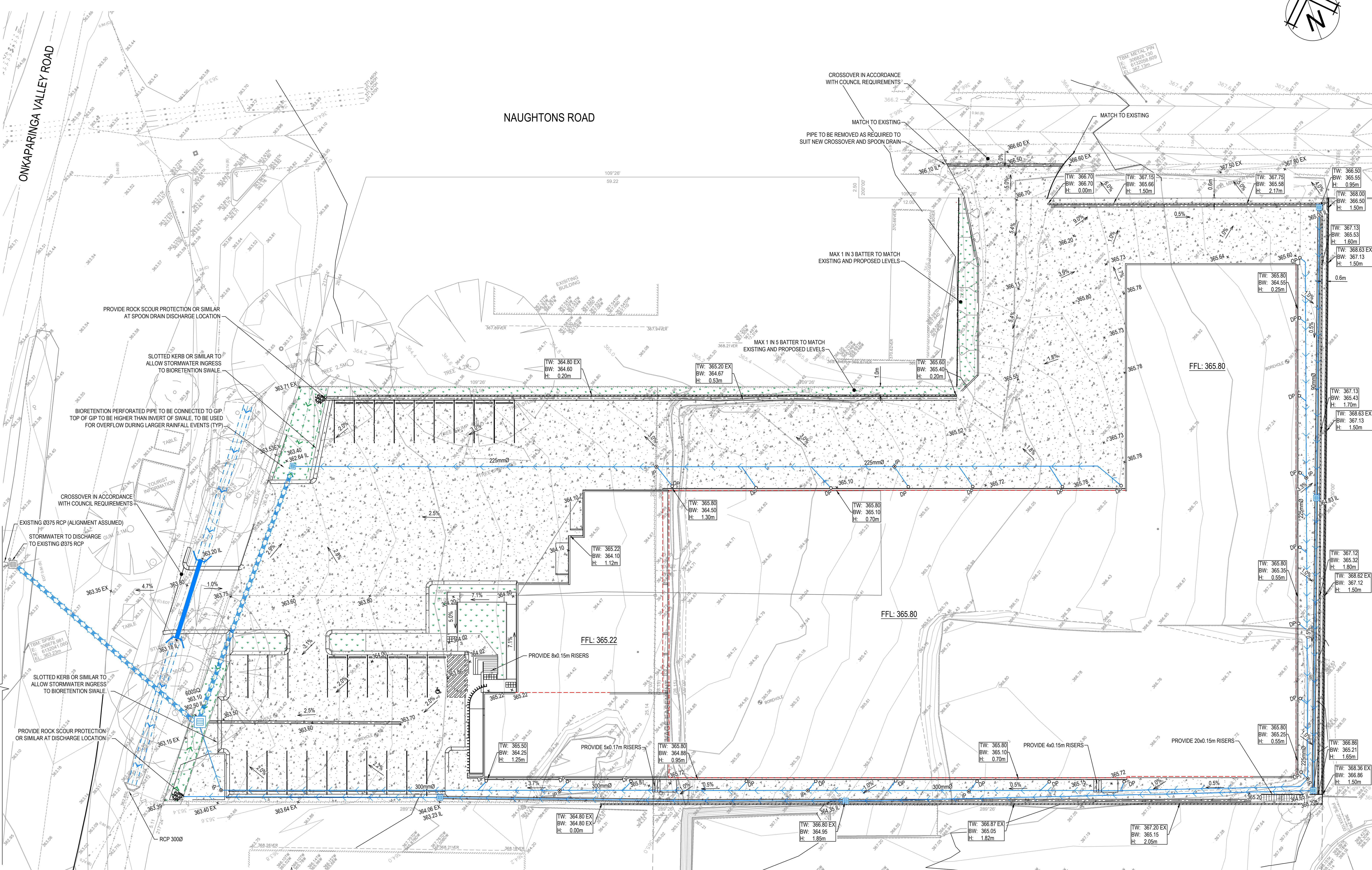
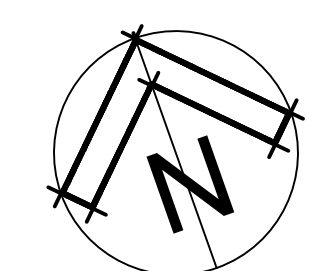
Soil component type	Particle size (mm)	Component (%)
Clay and Silt	< 0.05	< 3
Very fine Sand	0.05 - 0.15	5 - 30
Fine Sand	0.15 - 0.25	10 - 30
Medium to Coarse Sand	0.25 - 1.0	40 - 60
Coarse Sand	1.0 - 2.0	7 - 10
Fine Gravel	2.0 - 3.4	< 3
Soil Properties AS4419 - 2003 Soils for Landscaping and Garden Use		
Organic Matter Content	< 5%	
pH	As specified for natural soils and soil blends. 5.5 - 7.5 (pH 1.5 in water)	
Electrical Conductivity (EC)	As specified for natural soils and soil blends < 2.2 dS/m.	
Phosphorus	< 100 mg/kg. (Note: should be < 20 mg/kg for phosphorus sensitive plants).	



ISSUED FOR APPROVAL
NOT FOR CONSTRUCTION

ISSUE	DATE	DESCRIPTION	DRAWN	DESIGNED	APPROVED
C	18/02/21	ISSUED FOR APPROVAL	RD	MR	HP
B	16/12/20	ISSUED FOR APPROVAL	RD	MR	HP
A	28/10/20	ISSUED FOR APPROVAL	RD	MR	HP

CLIENT: BEYOND INK	 452 Pathway Street, Adelaide SA 5000 Ph (08) 4231 2632 www.mlei.com.au	WOODSIDE APPLE FACILITY COVER SHEET, INDEX AND GENERAL NOTES SITE WORKS & STORMWATER MANAGEMENT PLAN			
		DRAWN: RD DESIGNER: MR APPROVED: HP DWG No: A2020-10798 SHEET: C00 REV: C			



FOR CONTINUATION REFER C02

FOR CONTINUATION REFER C02



D	09/02/21	ISSUED FOR APPROVAL	RD	MR	HP
C	22/01/21	ISSUED FOR APPROVAL	RD	MR	HP
B	16/12/20	ISSUED FOR APPROVAL	RD	MR	HP
A	28/10/20	ISSUED FOR APPROVAL	RD	MR	HP
ISSUE	DATE	DESCRIPTION	DRAWN	DESIGNED	APPROVED

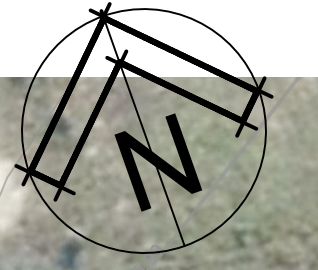
ISSUED FOR APPROVAL
NOT FOR CONSTRUCTION

CLIENT BEYOND INK



WOODSIDE APPLE FACILITY
WOODSIDE
SITE WORKS & STORMWATER MANAGEMENT PLAN

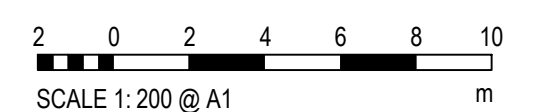
DRAWN RD DESIGNER MR APPROVED HP DWG No: A2020-10798 SHEET C01 REV D



FOR CONTINUATION REFER C01



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A	28/10/20	ISSUED FOR APPROVAL	RD	MR	HP
ISSUE	DATE	DESCRIPTION	DRAWN	DESIGNED	APPROVED



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CLIENT BEYOND INK

mlei
CONSULTING ENGINEERS
402 Pathway Street, Adelaide SA 5000 Ph (08) 6231 2832 www.mlei.com.au

WOODSIDE APPLE FACILITY
WOODSIDE
TRUCK TURNAROUND

DRAWN RD	DESIGNER MR	APPROVED HP	DWG No. A2020-10798	SHEET C02	REV B
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12. Appendix E – MUSIC Audit Report



Source Nodes

Parameter	User Input	Check	Guideline	Comments
Node Urban does not have any errors. (Node 3)				
Node Copy of Urban does not have any errors. (Node 4)				

Treatment Nodes

Parameter	User Input	Check	Guideline	Comments
Node Bioretention does not have any errors. (Node 1)				
Node Bioretention 2 does not have any errors. (Node 2)				

Start Date	1-01-1970	not one of	1/01/1979;1/01/1983;1/01/2001;1/01/2001	Should be based one of the recommended South Australia regional template alternative period is provided and justified.
End Date	31-12-1970 11:54:00 PM	not one of	31/12/1988 11:54:00 PM;31/12/1992 11:54:00 PM;31/12/2010 11:54:00 PM;31/12/2010 11:54:00 PM	Should be based one of the recommended South Australia regional template alternative period is provided and justified.
Rainfall Station	23034 ADELAIDE	not one of	023013 PARAFIELD; 023090 ADELAIDE; 023875 PARAWA; 023122 ROSEWORTHY	Should be one of the recommended South Australia regional template alternative period is provided and justified as being well suited for the site taking relevant climate conditions. This should generally be based on a weighted annual rainfall of multiple daily rainfall stations in proximity of the site with an appropriate corresponding period of mostly complete 6 minutes. Standard templates are available on Water Sensitive South Australia's website. Templates from website should be used rather than raw data as rainfall data is more representative of the site.
Mean Annual Rainfall (mm)	409	not one of	450;585;866;404	Should be based one of the recommended South Australia regional template alternative period is provided and justified.
Mean Annual ET (mm)	1129	not one of	1159;1143;1132;1145	Should be based one of the recommended South Australia Water regional template alternative period is provided and justified. Note that the PET station representative of the site may be chosen in in this case.

13. Appendix F – Catchment Analysis



BUILDINGS

LAND DIVISION

INFRASTRUCTURE

RESOURCES &
INDUSTRIAL

DEFENCE

Catchment Analysis

Naughtons Road &
Onkaparinga Valley Road

Reference A2020 – 10798_A

13 November 2020

Head Office 452 Pulteney Street
Adelaide, South Australia 5000
08) 8231 2832
mlei@mlei.com.au

ABN 15 638 302 913

Naughtons Road, Woodside - Stormwater Analysis Summary

MLEI Consulting Engineers were engaged to do a catchment analysis for the network contributing to a discharge location at the corner of Naughtons and Onkaparinga Valley Roads. The purpose of the investigation is to determine the change in peak flows as a result of the increased runoff from the development of the proposed apple facility and any increased burden on the existing stormwater infrastructure.

Council advised to compare the expected peak flow hydrographs from the development to the existing condition to observe whether flows correspond and compound for a 5% Annual Exceedance Probability (AEP) storm event.

Location SA and a site survey were used to determine the upstream catchment, refer enclosed calculation sheets. The site is located in a light industrial zoning and surrounded by developed sites, further upstream are rural residential sites and rural bushland.

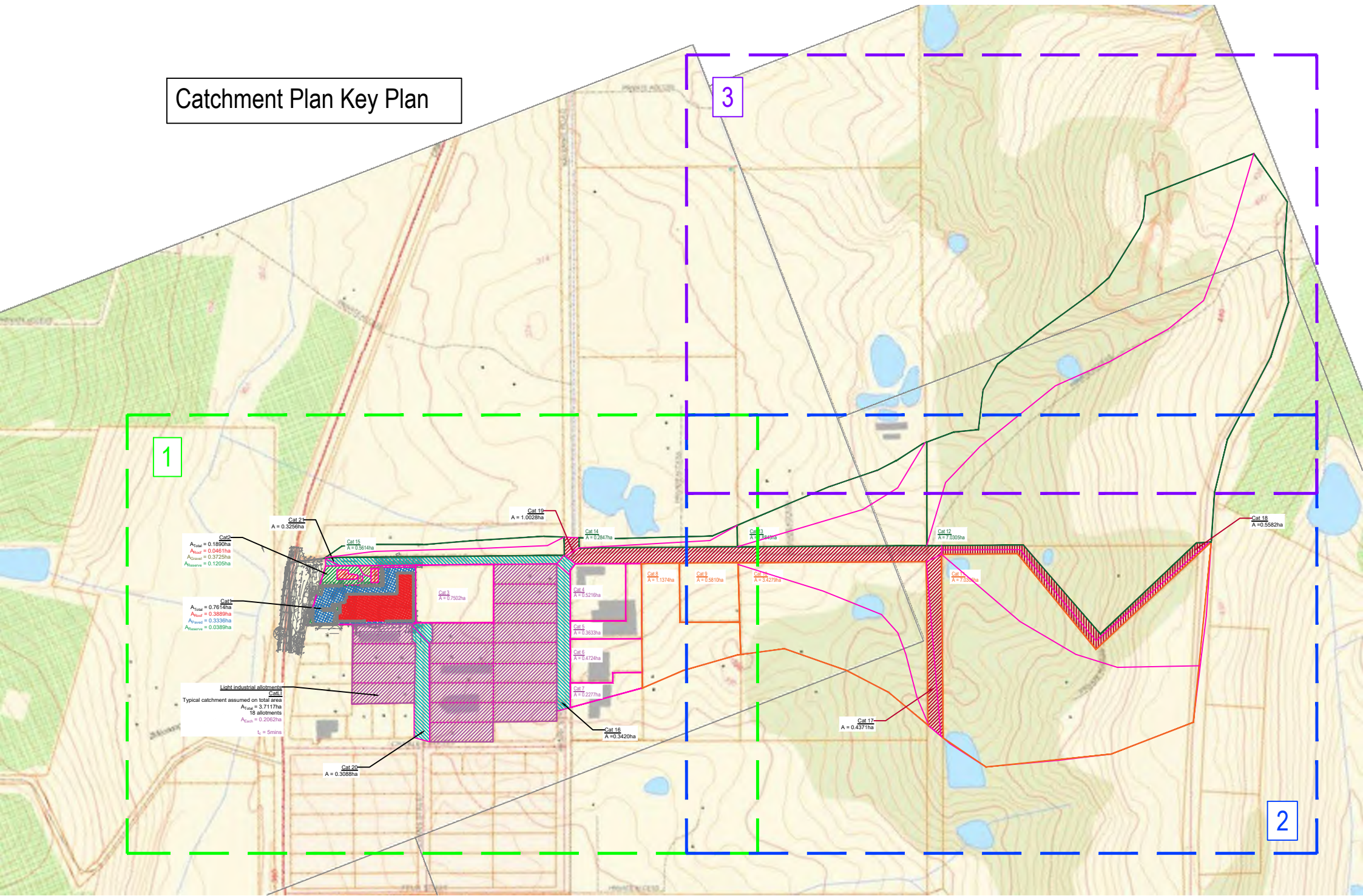
Stormwater peak flow rates have been determined for both the existing site and the proposed development. There is expected to be an increase in the total peak flow for the proposed development when compared to the existing site. Looking at the overall catchment at the point of discharge, the site is unlikely to have a major impact on overall peak flow based on the proximity to outlet and the upstream catchment's time of concentrations.

The volumes are computed by using DRAINS modelling. The calculations are based on the Horton/ILSAX method using the 1 in 20 year (5% AEP) peak storm to determine the runoff of stormwater from the site.

The results of the analysis find a negligible increase (0.23%) in peak flow from the catchment in the post-development scenario of the Woodside Apple Facility, in comparison to the current rural state of the site for the 5% AEP storm event.

Based on the findings of the analysis and the location being within several metres from the Onkaparinga River and no directly downstream properties, it would seem appropriate for unrestricted discharge of stormwater from the development, providing safe stormwater management practices are in place for major events. The expectation from the results is that the development will not create a sizeable increased burden to the existing network.

Catchment Plan Key Plan



1

Cat2
 $A_{Total} = 0.1890ha$
 $A_{Roof} = 0.0461ha$
 $A_{Gravel} = 0.3725ha$
 $A_{Reserve} = 0.1205ha$

Cat1
 $A_{Total} = 0.7614ha$
 $A_{Roof} = 0.3889ha$
 $A_{Paved} = 0.3336ha$
 $A_{Reserve} = 0.0389ha$

Light industrial allotments
CatLI
Typical catchment assumed on total area
 $A_{Total} = 3.7117ha$
18 allotments
 $A_{Each} = 0.2062ha$
 $t_c = 5mins$

Cat 21
 $A = 0.3256ha$

Cat 15
 $A = 0.5614ha$

Cat 19
 $A = 1.0028ha$

Cat 14
 $A = 0.2847ha$

Cat
 $A =$

Cat 8
 $A = 1.1374ha$

Cat 9
 $A = 0.5810ha$

C
 $A =$

Cat 3
 $A = 0.7502ha$

Cat 4
 $A = 0.5216ha$

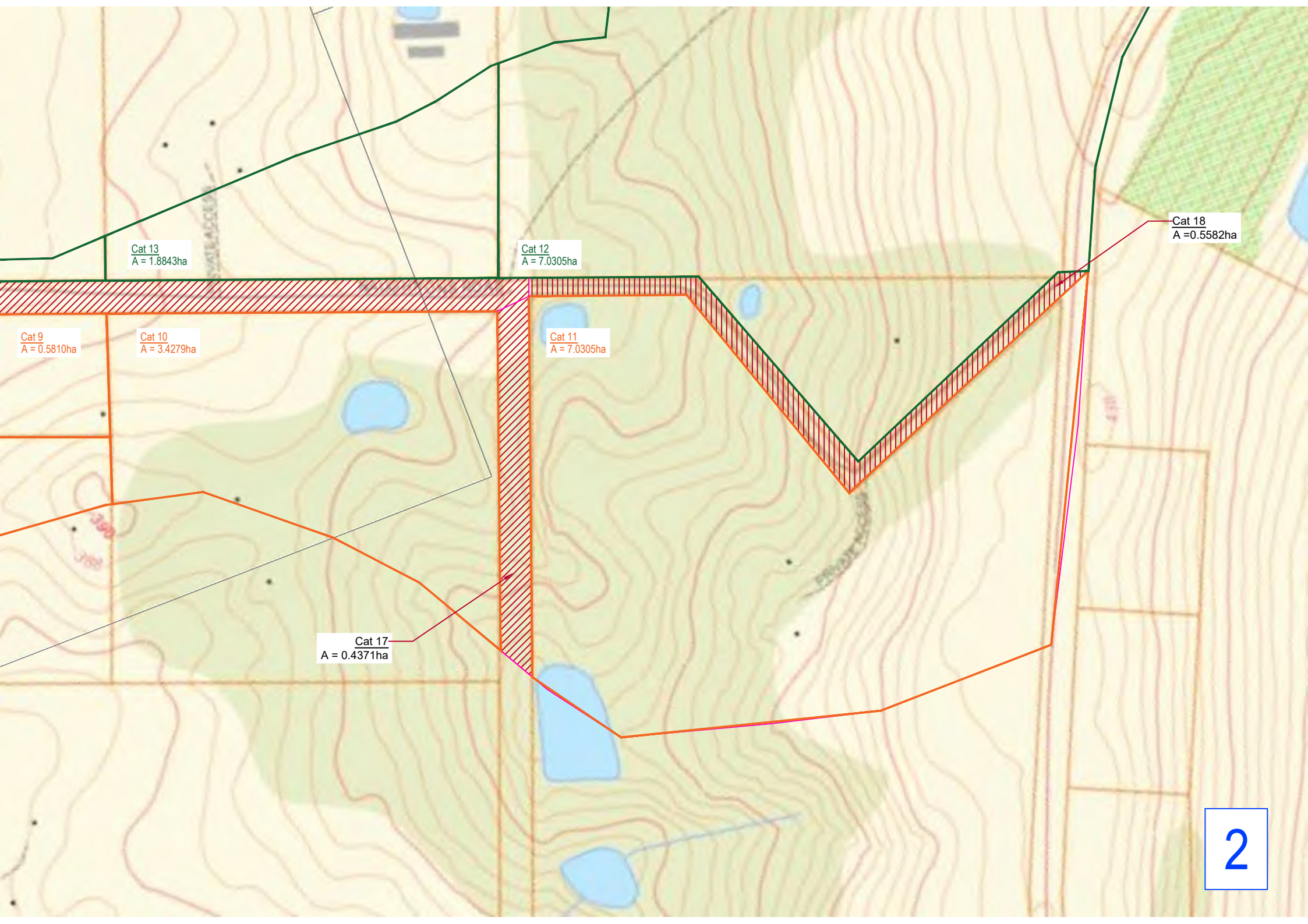
Cat 5
 $A = 0.3633ha$

Cat 6
 $A = 0.4724ha$

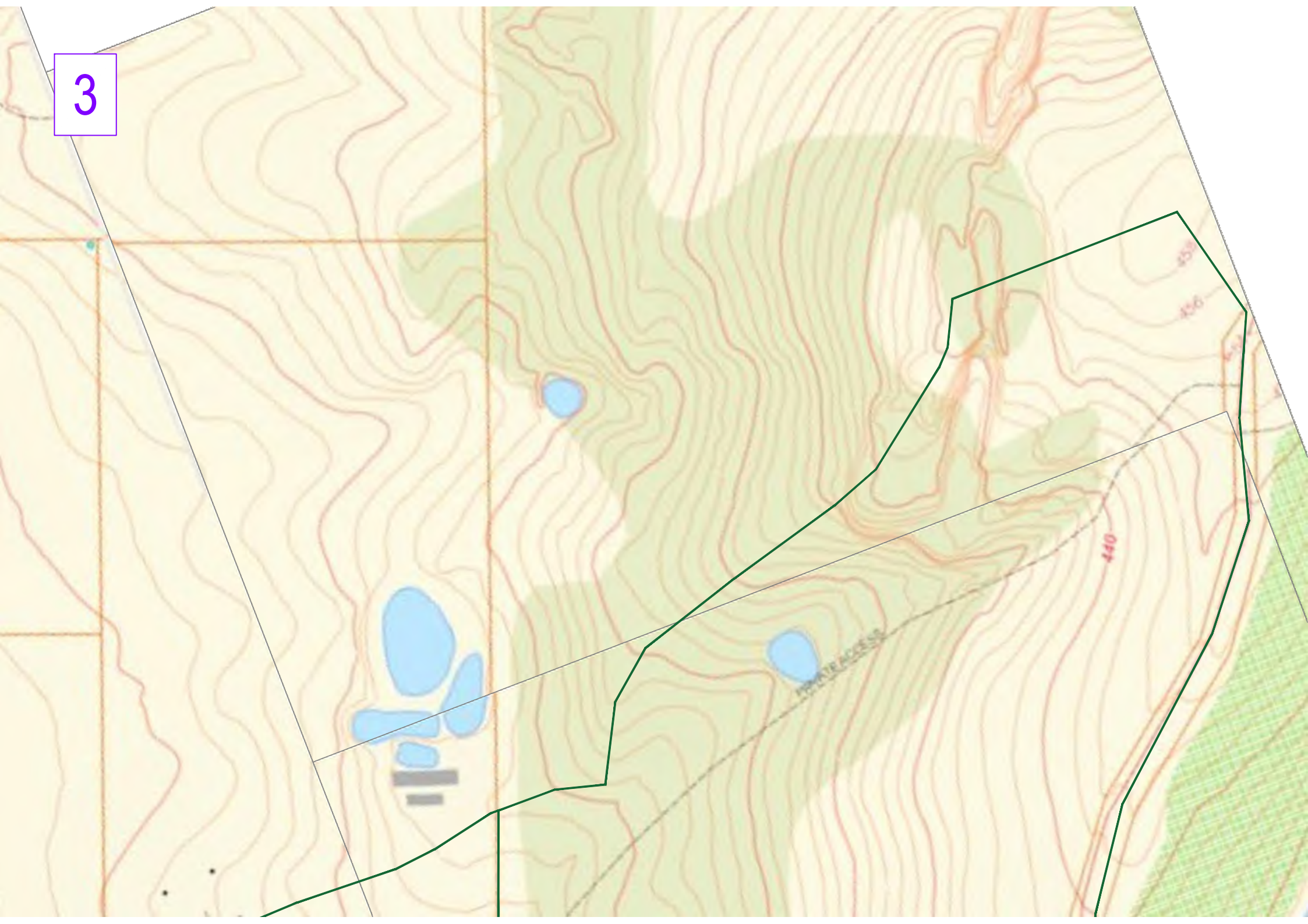
Cat 7
 $A = 0.2277ha$

Cat 16
 $A = 0.3420ha$

Cat 20
 $A = 0.3088ha$



3



CATCHMENT ANALYSIS

REF./COMMENT

Catchment Analysis

Refer 'Catchment plan'

Catchment name	Total area (ha)	Land use type (ha)				Impervious fraction, f				Overall impervious
		Roof	Allotment	Road/Paved	Reserve	Roof	Allotment	Road/Paved	Reserve	
CatLI	0.2062	0.0000	0.2062	0.0000	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat1	0.7614	0.3889	0.0000	0.3336	0.0389	1.0000	0.60	0.90	0.10	91.0%
Cat2	0.1890	0.0461	0.0000	0.0223	0.1205	1.0000	0.60	0.60	0.10	37.9%
Cat3	0.7502	0.0000	0.7502	0.0000	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat4	0.5216	0.0000	0.5216	0.0000	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat5	0.3633	0.0000	0.3633	0.0000	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat6	0.4724	0.0000	0.4724	0.0000	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat7	0.2277	0.0000	0.2277	0.0000	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat8	1.1374	0.0000	1.1374	0.0000	0.0000	1.0000	0.15	0.90	0.10	15.0%
Cat9	0.5810	0.0000	0.5810	0.0000	0.0000	1.0000	0.15	0.90	0.10	15.0%
Cat10	3.4279	0.0000	3.4279	0.0000	0.0000	1.0000	0.15	0.90	0.10	15.0%
Cat11	7.0305	0.0000	7.0305	0.0000	0.0000	1.0000	0.15	0.90	0.10	15.0%
Cat12	15.4611	0.0000	0.0000	0.0000	15.4611	1.0000	0.15	0.90	0.10	10.0%
Cat13	1.8843	0.0000	0.0000	0.0000	1.8843	1.0000	0.15	0.90	0.10	10.0%
Cat14	0.2846	0.0000	0.0000	0.0000	0.2846	1.0000	0.15	0.90	0.10	10.0%
Cat15	0.5614	0.0000	0.0000	0.0000	0.5614	1.0000	0.90	0.90	0.10	10.0%
Cat16	0.3420	0.0000	0.0000	0.3420	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat17	0.4371	0.0000	0.0000	0.4371	0.0000	1.0000	0.90	0.70	0.10	70.0%
Cat18	0.5582	0.0000	0.0000	0.5582	0.0000	1.0000	0.90	0.70	0.10	70.0%
Cat19	1.0028	0.0000	0.0000	1.0028	0.0000	1.0000	0.90	0.70	0.10	70.0%
Cat20	0.3088	0.0000	0.0000	0.3088	0.0000	1.0000	0.90	0.90	0.10	90.0%
Cat21	0.3256	0.0000	0.0000	0.3256	0.0000	1.0000	0.90	0.90	0.10	90.0%
Total	36.8345	0.4350	14.7182	3.3304	18.3508	1.0000	0.63	0.86	0.10	0.3927

Fraction Impervious values based on table 4.05.1 QUDM. Refer below

Table 4.05.1 Fraction impervious vs. development category

Development Category	Fraction Impervious (f)
Central Business	1.00
Commercial, Local Business, Neighbouring Facilities, Service Industry, General Industry, Home Industry	0.90
Significant Paved Areas e.g. roads and car parks	0.90
Urban Residential – High Density	0.70 to 0.90
Urban Residential – Low Density (including roads)	0.45 to 0.85
Urban Residential – Low Density (excluding roads)	0.40 to 0.75
Rural Residential	0.10 to 0.20
Open Space & Parks etc.	0.00

CATCHMENT ANALYSIS REF./COMMENT

For hardstand areas:
 - Refer Figure 4.09 & Equation 4.06 QUDM
 - Assume 20m max sheet flow to be conservative for bitumen
 - Assume 50m max sheet flow for rural
 - Assume 150m max sheet flow for compacted gravel
 - Assume Horton's Coefficient $n^*=0.010$ for bitumen
 - Assume Horton's Coefficient $n^*=0.018$ for compacted gravel
 - Assume Horton's Coefficient $n^*=0.09$ for rural

For reserve areas:
 - Refer Friend's equation Eq 4.06 QUDM
 - Refer Mannings's equation Eq 4.09 QUDM
 - Assume average grassed surface ($n=0.045$)

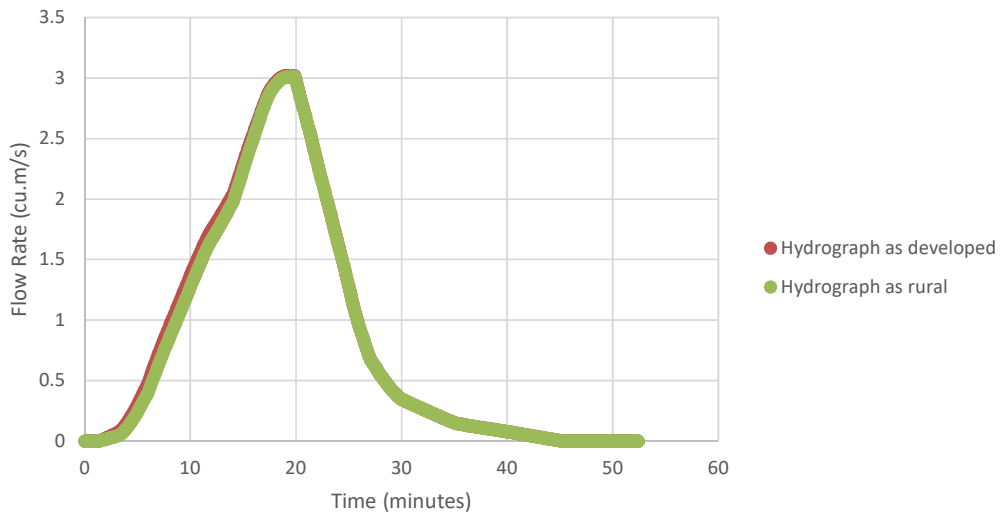
Catchment name	Reserve			Road/network/channel			Allotment	Total time of concentration		Comments
	Length (m)	Slope (%)	tc	Distance (m)	Fall (m)	tc		tc	Paved tc (mins)	
Cat11							8.0	8.0	10.0	+2mins grassed tc
Cat1							5.0	5.0	7.0	+2mins grassed tc
Cat2							5.0	5.0	7.0	+2mins grassed tc
Cat3							10.0	10.0	12.0	+2mins grassed tc
Cat4							10.0	10.0	12.0	+2mins grassed tc
Cat5							10.0	10.0	12.0	+2mins grassed tc
Cat6							10.0	10.0	12.0	+2mins grassed tc
Cat7							10.0	10.0	12.0	+2mins grassed tc
Cat8							8.0	8.0	10.0	+2mins grassed tc
Cat9							10.0	10.0	12.0	+2mins grassed tc
Cat10							10.0	10.0	12.0	+2mins grassed tc
Cat11							8.0	8.0	10.0	+2mins grassed tc
Cat12	694.0	9.9	22.4	644.0	64.0	5.0	0.0	25.4	27.4	-2mins paved tc
Cat13	288.9	6.6	24.3	238.9	15.7	1.5	0.0	23.8	25.8	-2mins paved tc
Cat14	208.7	3.4	27.8	158.7	5.3	1.5	0.0	27.3	29.3	-2mins paved tc
Cat15	310.2	2.9	28.6	260.2	7.5	2.2	0.0	28.8	30.8	-2mins paved tc
Cat16				193.0	8.0	3.6	0.0	3.6	5.6	+2mins grassed tc
Cat17				224.8	16.0	1.4	0.0	1.4	3.4	+2mins grassed tc
Cat18				446.4	45.0	4.0	0.0	4.0	6.0	+2mins grassed tc
Cat19				485.5	20.0	5.8	0.0	5.8	7.8	+2mins grassed tc
Cat20				158.8	4.0	2.1	0.0	2.1	4.1	+2mins grassed tc
Cat21				300.0	10.0	3.0	0.0	3.0	5.0	+2mins grassed tc

Table 4.06.1 Recommended standard inlet times

Location	Inlet Time (Minutes)
Road surfaces and paved areas	5
Urban residential areas where average slope of land at top of catchment is greater than 15%.	5
Urban residential areas where average slope of land at top of catchment is greater than 10% and up to 15%.	8
Urban residential areas where average slope of land at top of catchment is greater than 6% and up to 10%.	10
Urban residential areas where average slope of land at top of catchment is greater than 3% and up to 6%.	13
Urban residential areas where average slope of land at top of catchment is up to 3%.	15

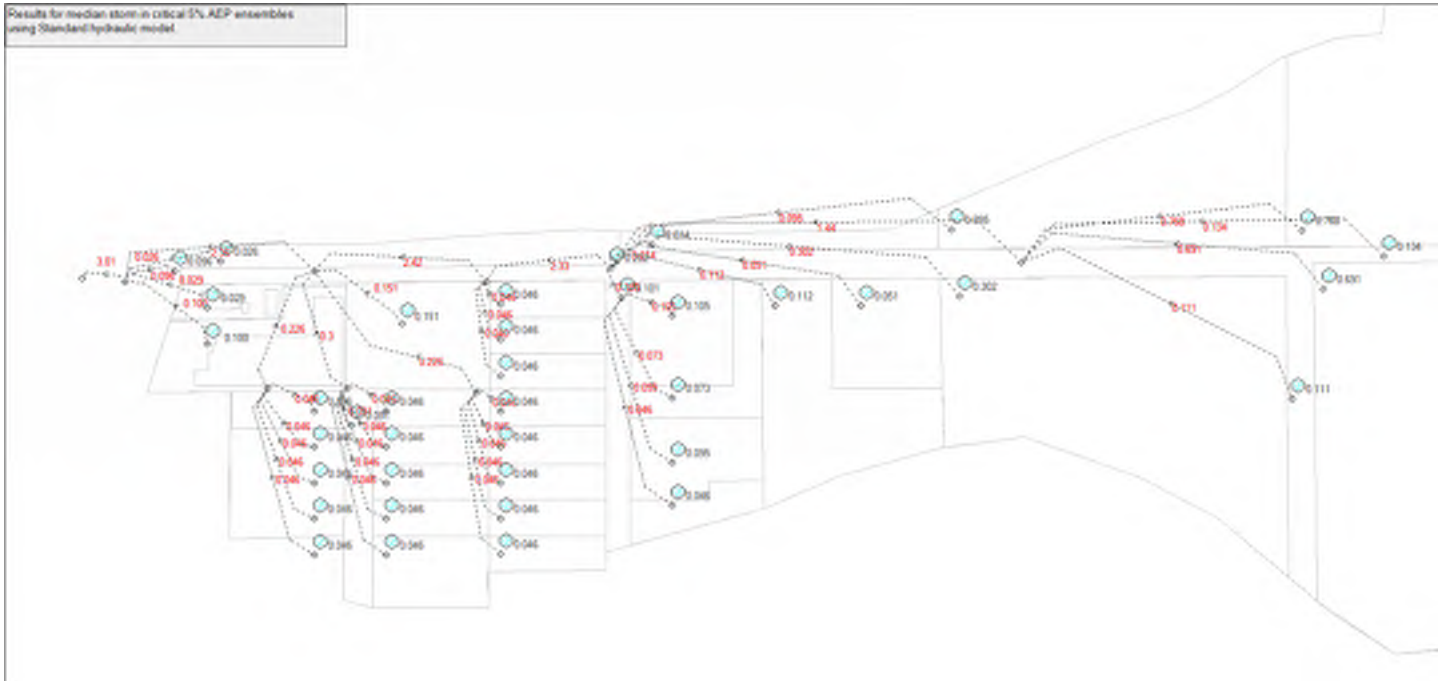
Note: The average slopes referred to are the slopes along the predominant flow path for the catchment in its developed state.

Median Hydrograph Comparison

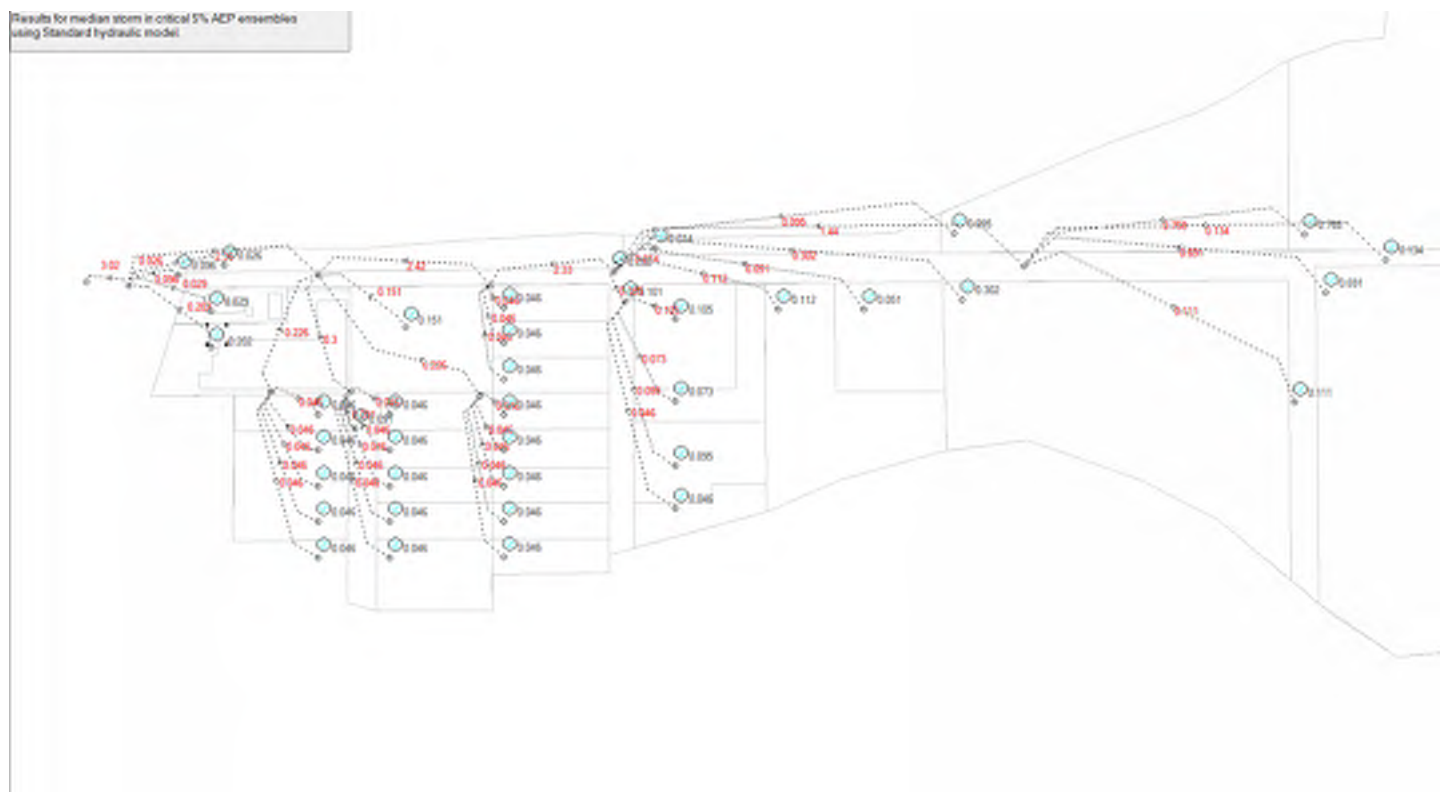


DRAINS 5% AEP Results

Undeveloped Site



Developed Site





THE *Adelaide*
TREE SURGERY

A division of *tree aware*

3 Ellemsea Circuit
LONSDALE SA 5160
ABN: 33 099 478 994

Phone: 8371 5955 Mobile: 0408 086 774 Fax: 8297 6885 Email: mark@adelaidetreesurgery.com

Pre-Development Arboricultural Assessment and Report

Prepared for
Joyson Orchards
Attention: Linda MacAuley

Site Address
Lots 202 & 204 Onkaparinga Valley Road
Woodside SA 5244

In Regard to
Various Trees and Proposed Development



Prepared By:
Mark Elliott Consulting Arborist/Diploma Arboriculture



THE *Adelaide*
TREE SURGERY
A division of *tree aware*

3 Ellemsea Circuit
LONSDALE SA 5160
ABN: 33 099 478 994

Phone: 8371 5955 **Mobile:** 0408 086 774 **Fax:** 8297 6885 **Email:** mark@adelaidetreesurgery.com

CONTENTS	Page No
1.0 Introduction	3
2.0 Term of Reference	3
3.0 Caveat Emptor	3
4.0 The Site	3
5.0 The Proposal	4
6.0 The Tree/s	5
7.0 Potential Impacts	6
8.0 Tree Discussion	8
9.0 Legislation	8
10.0 Recommendations	8
Appendix A: References	10
Appendix B: Disclaimer and Limitations	11

1.0 INTRODUCTION

- 1.1 This report has been prepared at the request of **Ms Linda MacAuley** from **Joyson Orchards** and is in relation to five trees and the **Proposed Development** of a **Packaging Facility** at **Lots 202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 1.2 Of the five trees consist of two **Regulated *Eucalyptus camaldulensis* (River Red Gum)** and the **Non Protected *Eucalyptus camaldulensis* (River Red Gum)** which one is a council owned tree.
- 1.3 The subject two **Regulated River Red Gums** have a stem circumference greater than 2 meters and less than 3 meters , when measured at a point 1.0 metre above natural ground level , therefore, these trees can be declared as a **Regulated Trees** as defined in the **SA Development Act 2008**.
- 1.4 The report also uses the **Australian Standards 4970 2009: The Protection of Trees on Development Sites** as the guiding principle in regards to the proposed development of the site and the protection of all trees.

2.0 TERMS OF REFERENCE

- 2.1 Instructions were received is September **2020** via email.
- 2.2 The instructions requested were to undertake a Pre-Development Arborist Report for **2 x Regulated *Eucalyptus camaldulensis* (River Red Gum)** and **2 Non Protected) *Eucalyptus camaldulensis* (River Red Gum)** which are located at **202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 2.3 This report uses the **Australian Standard, 4970-2009: The Protection of Trees on Development Sites** as the guiding principles in regard to the proposed development of the site.

3.0 CAVEAT EMPTOR

- 3.1 This is a stage 1 '**Ground Report**'. The trees were inspected from the ground only.
- 3.2 The report is limited by the time of the inspection.
- 3.3 The report reflects the trees as found on the day of inspection. Any changes to site conditions or surroundings, such as construction works, landscape works or further failures or pruning, may alter the findings of the report.
- 3.4 The report is based on the inspection of the site and the trees along with the supplied plans of the proposed development.
- 3.5 The inspection period to which this report applies is three months from the date of the report.

4.0 THE SITE

- 4.1 The proposed development is located at **Lot 202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 4.2 The **Gum Trees** are located along the front of the property of **202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 4.3 **Woodside** is a rural township which is located within the council boundaries of the **Adelaide Hills Council**.



Figure 1 shows an aerial image of the property at 202 & 203 Onkaparinga Valley Road, Woodside SA 5244.

5.0 THE PROPOSAL:

- 5.1 It is proposed to construct a new **Packing Facility** at the property of **202 & 204 Onkaparinga Valley Road, Woodside SA 5244** (refer Figure 2).
- 5.2 The designated car parking area along the northern side is to be constructed using a permeable product such as granitic sand/gravel
- 5.3 The northern side boundary line fence is to be a 2.8 meter acoustic fence

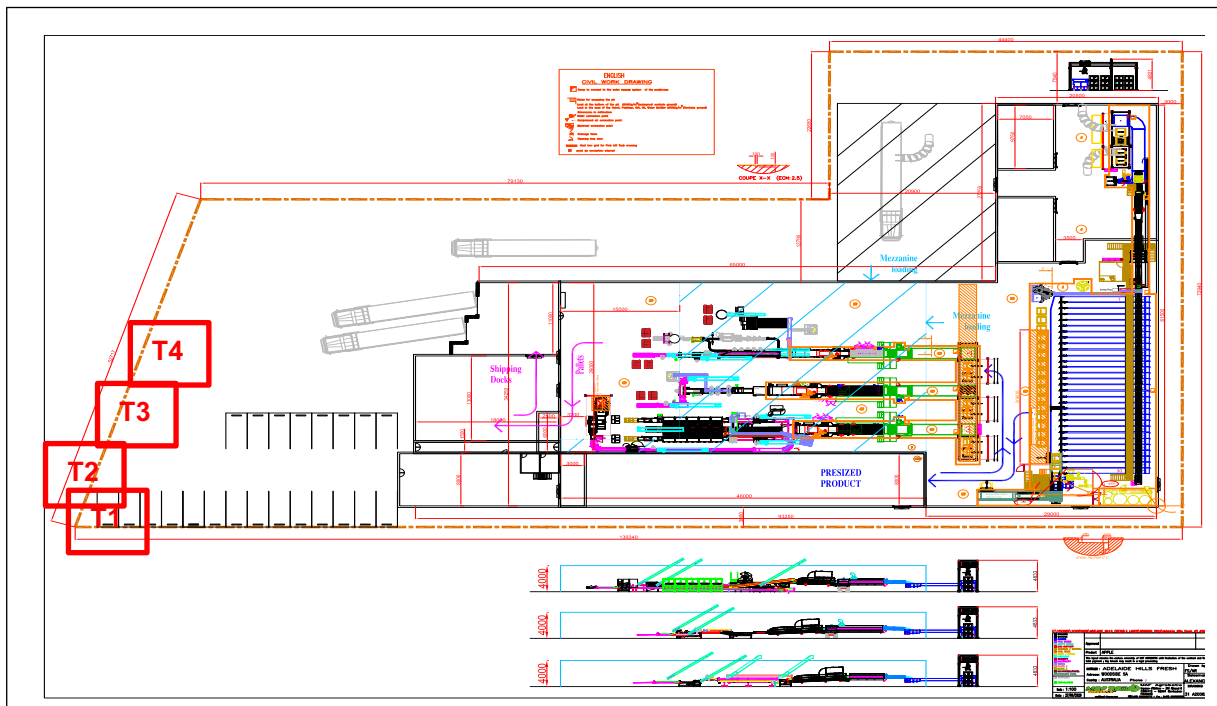


Figure 2 shows the proposed new plans for the Packing Facility of 202 & 204 Onkaparinga Valley Road, Woodside SA 5244.

5 THE TREE/S -


5.1 Following is information of the five subject trees within this **Pre-Development Arbpricultural Report**

Tree Number	Tree Name	Significant and or Regulated	Tree Condition/Information
1	River Red Gum	Regulated	<p>This River Red Gum is showing good health and condition. The tree is located along the front boundary line.</p> <p>The tree is located closest to the side boundary line furthest away from the proposed main driveway</p> <p>There trees foliage had good density and vigour.</p> <p>There was some minor deadwood located throughout the crown.</p> <p>The height of the tree is approximately 14 – 15 meters.</p> <p>The crown of the tree connect with the neighbouring River Red Gum which both crowns for a large broad spreading crown.</p>
2	River Red Gum	Regulated	<p>This River Red Gum is showing good health and condition. The tree is located along the front boundary line.</p> <p>This tree is the second tree from the righthand side boundary line.</p> <p>There trees foliage had good density and vigour.</p> <p>There was some minor deadwood located throughout the crown.</p> <p>The height of the tree is approximately 15 plus meters.</p> <p>The crown of the tree connect with the neighbouring River Red Gum which both crowns for a large broad spreading crown.</p>
3	River Red Gum	Non Protected	<p>This tree is a small tree that is showing good health and condition</p>

			<p>The height of the tree is approximately 5 – 6 meters and the tree has a small crown as a result of being over shadowed by the larger tree.</p> <p>This tree is a council owned tree.</p>
4	River Red Gum	Non Protected	<p>This tree is a small tree that is showing good health and condition</p> <p>The height of the tree is approximately 5 meters and the tree has a small crown as a result of being over shadowed by the larger tree.</p>



Figure 3 shows the four River Red Gums which are located at the front of the property at Lot 202 & 204 Onkaparinga Valley Road, Woodside Sa 5244.



5.2 Tree Measurements (Protected Trees):

Tree Number	Structural Root Zone (SRZ)	Tree Protection Zone (TPZ)
Tree #1	2.55 meters	10.98 meters
Tree #2	2.63 meters	10.68 meters

5.3.1 The measurements within the above table are measured as a radius from the center of the tree base.

6.0 PROPOSED DEVELOPMENT & POTENTIAL IMPACTS

6.1 The aim of this report is to provide guidelines for best practise tree protection measures in accord with ***Australian Standard AS4970-2009 'Protection of Trees on Development Sites'***

6.2 The preamble of the standard provides a brief outline of why it is important to retain and protect trees on development sites and the following Section: The Tree Protection Zone details the zones around a tree that are required to protect it.

6.3 AUSTRALIAN STANDARD: AS4970-2009 'Protection of Trees on Development Sites'

6.3.1 *A living tree is a dynamic organism that needs specific environmental conditions to continue healthy, stable growth. It is rarely possible to repair stressed and injured trees, so substantial injury needs to be avoided during all stages of development and construction.*

6.3.2 *For trees to be retained and their requirements met, procedures must be in place to protect trees at every stage of the development process. This should be taken into account at the earliest planning stage of any outdoor event or design of a development project where trees are involved.*

6.3.3 *Trees and their root systems may occupy a substantial part of any development site and because of their potential size, can have a major influence on planning the use of the site.*

6.3.4 *Existing trees of appropriate species and sound structure can significantly enhance new development by providing immediate benefits such as shade and stormwater reduction as well as complementing new development.*

6.3.5 *Most trees will take many years and possibly decades to establish but can be injured or killed in a very short time, as their vulnerability is commonly not understood. This is especially so in relation to tree root systems which cannot be seen. Irreparable injury frequently occurs in the early stages of site preparation and remedial measures routinely fail.*

6.3.6 *Early identification and protection of important trees on development sites is essential from the outset and will minimise the problems of retaining inappropriate trees.*

6.3.7 *Successful long-term retention of trees on development sites depends on an acceptance and acknowledgement of the constraints and benefits that existing trees generate. Protecting trees in accordance with the Standard may influence design and construction costs and this should be considered in project budgets and contracts. The gains and benefits of retaining trees will accrue if the measures detailed in the Standard are applied.*

6.4 THE TREE PROTECTION ZONE

6.4.1 The **Tree Protection Zone (TPZ)** is the principal means of protecting trees on development sites. The TPZ is a combination of the root area and crown area requiring protection. It is an area isolated from construction disturbance, so that the tree remains viable. The TPZ incorporates the structural root zone (SRZ).

6.4.2 It may be possible to encroach into or make variations to the standard or optimal TPZ. Encroachment includes excavation, compacted fill and machine trenching.

6.4.3 If the proposed encroachment is greater than 10% of the TPZ or inside the SRZ, the project arborist must demonstrate that the tree(s) would remain viable.

6.5 SRZ & TPZ CALCULATIONS

6.5.1 **The SRZ (or CRZ): Structural/Critical Root Zone** is the zone around a tree required to protect the tree's stability. **Generally, no development activities are permitted within this zone unless there are no other suitable options.**

6.5.2 **The TPZ or Optimal Tree Protection Zone is the principal means of protecting the tree and is calculated using the formula TPZ = DBH (diameter @ 1.4 meters above ground level) x 12.**

7.1 POTENTIAL IMPACTS

- 7.1.1 I have provided general comment in relation to the development of the Packaging Warehouse and its potential impact on the two **River Red Gums**.
- 7.1.2 There is potential that some tree roots from the tree closest to the proposed driveway could be exposed during the works.
- 7.1.3 Refer to **Section 10** of this report for recommendations and correct methodologies. It is my opinion that these recommendations and methodologies apply to protected and no protected trees.

7 TREE DISCUSSIONS

- 8.1 I believe the proposed development will not have a detrimental effect of the three trees which are to remain (**Trees 1 and 2**) if the correct methodologies are followed and implemented.
- 8.2 The two **River Red Gums** which are located at the front of the property are located far enough away from the proposed driveway to have minimal impact on the trees health and condition. The encroachment into the Tree Protection Zones (TPZ;s) of the trees is less than 10%.
- 8.3 Unfortunately the two smaller **River Red Gums (Trees 3 and 4)** will be recommended for removal which does allow for better protection of the **Regulated River Red Gums**.
- 8.4 There are no other areas of the proposed development that has any effect on any trees within the property or neighbouring properties.
- 8.5 Refer to the recommended methodologies (**refer section 10**) for this proposed development to ensure that the trees remain in their current good health and condition.

9 LEGISLATE REQUIREMENTS

- 9.1 The two **Regulated Trees** have a single stem greater than 2 meters and less than 3 meters when measured at 1 metre above ground level, therefore fulfilling the criteria contained within the **Development Regulations 2008** and also refers to the **Adelaide Hills Development Plan** consolidated **8th August 2019**.

PRINCIPLES OF DEVELOPMENT CONTROL

Development should have minimal adverse effects on regulated trees. **Yes – I believe the two River Red Gums will not be affected by the proposed development which includes the driveway entrance and driveway past the Protected Trees for the proposed Packaging Warehouse. The encroachment into both River Red Gums Tree Protection Zones (TPZ.s) have been calculated at less than 10% each.**

10 RECOMMENDATIONS

10.1 Having considered all five trees within this report, I am recommending the following:

10.3 DEVELOPMENT PHASE:

- 10.3.1 All works required within the area of the Tree Protection Zones (TPZ's) must be undertaken by hand, using non-destructive excavation methods or under the supervision of the Project Arborist.
- 10.3.3 All underground services are to avoid the Tree Protection Zone (TPZ) of all trees where possible.

10.4 TREE PROTECTION DURING DEVELOPMENT PHASE:

10.4.1 The Tree Protection Plan for the Regulated Trees is to be installed for duration of the development. This fencing is to be chain mesh panels (1.80 meters high) and clamped together to avoid easy access inside the Tree Protection Zones (TPZ).

10.4.2 Mulch the TPZ using 100mm of course mulch

10.4.3 Irrigate the Tree Protection Zones (TPZ's) periodically as directed by Project Arborist

10.4.4 **NO STORAGE OF ANY BUILDING MATERIAL OR MACHINERY WITHIN THE FENCED TREE PROTECTION ZONE (TPZ) AREAS.**

10.4.5 Protection Zone (TPZ) fencing is to be clearly marked with signage saying – **TREE PROTECTION ZONE, DO NOT ENTER. PROJECT ARBORIST – THE ADELAIDE TREE SURGERY – 08 83715955**

10.5 It is recommended that the two non-protected River Red Gums are to be completely removed as they are located within area of main driveway to the property.

10.5 **Council Approval from the Adelaide Hills Council needs to be granted prior to commencement of any works.**

Mark Elliott
Consultant Arborist/Diploma Arboriculture

APPENDIX A: REFERENCES

The Development Act (1993) South Australian Legislation

The Development Regulations (1993) South Australian Legislation

City of Burnside Development Plan Consolidated 8th August 2017

Australian Standards AS 4970-2009: Protection of Trees on Development Sites

ADELAIDE HILLS COUNCIL
RECEIVED 26/11/2020

APPENDIX B: DISCLAIMER AND LIMITATIONS

This report only covers identifiable defects present at the time of inspection. The author accepts no responsibility or can be held liable for any structural defect or unforeseen event/situation that may occur after the time of inspection, unless clearly specified timescales are detailed within the report.

The author cannot guarantee trees contained within this report will be structurally sound under all circumstances, and cannot guarantee that the recommendations made will categorically result in the tree being made safe.

Unless specifically mentioned this report will only be concerned with above ground inspections, that will be undertaken visually from ground level. Trees are living organisms and as such cannot be classified as safe under any circumstances. The recommendations are made on the basis of what can be reasonably identified at the time of inspection therefore the author accepts no liability for any recommendations made.

Care has been taken to obtain all information from reliable sources. All data has been verified insofar as possible; however, the author can neither guarantee nor be responsible for the accuracy of information provided by others.

MLM/20-0156



12 November 2020

Traffic • Parking • Transport

Unit 6, 224 Glen Osmond Road
FULLARTON SA 5063

T: +61 8 8338 8888

F: +61 8 8338 8880

E: mfy@mfy.com.au

W: mfy.com.au

MFY Pty Ltd

ABN 79 102 630 759

Mr Mark Kwiatkowski
Adelaide Planning and Development Solutions
52A Mount Barker Road
HAHNDORF SA 5245

Dear Mark,

PROPOSED APPLE SORTING FACILITY, 198-200 ONKAPARINGA VALLEY ROAD, WOODSIDE

I refer to the proposed apple sorting facility to be constructed at the above site. As requested, I have reviewed the traffic and parking aspects of the proposal and provided advice in respect to access requirements. In undertaking the assessment, I have liaised with the Department of Infrastructure and Transport (DIT) and Council.

The assessment has been based on Beyond Ink Drawing No. PD001 dated 11 November 2020.

1 EXISTING SITUATION

The subject site has frontages to Naughtons Road and a service road on the eastern side of Onkaparinga Valley Road. Access to the site is provided via Naughtons Road.

Onkaparinga Valley Road is an arterial road within the care and control of the Commissioner of Highways. It has an annual daily volume of 9,300 vehicles and a posted speed limit of 60 km/h adjacent the subject site.

The service road provides access to adjacent allotments, on which a number include commercial developments and the balance are undeveloped. The service road intersects with Onkaparinga Valley Road at its southern end and at the Onkaparinga Valley Road/Naughtons Road intersection.

The existing intersections with the service road appear to have been created without having specific regard to recommended design criteria (as is historically the case with much of the road network in the Adelaide Hills) as there are a number of alignment and safety deficiencies. The following aspects of the design are particularly relevant to note:

- the angle of the southern approach of the service road with Onkaparinga Valley Road is currently approximately 12 degrees which is significantly more acute than the recommended

minimum angle of 70° (reference Austroads “*Guide to Road Design – Part 4A: Unsignalised and Signalised Intersections*”). This compromises sight lines for drivers exiting the intersection. Figure 1 illustrates the angle of the existing intersection;



Figure 1: Angle of existing intersection

- Drivers of large vehicles exiting the service road are not able to turn right to Onkaparinga Valley Road from the southern intersection. While the intent of the service road may have been to provide for right turn movements at the northern end, there is no traffic control prohibiting right turn movements at the southern intersection. Figure 2 illustrates a right turn movement by a semi-trailer at the intersection; and

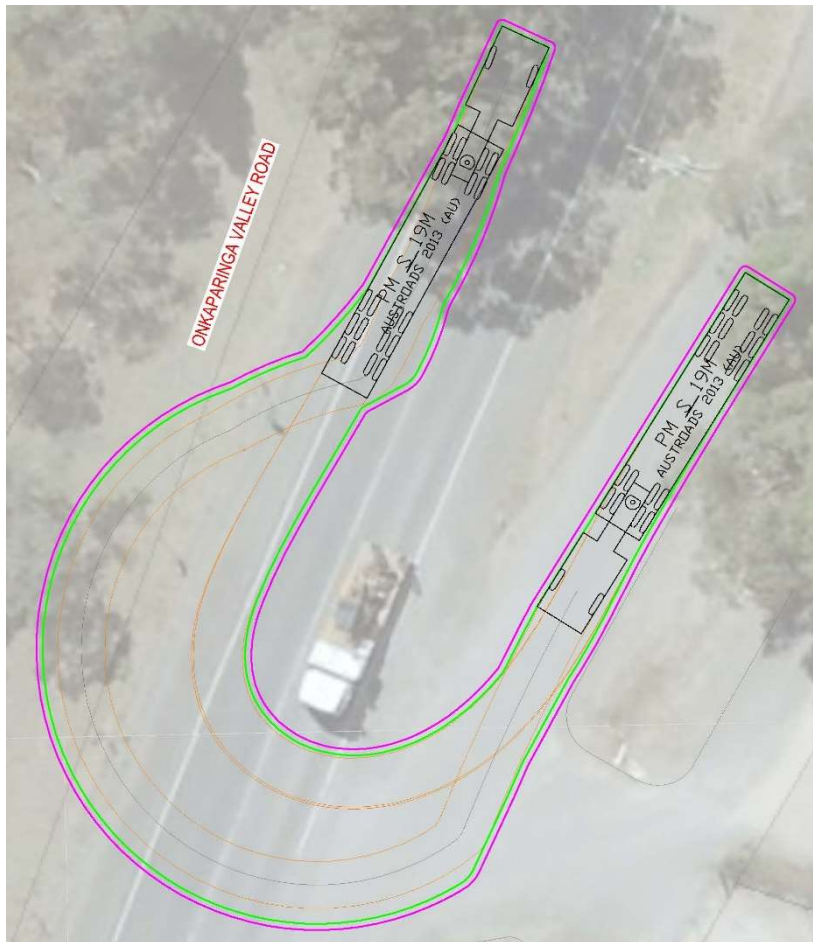


Figure 2: right turn movement by semi-trailer restricted at intersection

- the above two issues are compounded by the open drain adjacent the service road, which limits the flexibility for turning vehicles.

Naughtons Road is a local road within the care and control of Council. It is located along the northern extent of the township of Woodside, with Commercial development along the southern side and rural properties along the northern side. The road is sealed between Onkaparinga Valley Road and Henry Road. East of Henry Road, Naughtons Road is unsealed.

Naughtons Road forms an intersection with Onkaparinga Valley Road and the service road. All movements are permitted at the intersection.

The Amy Gillet Bikeway is located along the western boundary of the site. It forms an intersection with Naughtons Road to the east of the Onkaparinga Valley Road/Naughtons Road intersection.

2 PROPOSAL

The proposal is for the development of an apple sorting and packing facility. The development will include a building with a floor area of approximately 3,885 m². The uses within the building will comprise of the following components:

- approximately 145 m² of office area plus ancillary areas;
- approximately 810 m² of storage area;
- approximately 2,650 m² of processing area; and
- 30 parking spaces.

Apples which will be sorted and packed at the facility will be farmed at an apple orchard located to the south of the site and will be then distributed for sale.

2.1 ACCESS

Access for the proposal will be provided via Naughtons Road and the service road. The Naughtons Road access will be an ingress for commercial vehicles servicing the proposed development. The service road access will provide for two-way movements for staff parking and egress movements for commercial vehicles.

The proposed crossovers will be designed in accordance with Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking (AS/NZS 2890.1:2004)* and Australian Standard, *Parking Facilities Part 2: Off-street commercial vehicle facilities (AS 2890.2:2002)*. The existing crossover to the site will be closed.

The largest vehicle used for delivery will be the 19.0 m semi-trailers. Such vehicles will enter the site via Naughtons Road and exit to the service road. Figure 3 illustrates that a semi-trailer will be able to enter and exit the proposed development.



Figure 3: Semi-trailer entering and exiting the subject site

The proposed access to the service road will bisect the section of Amy Gillet Bikeway adjacent the site. The access design has ensured that there will be adequate sightlines to cyclists and pedestrians on the shared path.

The levels of the site will necessitate that a ramp be provided at the ingress on Naughtons Road and within the circulation road along the northern boundary of the site. The ramp will comply with Australian Standard, *Parking Facilities Part 2: Off-street commercial vehicle facilities (AS 2890.2:2018)* and will include appropriate transitions to provide for semi-trailer access.

2.2 SERVICE ROAD

In developing the access solution for the site, consideration was not only given to the direct access arrangements for the site but also to the accessibility to the broader road network, given that the proposal will increase the volume of semi-trailers using the service road. The following options were considered:

- A direct access being provided across the service road to Onkaparinga Valley Road; and
- Utilising the service road to then gain access to Onkaparinga Valley Road.

While a direct access could be provided to Onkaparinga Valley Road, the clearance between the two roads is insufficient to provide for queuing at the access. This could result in conflict between a vehicle stored waiting to turn to Onkaparinga Valley Road and a vehicle using the service road. Accordingly, it was considered safer to utilise the service road as the exit route.

Notwithstanding the above, it has been previously identified that the angle of the southern intersection of the service road and Onkaparinga Valley Road is deficient in respect to its intersecting angle. While this is an existing situation, it would have two ramifications for drivers exiting the subject development, namely:

- drivers of delivery vehicles would have restricted line of sight; and
- drivers of larger vehicles would not be able to turn right to Onkaparinga Valley Road.

A review of options to improve these deficiencies have therefore been considered as part of the development. In regard to the angle of approach of the intersection, minor widening of the service road on the approach to Onkaparinga Valley Road will enable the angle of approach to be increased the 67°. While this angle is still less than the recommended 70°, it is a substantial improvement when compared with the existing scenario. Further, the angle of sight lines for a truck driver will meet the 50° angle where sight lines are available, as illustrated in Figure 4

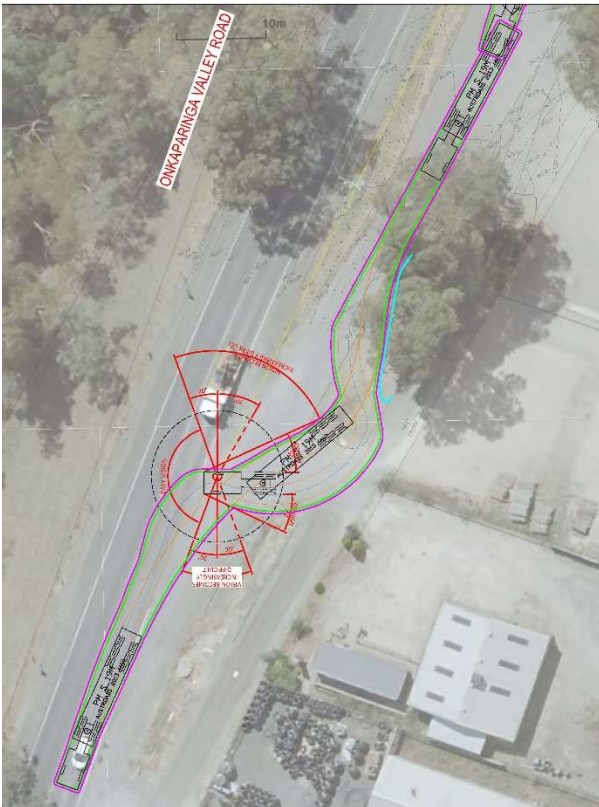


Figure 4: Sight line angle which would be achieved at modified intersection

The widening of the service road would only be required south of the proposed access and could be achieved by installing a pipe within the existing open drain adjacent the carriageway. There would be no impact on trees and no further works required at the actual intersection with Onkaparinga Valley Road.



Figure 5: Proposed minor widening of the service road

In regard to the right turn constraint at the intersection, the proposal will not generate a demand for right turning movements of large commercial vehicles. The orchard is located to the south of the site and hence apple delivery vehicles will enter via a right turn to Naughtons Road and exit via a left turn to the service road and then Onkaparinga Valley Road.

Once packed, drivers of vehicles loaded with boxed apples will travel to the South-Eastern Freeway to distribute the goods. This will require a left turn to the service road and Onkaparinga Valley Road.

Notwithstanding the above, it is proposed that a traffic management plan will be developed that requires all drivers of commercial vehicles to turn left to both the service road and Onkaparinga Valley Road when exiting the site. This will be supplemented with an All Traffic Left sign at the exit from the site.

In the unlikely event that a driver of a commercial vehicle wished to travel north, an alternate travel route is available via Charles Street, Henry Street and Naughtons Road. The intersections of these roads are designed to accommodate semi-trailer movements and drivers will be able to safely navigate this route to then proceed north.

Accordingly, there will be no requirement for drivers of commercial vehicles associated with the site to turn right to Onkaparinga Valley Road at the southern service road intersection. That said, the proposed works to the service road will improve the ability for such a turn to be executed. Accordingly, while such a movement is not proposed in association with the subject development, the modifications to the service road will at least mean that this turn can be executed within the formed section of Onkaparinga Valley Road (inclusive of the shoulder), as illustrated in Figure 6.

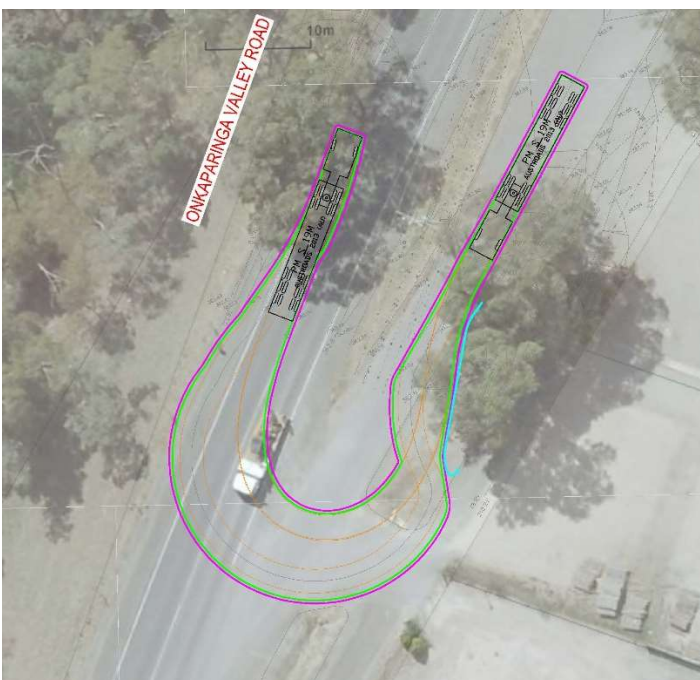


Figure 6: Right-turn movement to Onkaparinga Valley Road via the service road

The proposal will, therefore, also effect an improvement to the safety of existing movements at the intersection, should such a right turn be executed.

2.3 LOADING

Apples arriving from the orchard will be unloaded at the goods receive area located at the northern end of the site, adjacent the Naughtons Road ingress. Figure 7 illustrates a 19.0 m semi-trailer accessing the unloading area.

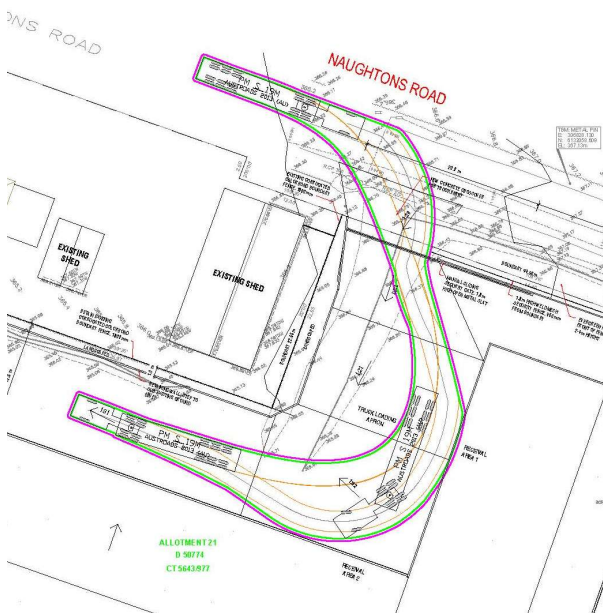


Figure 7: 19.0 m semi-trailer accessing goods receive area

Packed apples leaving the facility will be collected from the loading docks at the western end of the facility. Figure 8 illustrates a semi-trailer accessing the loading docks.

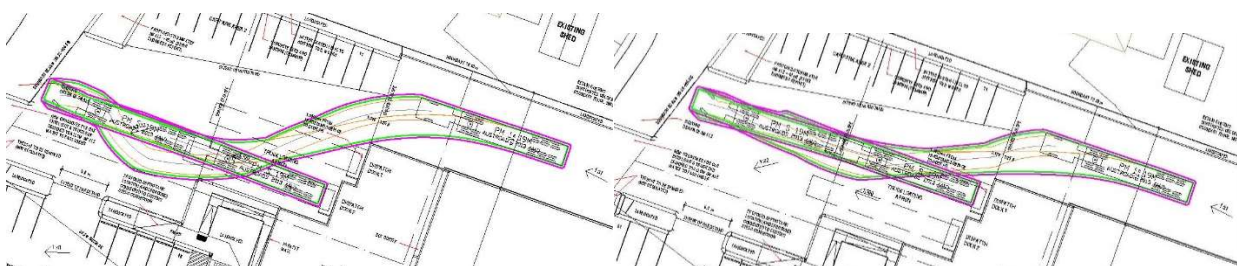


Figure 8: 19.0 m semi-trailers accessing the western loading area

Note the proposal does not seek independent access to the loading docks for semi-trailers but rather flexibility to load at either location. Two docks will not be used simultaneously. Importantly all delivery vehicles will be able to manoeuvre safely on the site and will enter and exit the site in a forward direction.



2.4 PARKING

Parking for the proposed development will be provided within the subject site. The parking area will comprise of 20 parking spaces. Additional parking will be provided along the northern boundary of the site and will be allocated to staff. The design of the parking spaces will generally comply with and will comply with *AS/NZS 2890.1:2004*.

The proposal will include one parking space for use by people with disabilities. This space will be 5.4 m long and 2.4 m wide and adjacent to a similarly sized shared space, as per the dimensional requirements of the Australian/New Zealand Standard, *Parking Facilities Part 6: Off-street parking for people with disabilities (AS/NZS 2890.6:2009)*.

3 PARKING ASSESSMENT

Adelaide Hills Council's Development Plan (consolidated 8 August 2019) identifies the following parking provision rate for industry, store and warehouse type developments:

- 3.3 spaces per 100 m² total floor area of the office component, plus for the non-office component:
 - 2 spaces per 100 square metres up to 200 square metres;
 - 1.33 spaces per 100 square metres between 200 and 2000 square metres; and
 - 0.67 spaces per 100 square metres over 2000 square metres.

Based on the above rates, the proposal will generate a parking requirement of:

- five spaces for the office component; plus
- 32 spaces for the non-office component.

If Council rates were adopted, therefore, there would be a total requirement for 37 parking spaces.

The above rates, however, would apply to a facility with a much higher staffing level than the subject proposal. In such circumstances, the most accurate methodology to assess the anticipated parking demand is to use a first principles assessment.

The proposed facility will be largely automated. The proposal will include two machines that will pre-size and sort the apples and three packing lines. The following staffing levels are anticipated to be required for each operation:

- two staff per machine for the pre-sizing and sorting operations which are less labour intensive; and
- four staff per packing lines.

Accordingly, there will be a demand for 16 staff when the facility is used to full capacity. If all staff were to drive to the site, there would be a requirement for 16 spaces.



In addition, there will be a requirement for the office and storage components of the proposal. In regard to the storage area, the Transport for New South Wales (formerly RMS) "Guide to Traffic Generating Development" specifies a rate of one space per 300 m² for warehouses. Based on this rate, the storage area will generate a requirement for three spaces.

It is expected that the proposal would, therefore generate a demand for 24 parking spaces. Such a demand would be readily accommodated for within the proposed parking provision.

4 TRAFFIC ASSESSMENT

The RMS guide specifies a traffic generation rate of 1 trip per 100 m² for a factory during the peak hour. Based on this rate, the proposal will generate approximately 40 trips during the peak hour periods.

In reality, the proposed development would not be expected to generate such a volume. Even if all staff were to arrive and/or depart during the peak hour, this would only equate to 24 trips. Deliveries to and from the site will only be approximately two to four trucks per day. Even if one arriving truck and one departing truck were to occur during a peak hour (which is not likely), there would still only be a forecast volume of 26 vehicles per hour (vph) during the peak period.

Notwithstanding the above, should a peak hour volume of 40 trips be realised, such a volume is still low and will have negligible impact on the adjacent road network.

5 SUMMARY

The proposed apple sorting facility will be designed to ensure that all vehicles would be able to safely enter and exit the site in a forward direction. Access and parking for the proposal will comply with AS/NZS 2890.1:2004 and AS 2890.2:2018.

The proposal has also considered the transport route to and from the site, given the existing substandard intersection of the service road with Onkaparinga Valley Road. Minor modifications to the service road will provide for safe turning movements of semi-trailers associated with the proposal. These modifications will also provide for improved safety for existing users of the service road.

Adequate parking spaces will be provided to cater for the anticipated peak parking demand associated with the development and the traffic generated by the proposal will have minimal impact and will not change the nature or function of the adjacent road network.

Yours sincerely,
MFY PTY LTD

MELISSA MELLEN
Director



2010 NATIONAL WINNER
2010 TELSTRA SOUTH AUSTRALIAN
BUSINESS WOMAN OF THE YEAR

Planning Statement

CONSTRUCTION OF FRUIT PACKING FACILITY WITH ANCILLARY
OFFICES, PARKING, RETAINING WALLS, BOUNDARY FENCING AND
LANDSCAPING

202 - 204 ONKAPARINGA VALLEY ROAD AND 21 NAUGHTONS
ROAD, WOODSIDE.



Prepared for Joyson Orchards

Report prepared by Adelaide Planning and Development Solutions

Contact Mark Kwiatkowski

Phone 0499933311

Email mark@adelaideplanning.com.au

AMENDED 14 January 2021



PLANNING STATEMENT

Lead Consultant Adelaide Planning and Development Solutions

In association with Beyond Ink

MFY Traffic Engineers

Project Manager

Mark Kwiatkowski

Ph 0499933311

Email mark@adelaideplanning.com.au

Document History and Status

Version	Date	Author
Version 1.0	November 2020	Mark Kwiatkowski APDS
Version 2.0	23 November 2020	Mark Kwiatkowski APDS
Version 3.0	25 November 2020	Mark Kwiatkowski APDS
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ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

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1.0 Application Overview

Applicant	Joyson Orchards
Property Address	202 - 204 ONKAPARINGA VALLEY ROAD AND 21 NAUGHTONS ROAD, WOODSIDE
Description of land	CTs Volume 5643 Folio 977, Volume 5440 Folio 810, Volume 5440 Folio 920
Site area	Approximately 7650 Square Metres
Development Plan	Adelaide Hills Council Development Plan Consolidated 8 August 2019
Zone	Light Industry Zone
Policy area	Light Industry (Woodside) Policy Area
Development Plan maps	AdHi/18
Existing land use	Vacant land
Development proposal	Construction of fruit packing facility with ancillary offices, parking, retaining walls, boundary fencing and landscaping.
Public notification	To be advised by Council.
Referrals	DIT
Relevant Authority	Adelaide Hills Council
Primary contact person	Mark Kwiatkowski Mark@adelaideplanning.com.au 0499933311



ABN 55 289 434 618
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 Town Planning Specialists and Planning Private Certifiers
 e: mark@adelaideplanning.com.au
 ph: 0499933311
 w: www.adelaideplanning.com.au
 a: 52A Mount Barker Road, Hahndorf SA 5245

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2.0 Introduction and Background

2.1 Introduction

This Planning Statement has been prepared by Adelaide Planning and Development Solutions (APDS) on behalf of Joyson Orchards.

Pursuant to Map AdHi/18, the subject land is located within the Light Industry (Woodside) Policy Area of the Light Industry Zone of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

In preparing this supporting statement, I can confirm that I have reviewed the proposal plans prepared by Beyond Ink, supporting documents along with the relevant provisions of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

I have also inspected the subject land and locality.

2.2 Preliminary discussions with council

A preliminary meeting was held with Council in relation to the project which considered a number of matters including

- Nature of development and proposed built form
- Access and egress arrangements including traffic movements
- Acoustic considerations
- Relocation of the Lions park bench and access to Onkaparinga Valley Road for the subject land over the Amy Gillet Bike Path
- Impacts of the proposal on road network
- Impacts on trees and tree removal
- Stormwater implications of the proposal and removal of easement

On this basis, the applicant proceeded with the development of the proposal in more detail ready for the submission of a formal planning application.

2.3 Discussions with adjoining landowners

As part of the design process, all adjoining landowners were consulted with and allowed to add input to the design process. As a result, all neighbours have provided in principle support of the proposal which has been supplied with the application documentation.



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

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3.0 Subject Land and Locality

3.1 Subject Land



Figure 1 Subject land identified in yellow

The subject site has a street address of 202 - 204 Onkaparinga Valley Road and Naughtons Road, Woodside.

The site is located in the Light Industry (Woodside) Policy Area of the Light Industry Zone of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

The site is an irregular L shaped site consisting of three allotments with a site area of approximately 7650 square metres. The site has frontage of 44.4 metres to Naughtons Road and a frontage of 52.3 metres to Onkaparinga Valley Road.

The property of 21 Naughtons Road Woodside has vehicle access via Naughtons Road, while the properties of 202 and 204 Onkaparinga Valley Road do not have any existing vehicle access.

Onkaparinga Valley Road is an arterial road within the care and control of the Commissioner of Highways. It has an annual daily volume of 9,300 vehicles and a posted speed limit of 60 km/h adjacent the subject site.

Allotment 21 of Deposited Plan 50774 previously had an easement registered on the title for drainage purposes along the allotments north-east boundary for a length of 28.15m. The remaining allotments do not have any easements registered on titles.

The site has a slope from the rear of the site to the front of the site with flat sections towards the front portions of the site.

The subject land has connection to all services (power, water, gas, electricity, telephone).



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Adelaide Planning & Development Solutions Pty Ltd
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e: mark@adelaideplanning.com.au
ph: 0499933311
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a: 52A Mount Barker Road, Hahndorf SA 5245

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3.2 Locality



Figure 2 Locality Plan

The locality consists of a mix of commercial / light industrial and rural living associated with the different zoning.

The allotments to the south and east of the subject land contain a range of commercial land uses including storage facilities, cabinet makers, light industrial land uses, food stores / agronomy store on larger commercial allotments. Further commercial land uses along Onkaparinga Valley Road include an irrigations supplier, camper trailer repair and sales, and a car yard as part of the Woodside North Business Park.

The allotment abutting the subject land to the north includes a dwelling with an additional outbuilding to the rear which is used as a motorcycle mechanic within the Light Industry Zone.

A dwelling is located on the opposite side of Naughtons Road to the north within the Watershed (Primary Production) zone and a further dwelling with outbuildings is located on the allotment on the opposite side of Onkaparinga Valley Road also within the Watershed (Primary Production) Zone, both which are screened from the subject land with extensive vegetation to the periphery of their sites.

The Amy Gillet cycle path and the Lions Gazebo and Chairs are located within the verge between the subject land and Onkaparinga Valley Road.



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e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

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4.0 Planning Assessment

4.1 Nature of Development

The application seeks the construction of fruit packing facility with ancillary offices, parking, retaining walls, boundary fencing and landscaping.

The building will be the home of ADELAIDE HILLS FRESH and will also be the administration Head Office for the Joyson Group of companies which includes:

- Joyson Orchards
- Balhannah Nurseries
- Fleurieu Avocados
- Lenswood Estate Wine
- Jak Joy Livestock

The plant itself will bring new technology to the region with cost benefits to more efficiently increase production and improve profitability right back to the grower.

The proposed use of the facility involves the storage and handling of primary produce (apples) which will be transported to and from the facility. The volume and nature of produce involved are such that it requires the use of heavy vehicles, and large dry and cool room storage area and receipt and dispatch processing area. The facility must also include elements that facilitate the entry, unloading, loading and exit of these large vehicles.

The proposed facility will involve

- Apples arriving from the orchard will be unloaded at the goods receipt area located at the northern end of the site, adjacent the Naughtons Road ingress
- Deliveries to and from the site will only be approximately four to six trucks per day during non-peak time and six to ten trucks per day during peak picking months.
- Collected fruit of differing size and quantities will be delivered to the site and placed into the processing machine. This equipment has been designed with substantial automation to limit the number of workers required from grading through to the packaging lines before dispatch via cold storage trucks from the dispatch bays.
- The fruit will be sorted by size and then stored in either the general storage area for short term storage or cool rooms for medium to long term storage. Bulk apple storage is accommodated from the sorting lines to the large pre-sized cool storage room with electric forklifts.
- The facility will store around 40 to 50 tonnes of produce on the floor at any time.
- The facility will process between 2 - 4 dispatch semi-trailers per day with an average load of 40 to 50 tonnes of produce from the site daily.



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e: mark@adelaideplanning.com.au
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- The size and type of produce for dispatch will vary according to season and the orders for the produce. Some produce will be loaded onto the dispatch vehicles directly from the daily incoming collection trucks, with others will be loaded with a combination of stored and daily collected produce.
- Packed apples leaving the facility will be collected from the loading docks at the front of the facility.
- Once packed, drivers of vehicles loaded with boxed apples will travel to the South-Eastern Freeway to distribute the goods
- Waste will be picked up twice per week or as required.

More specifically, the proposal includes:

- Construction of a large commercial building with an area of 3727 square metres comprising of
 - Receiving area for fruit (predominately apples)
 - Packing and processing area
 - Office area in the south western portion of the site
- A total of 28 carparking spaces in the south western and north western corners of the subject land.
- The building will be setback 3 metres from the southern boundary, 3.9 metres from the eastern boundary at the rear and 12.9 metres to the internal northern boundary, 7.9 metres to Naughtons Road, and a setback of 38.5 metres to the western boundary (Onkaparinga Valley Road)
- Landscaping to the western boundary and along the northern boundary and within the carparking area.
- One way access proposed through the site from Naughtons Road and exit onto Onkaparinga Valley Road.
- Retaining walls along the eastern, southern, and northern boundaries.
- 2.8 metre high acoustic boundary fencing along the northern boundary on the common boundary of the adjoining residential / commercial property
- One freestanding sign in the northern garden bed, and two fascia signs to the main building.
- Consolidation of the three titles into one title.
- Alterations to the service road (outside of the subject land)
- In terms of operation the facility will operate differently during different periods of the year.

Non-peak (May – January)

Full business operation generally Monday to Friday

Only urgent or occasional supermarket truck deliveries on the weekends or after hours Truck arrivals generally 7.00am to 2.00 pm Mon – Fri departures generally 2.00-4.30 pm Mon-Fri.

During this period, it is likely there will be 4 to 6 truck movements per day.

Peak picking season (February-April)

Full business operation generally Monday-Friday and Saturdays for arrivals to cold room storage.

Trucks arrivals generally 7.00am to 2.00 pm Mon – Fri, departures generally 2.00-6.00 pm Mon-Fri



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Some arrivals for cold room storage on Saturday between 7.30-3:30 pm during peak picking time

During this period, it is likely there will be 6 to 10 truck movements per day

Office Hours of operation (All year round)

Monday to Friday - Staff arrivals from 6.30am to 6.00pm

Staff parking - entry via internal service road with parking in staff car park in south western corner of the site.

We would therefore seek the hours of operation five (5) days per week (Monday to Friday) between the hours of 6:30 am to 7:00 pm and Saturday between 7.00 am to 5:00 pm during peak picking time to enable the delivery and collection of produce from the Adelaide Hills region to the facility, and processing of pallets of produce for distribution to local, interstate and international markets.

- The office designed to accommodate 8-10 staff and processing 6-14 (seasonally dependent) with remaining for (infrequent) visitor parking. For the majority of the time there will be no more than 20 staff onsite.
- The only other people on site will be drivers either delivering produce to the facility that has been collected from growers in the region and dispatch drivers for the delivery of the produce.
- Details of the external colour scheme include:

Wall, Tilt-Up Panels = Precast external walls grey smooth finish (as used on all elevations);

Wall, Metal Sheet Cladding = Revolution Roofing 'Maxline' profile, installed vertically, Nextstar 'Kalkajaka' colour (as used on north, south and west elevations);

Sun Shade Blades = Anodised aluminium 'bronze';

Gutters = Nextstar 'Kalkajaka' colour;

Roof, Metal Sheet Cladding = 'Pan' profile, Nextstar 'Hotham' or Colorbond 'Surfmist' colour.

Retaining Walls = Precast sleepers, grey smooth finish;

Roller Doors = Zinalume roller curtain;



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e: mark@adelaideplanning.com.au
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4.2 Kind of Development

Pursuant to Map AdHi/18, the subject land is located within the Light Industry (Woodside) Policy Area of the Light Industry Zone of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

The Adelaide Hills Council Development Plan Consolidated 8 August 2019, Light Industry Zone, lists a Fruit Packing Facility as neither a complying or non-complying form of development, therefore pursuant to Regulation 16 of the Development Regulations, the development should be processed via the merit pathway.

In my view, the proposal is acceptable when balanced against all the relevant provisions of the Development Plan, the existing Light Industry Zoning of the site, recent development along in the immediate locality Industry Zone and the intent of the relevant provisions of the Light Industry (Woodside) Policy Area of the Light Industry Zone.

This will be explored within the content of this report.



ABN 55 289 434 618
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Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
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4.3 Development Plan Assessment

4.3.1 Light Industry Zone

Objective 1 A zone accommodating a range of light industrial, storage and warehouse land uses.

Objective 2 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone will be an intensively developed, high quality, landscaped industrial area. The zone will accommodate a wide range of low-impact industrial activities including manufacturing, warehousing, transport, and distribution. The zone will be protected from the intrusion of residential and other inappropriate uses which will reduce the land resource for industrial uses or create potential for land use conflicts. High impact industrial uses will not be located within the zone.

Development within the zone will achieve generous set-backs from roads and residential development in adjoining zones in order to minimise visual amenity and environmental impacts. The appearance of the zone will be improved by additional tree planting and landscaping which 'break-up' views to buildings and structures from adjoining roads. Where industrial development is proposed adjacent to a more sensitive use or along arterial roads, substantial and effective vegetated buffers will be provided within individual development sites to reduce the visual and environmental impact of the development.

Industrial buildings will be designed to meet the needs of their intended use, however the mass and scale of buildings will be located and designed to minimise their visual impact when viewed from public roads and surrounding properties.

Building mass will be well articulated, using smaller building modules, variation in the facades and varying roof form and pitch. Building material and colour will reduce the apparent bulk of the buildings and will enhance the nature of the surrounding area, particularly for sites that are more publicly visible.

The Mount Lofty Ranges Watershed Area is of importance to Adelaide's public water supply system. The maintenance and enhancement of water quality and prevention of pollution is a priority and given the multi-use nature of the water supply catchments, a balance between best practice watershed protection and development is required.



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AMENDED 14 January 2021

Light Industry is defined in Schedule 1 (definitions) of the Development Regulations 2008 as

An industry where the process carried on, the materials and machinery used, the transport of materials, goods or commodities to and from the land on or in which (wholly or in part) the industry is conducted and the scale of the industry does not—

(a) detrimentally affect the amenity of the locality or the amenity within the vicinity of the locality by reason of the establishment or the bulk of any building or structure, the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit, oil, spilled light, or otherwise howsoever; or

(b) directly or indirectly, cause dangerous or congested traffic conditions in any nearby road;

With regard to the above Objectives and Desired Character statement of the Light Industry Zone, it is considered that the proposal satisfies the intent of the Development Plan in that:

- The proposal will result in a well-designed and articulated commercial building which provides a range of articulation and design elements to Onkaparinga Valley Road frontage with generous setbacks from front and side boundaries allowing for landscaping and a form which sits comfortably on the subject land.
- The proposal results in a light industrial land use as per the definition above as defined by Schedule 1 (definitions) of the Development Regulations 2008. The land use as a fruit packing facility with ancillary offices, parking, retaining walls and landscaping is in keeping with the intent of the Light Industry zone which allows for transport and distribution, warehousing and storage and other land uses which will not have a high impact on the amenity of adjoining land uses.
- The proposal has been designed to ensure no impact to Adelaide's public water supply system through an appropriately designed stormwater and waste system connecting to the existing infrastructure afforded to the subject land and wider locality.
- Appropriate landscaping is proposed to the periphery of the site which will aid in enhancing the appearance of the subject land whilst retaining trees to the periphery of the site and not impacting on adjoining trees.
- Further, the building will complement the appearance of the adjoining commercial buildings in the Woodside Business Park and will provide additional landscaping on the site.

Principle of Development Control 1 The following forms of development are envisaged in the zone:

- *light industry*
- *office in association with and ancillary to industry*



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AMENDED 14 January 2021

Principle of Development Control 3 Development should not be undertaken unless it is consistent with the desired character for the zone.

The proposal is considered to be a suitable light industrial land use within the Light Industry Zone and provides a primary land use which falls under the broad spectrum of a light industrial land use and is suitable as per the above considerations in relation to the Objectives and Desired Character statement of the Light Industry Zone.

Principle of Development Control 4 Development should be set back at least 8 metres from any road frontage, except where fronting an arterial road in which case it should be set back at least 20 metres.

The design of the building is setback 38.5 metres from the front property boundary which results in a built form which satisfies the intent of the above Principle of Development Control.

Principle of Development Control 5 Advertisements and advertising hoardings should not include any of the following:

- (a) flashing or animated signs*
- (b) bunting, streamers, flags, wind vanes and similar*
- (c) roof-mounted advertisements projected above the roofline*
- (d) parapet-mounted advertisements projecting above the top of the parapet.*

The proposed advertising in the form of a small front pylon sign, and fascia signage to the building will result in advertising / signage which will not be animated or project above the roofline of the proposed built form. A further and more detailed assessment of the suitability of the signage will be contained within the Council Wide – Advertisements assessment, but it is considered that the signage proposed is suitable.

4.3.2 Light Industry (Woodside) Policy Area

Objective 1 Provision for the development of light industrial activities.

Objective 2 The preservation of the rural character especially as viewed from Onkaparinga Valley Road.

Objective 3 Development that contributes to the desired character of the Policy Area.

DESIRED CHARACTER

The Light Industry (Woodside) Policy Area forms part of the travellers' approach to Woodside from the north. As a gateway to the town it is important that development in this area enhances the area's visual appeal.



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e: mark@adelaideplanning.com.au
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Views from the Onkaparinga Valley Road will reflect a vigorous local economy and developments with a form that enhances the overall views and rural character. Large buildings will be articulated to avoid blank walls, and landscaping will be used to break up the visual appearance of large roofs and structures.

Management of vehicles and access will avoid conflict with road users on the Onkaparinga Valley Road.

Signage will enhance the appearance of the policy area and views across the town by grouping occupant signs near the main road, and by locating smaller individual business identification signs above entry doors viewable from internal roadways.

With regard to the above Objectives and Desired Character statement of the Light Industry (Woodside) Policy Area, it is considered that the proposal satisfies the intent of the Development Plan in that:

- The proposal will result in a well-designed and articulated commercial building which provides a range of articulation and design elements to Onkaparinga Valley Road with generous setbacks from front and side boundaries allowing for landscaping and a form which sits comfortably on the subject land resulting in a development which enhances the area's visual appeal. .
- The light industrial land use as a fruit packing facility with ancillary offices, parking, retaining walls and landscaping is in keeping with the intent of the zone which allows for transport and distribution, warehousing and storage and other land uses which will not have a high impact on the amenity of adjoining land uses.
- Appropriate landscaping is proposed to the periphery of the site which will aid in enhancing the appearance of the subject land whilst retaining trees to the periphery of the site and not impacting on adjoining trees.
- Signage proposed has been designed in keeping with the Council Wide provisions as they relate to Advertisements and will result in a freestanding pylon sign and fascia signage to the building will result in advertising / signage which will not be animated or project above the roofline of the proposed built form.
- Appropriate access will result in avoiding conflict with road users on the Onkaparinga Valley Road as outlined in the traffic assessment by MFY Traffic Consultants.

Principle of Development Control 1 The following forms of development are envisaged in the Policy Area:

- *light industry*
- *office in association with and ancillary to industry*
- *store*
- *warehouse.*



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

AMENDED 14 January 2021

Form and Character

Principle of Development Control 2 Development should not be undertaken unless it is consistent with the desired character for the Policy Area.

The proposal is considered to be a suitable land use within the Light Industry (Woodside) Policy Area and provides a primary land use which falls under the broad spectrum of a light industrial land use and is suitable as per the above considerations in relation to the Objectives and Desired Character statement of the Light Industry Zone.

Principle of Development Control 3 Industrial development should only be undertaken if adequate measures are taken to ensure that all waste and effluent is removed from the site or treated to prevent pollution of the water catchment area, and all premises are connected to the sewer.

An appropriately designed waste system has been designed which will ensure that all waste and effluent is removed from the site or treated to prevent pollution of the water catchment area, and the premises will be connected to the sewer in keeping with the intent of the above Principle of Development Control.

Principle of Development Control 4 Development fronting Onkaparinga Valley Road should be suitably landscaped along that road frontage to minimize the visual impact of that development from the road.

The proposal includes two large garden beds along the Onkaparinga Valley Road frontage which will provide a suitable area for the provision of landscaping which will aid in minimising the visual impact of the proposed built form noting that development in the immediate locality within the Light Industry (Woodside) Policy Area.

Principle of Development Control 5 Buildings should be sited to have minimal interference with existing trees.

A predevelopment Arboricultural Assessment has been provided by The Adelaide Tree Surgery which has considered the health of the trees contained within the north western corner of the site and on the adjoining site to the north of the subject land.

A total of five trees were identified within the report including the location of two regulated River Red Gum trees to the front of the site, a further two small gum trees and an Elm tree on the adjoining site to the north.

A response from Sharon Gillam from the Native Vegetation Branch, Department for Environment and Water has considered the removal of two gum trees which indicates



ABN 55 289 434 618
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w: www.adelaideplanning.com.au
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AMENDED 14 January 2021

The two small gums trees that have been recommended for removal are able to be removed under the Native Vegetation Council's Guidelines for the Management of Roadside Native Vegetation and Regrowth Vegetation, under Native Vegetation Regulation 11(23) (2019), Part 3, Section 5. Clearance for access to adjoining land, where the clearance is required for normal vehicle access and is no more than 5m wide.

In relation to the regulated trees, the report indicates

The two River Red Gums will not be affected by the proposed development which includes the driveway entrance and driveway past the Protected Trees for the proposed Packaging Warehouse. The encroachment into both River Red Gums Tree Protection Zones (TPZ.s) have been calculated at less than 10% each.

The Elm Trees on the adjoining site to the north will be removed as per discussions between the adjoining landowner and the applicant.

Further recommendations in relation to Tree Protection Zones during the development phase and permeable paving are recommended in the report which will ensure that the health of the trees will not be affected.

On this basis, it is considered that the proposal will protect the health of the existing trees and will provide additional landscaping to the site which will offset the removal of the two street trees.

4.3.3 Council Wide Section

4.3.3.1 Council Wide – Advertisements

The relevant Council Wide provisions seek advertisements which enhance the appearance of the building and locality, will not cause hazards and provide signage which will ensure that the urban landscape is not disfigured by advertisements and/or advertising hoardings.

With regard to the relevant Council Wide provisions, the proposed signage on the fascia's of the buildings and the proposed freestanding sign are considered to satisfy the intent of the Development Plan in that the signs will not disfigure the urban landscape, will not create a hazard and will enhance the appearance of the buildings and the immediate locality. The proposed signage will result in a coordinated appearance over the site and will complement the architectural form of the proposed built form. The fascia signage associated with the proposed built form will not result in clutter, disorder, or untidiness on the buildings and will sit comfortably on the site and will identify the proposed land use and will also reflect the signage in the wider commercial locality.



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AMENDED 14 January 2021

Further, the signage will not cause discomfort to approaching drivers, or to create difficulty in the driver's perception of the road, will be easily interpreted, will not distract drivers, and will not obstruct views of pedestrians or the road activities

On this basis it is considered that the signage is in keeping with the intent of the signage provisions envisaged in the Development Plan.

4.3.3.2 Council Wide – Design and Appearance

The Council Wide - Design and Appearance Objectives and Principles of Development Control seek well-designed buildings which provide a high standard of design using a range of colours and materials complimenting the built form and rural / hills face environment.

The application proposes a well-designed industrial building incorporating a range of architectural elements which integrate of a range of colours and materials and articulation to add visual interest. The proposal will complement the existing industrial environment and sit comfortably on the site with appropriate siting and landscaping in keeping with the intent of the Light Industry Zone and more specifically the Light Industry (Woodside) Policy Area.

The proposal will provide a built form on the site with appropriate setbacks from boundaries and proposes a formalised parking area to the front portion of the building and allows for suitable traffic movements through the site. The proposal will result in acceptable impacts on adjoining properties and the articulated built form will not result in an inappropriate visual impact when viewed from the immediate or wider locality.

The massing of the building is carefully managed, firstly through the size which presents a built form to what would reasonably be expected within the Light Industry Zone with a commercial appearance towards the rear of the site matching adjoining large, shed structures in association with the processing areas and an articulated built form to the front of the site as it relates to Onkaparinga Valley Road associated with the office component.

Details of the external colour scheme are as follows:

- Wall, Tilt-Up Panels = Precast external walls grey smooth finish (as used on all elevations);
- Wall, Metal Sheet Cladding = Revolution Roofing 'Maxline' profile, installed vertically, Nextstar 'Kalkajaka' colour (as used on north, south and west elevations);
- Sun Shade Blades = Anodised aluminium 'bronze';
- Gutters = Nextstar 'Kalkajaka' colour;
- Roof, Metal Sheet Cladding = 'Pan' profile, Nextstar 'Hotham' or Colorbond 'Surfmist' colour.
- Retaining Walls = Precast sleepers, grey smooth finish;
- Roller Doors = Zinalume roller curtain;



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AMENDED 14 January 2021

The proposed building has a form that is consistent with the building height of adjoining properties and provides the use of a range of materials providing variation in wall and roof lines proposed, wall materials and colours. The development as a whole including driveways and parking area has been designed to ensure the structures are appropriately located to allow for access through the site for pedestrians and vehicles whilst being functional. The design of the site has been focused on creating a safe, well-designed pedestrian environment and a functional space.

The proposal has been designed with facades which avoid extensive areas of uninterrupted walling facing areas exposed to public view. The external walls and roofs of the additions proposed buildings will not incorporate highly reflective materials and will not result in glare to neighbouring properties, drivers, or cyclists. The building has been designed to complement and harmonise with the character of the existing approved buildings in the immediate locality.

It is considered that the proposed building has been designed to satisfy the requirements of the Development Plan in relation to design and appearance and will result in an appropriate form which will sit comfortably within the Woodside North Business Park and Onkaparinga Valley Road streetscape complementing the existing built form on adjoining sites.

4.3.3.3 Council Wide – Bulk Handling and storage facilities

The Council Wide - Bulk Handling and Storage Facilities Objectives and Principles of Development Control seek developments which have been designed to minimise adverse impacts on the landscape and on and from surrounding land uses whilst being located within appropriate zones.

The proposal results in a development which is located within the Light Industry Zone and has been designed and sited to minimise risks of contamination to the environment and adverse impacts on nearby sensitive land uses and from surrounding land uses.

The proposal has been designed to allow for suitable access and manoeuvring of vehicles through the site providing appropriate vehicle circulation between activity areas through the site, offers a sealed internal driveway and parking areas (parking areas will be gravel or granitic sand or other) to control dust emissions from the site. Vehicles will enter the site from Naughtons Road and exit the site via Onkaparinga Valley Road ensuring vehicles enter and exit in a forward manner.

The proposal provides appropriate landscaping to the periphery of the site for the purpose of providing shade and shelter assisting with screening and dust filtration to adjoining properties and the street. Further, appropriate fencing is proposed which will aid in providing adequate acoustic buffering in addition to the security of the perimeter of the site.



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e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
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AMENDED 14 January 2021

4.3.3.4 Council Wide – Crime Prevention

With regard to the Council Wide section – Crime Prevention Objectives and Principles of Development Control relating to Crime prevention, the design of the proposal provides a fruit packing facility with ancillary offices which will allow increased casual surveillance from users and traders on the site.

The location and design of the main building and open nature of the building layout on the site allows for clear sightlines though the site with passive surveillance over the car park and internal driveway from the building and surrounds.

The materials proposed will provide a robust environment that is resistant to vandalism and graffiti and will withstand normal use. The design building and additions minimises areas for vandalism and graffiti. In any event, should the buildings be vandalised, this would be immediately removed from the site by the tenants.

Whilst the final detail of the lighting to the carparking area has not been finalised, all lighting of the site will be of a low level and designed in a manner to ensure that the subject land is appropriately lit whilst not impacting on the amenity of adjoining land uses. Any lighting used at the site will comply with AS 4282-1997-Control of the Obtrusive Effects of Outdoor Lighting, and AS 1158.1 Public Lighting Code for the illumination level of the car parking area

Access to the buildings from the carpark and internal driveways will not result in pedestrian entrapment spots and the open nature of the parking area will allow for casual surveillance of the site.

It is considered that the proposal will result in a safe, secure, crime resistant environment which satisfies the intent of the Development Plan in relation to Crime Prevention.

4.3.3.5 Council Wide – Hazards

With regard to the Council Wide section – Hazards, the proposal should be designed to ensure the maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk, whilst being located and designed to minimise the risks to safety and property from flooding.

A catchment analysis has been provided by Mlei Consulting Engineers in addition to a stormwater plan with calculations.

The catchment analysis report indicates



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
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ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

AMENDED 14 January 2021

MLEI Consulting Engineers were engaged to do a catchment analysis for the network contributing to a discharge location at the corner of Naughtons and Onkaparinga Valley Roads. The purpose of the investigation is to determine the change in peak flows as a result of the increased runoff from the development of the proposed apple facility and any increased burden on the existing stormwater infrastructure.

Stormwater peak flow rates have been determined for both the existing site and the proposed development. There is expected to be an increase in the total peak flow for the proposed development when compared to the existing site. Looking at the overall catchment at the point of discharge, the site is unlikely to have a major impact on overall peak flow based on the proximity to outlet and the upstream catchment's time of concentrations.

The volumes are computed by using DRAINS modelling. The calculations are based on the Horton/ILSAX method using the 1 in 20 year (5% AEP) peak storm to determine the runoff of stormwater from the site.

The results of the analysis find a negligible increase (0.23%) in peak flow from the catchment in the post-development scenario of the Woodside Apple Facility, in comparison to the current rural state of the site for the 5% AEP storm event.

Based on the findings of the analysis and the location being within several metres from the Onkaparinga River and no directly downstream properties, it would seem appropriate for unrestricted discharge of stormwater from the development, providing safe stormwater management practices are in place for major events. The expectation from the results is that the development will not create a sizeable increased burden to the existing network.

On this basis, it is considered that the proposal will satisfy and meet the requirements of the Development Plan in relation to the impacts on flooding and will not adversely affect flood levels or create a sizeable increased burden to the existing network.

4.3.3.6 Council Wide – Interface between land uses

The Council Wide - Interface between Land Uses seeks development that does not cause unreasonable interference, particularly referencing the emission of effluent, odour, smoke, fumes, and dust or other airborne pollutants, noise, hours of operation and traffic impacts.

Further the development plan seeks development to be designed and sited to minimise negative impact on existing and potential future land uses considered appropriate in the locality and be designed, constructed, and sited to minimise negative impacts of noise and to avoid unreasonable interference.



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AMENDED 14 January 2021

The proposed hours of operation of the packing facility will generally be from 6:30 am until 7:00 pm Monday to Friday with additional hours during peak season from 7:00 am to 3:30 pm on Saturdays as outlined in section 4.1 of the report.

The proposed nature of the proposal within the Light Industrial Zone and proposed hours of operation will have an acceptable impact on the adjoining locality and is consistent with the hours of operation of other non-residential land uses in the immediate and wider locality and the hours envisaged by the Environment Protection (Noise) Policy 2007.

Within areas of the Light Industry Zone where there are commercial activities, it is envisaged that a range of impacts will occur to adjoining properties including noise, traffic, hours of operation and will be more significant when compared with a traditional residential area during certain periods of the day. When considering the impacts of the proposal, it should be noted that two residential allotments are located in close proximity to the subject land, one of which abuts the site to the north and one on the opposite side of Naughtons Road. There is also a residential allotment on the opposite side of Onkaparinga Valley Road.

A preliminary review of the proposal has been undertaken by Sonus to determine existing operational noise levels from the existing facility at Monarto and collection of noise from the Woodside site to inform the influence of Onkaparinga Valley Road on the existing environment. The preliminary review indicated

The site is located in a Light Industry Zone of the Adelaide Hills Council Development Plan.

The nearest residence, being the one adjacent to the site to the north, is also in the Light Industry Zone. The next nearest residence on the opposite side of Naughtons Road is in a Watershed (Primary Production) Zone.

Based on this zoning, the Environment Protection (Noise) Policy 2007 (the Policy) provides noise level criteria to be achieved at the nearby residences. The criteria are influenced by different factors, such as the zoning, the development status of the project and the character of the noise and compliance with the criteria ensures compliance with the Development Plan provisions relating to noise.

The goal noise levels to be achieved by operations at the site when considered at the nearest dwellings are:

- 47 dB(A) to be achieved during daytime operation, from 7am to 10pm
- 40 dB(A) to be achieved during nighttime operation, before 7am and after 10pm.



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AMENDED 14 January 2021

The Policy enables the contribution of other noise sources such as Onkaparinga Valley Road to be taken into account. Monitoring was conducted at the site between 2 September and 16 September. The lowest background noise level (the noise level during lulls in traffic) measured during the week, i.e. Monday to Friday, between 6:30am and 7:00am was 43 dB(A).

In this circumstance, the Policy enables the criteria for the night period to be increased to 43 dB(A) (during the week).

The proposal provides a 2.8 metre high acoustic fence to the northern boundary and additional landscaping in the form of compact pencil pines which will result in a suitable acoustic screen providing a suitable acoustic barrier to the adjoining residential property.

From an operational perspective, electric forklifts only, removing forklift beeps and replace with low flashing lights and fitted with white noise muffler filters and trucks and truck refrigeration units be turned off during unloading and loading.

The majority of the activities associated with the fruit packing facility will be contained within the proposed building with the exception of the movement of vehicles for loading and unloading. The proposed activities and hours of operation will ensure there will be limited noise or light spill impacts to the adjoining allotments and will have an acceptable impact in keeping with the EPA Noise Policy.

It is considered that the noise levels at dwellings in the wider locality will not be impacted by the proposal and the noise when measures from any dwellings from activity at the site is predicted to achieve the conservatively applied requirements of the Environment Protection (Noise) Policy 2007.

The proposal will have an acceptable impact on the existing road network which is consistent with other light industrial land uses in the immediate and wider locality. The proposed access point from Naughtons Road and egress to Onkaparinga Valley Road will not result in additional detrimental impacts from traffic movements to adjoining properties and will be consistent with the current existing traffic movements along Naughtons Road and Onkaparinga Valley Road. Further the formalisation of the parking area will result in safe and convenient access and parking through the site.

With regard to lighting, whilst the final detail of the internal lighting to the site have not been finalised, it is likely that the site will be lit with low level lighting surrounding the building which will be designed in a manner to ensure that the subject land is appropriately lit whilst not impacting on the amenity of adjoining land uses including along pedestrian pathways and main access routes in accordance with Australian Standard AS 4282-1997 Control of the obtrusive effects of outdoor lighting.



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Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

AMENDED 14 January 2021

With regard to the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants, vibration, electrical interference and glare, the proposal will have an acceptable impact given distances to adjoining properties.

On balance, It is considered that the proposal will have an acceptable impact on the amenity of the adjoining properties in accordance with the Council Wide requirements of the Development Plan as it relates to Interface between Land Uses.

4.3.3.7 Council Wide – Landscaping, Fences and Walls

The Council Wide – Landscaping, fences and walls seeks that the amenity of land and development being enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible as well as functional fences and walls that enhance the attractiveness of development

As aforementioned, the proposal will result in the construction of a 2.8 metre high acoustic fence to the northern boundary abutting the adjoining residential property and further 1.8 metre high security fencing to the periphery of the site to maintain privacy and security without adversely affecting the visual amenity or access to sunlight of adjoining land. The fencing height to the adjoining boundary with the residential property was at the request of the landowner and will result in an appropriate acoustic screen between the two properties. Naughton Road entry will also be secured with an automatic sliding gate which will be remotely operated and remain closed during no operational hours.

Further retaining walls and fencing will be constructed along the eastern portion of the site to retain the proposed cut into the land.

Provision for suitable landscaping areas have been provided which identifies the areas of landscaping and provides for a range of species to be planted including trees, shrubs and ground covers which will provide adequate screening and designed to not create concealment points. The landscaping will aid in complementing the built form and reducing the visual impact of the buildings whilst screening service yards, loading areas, outdoor storage areas. Appropriate species will be selected which require minimal maintenance requirements and watering requirements, promote water and biodiversity conservation, and will result in appropriate clearance from power lines and other infrastructure.

On this basis, it is considered that the proposal has been designed to satisfy the relevant provisions of the Development Plan as they relate to Landscaping, Fences and Walls.



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

AMENDED 14 January 2021

4.3.3.8 Council Wide – Orderly and Sustainable Development

The proposal on the site is considered appropriate and orderly and consistent with the relevant provisions of the Development Plan in that

- The proposal expands the economic base of the region in a sustainable manner and value adds to range of land uses within the Light Industry Zone on the Woodside North Business Park.
- The proposal which will ensure a safe convenient and pleasant environment for uses and adjoining landowners and provides significant investment to the locality.
- The proposal does not prejudice the zone for its intended purpose as outlined in the above assessment against the Zone provisions and is consistent with the intended of the site and adjoining land uses within the Light Industry Zone and adjoining Watershed (Primary Production) Zone.
- Will not jeopardise the mix of land uses in the surrounding allotments,
- The site will utilise the existing facilities and services to the site (power, access to roads etc.) and will not jeopardise the continuance of the adjoining land uses within the Light Industry Zone and adjoining Watershed (Primary Production) Zone
- The proposed development does not undermine the objectives of the zone and protects the character and amenity of the light industrial character of the locality.
- Provides safe and convenient traffic movements and sufficient on-site car parking.
- Results in a development which retains and protects the natural resources and environment.
- Provides an environmentally sustainable and innovative tourism development that does not adversely affect the use of the land or adjoining properties.

On this basis it is considered that the proposal satisfies the above provisions in relation to orderly and sustainable development.

4.3.3.8 Council Wide – Transportation, access, and parking

The relevant transportation and access objectives and principles promote development that provides for the safe and efficient movement of all motorised and non-motorised transport modes. Given the small-scale nature of the amendments to the development and the hours of operation proposed, the proposal will not result in unacceptable traffic movements to and from the site.

A review of the proposal has been undertaken by MFY Traffic Consultants in relation to the traffic and parking aspects of the proposal and provided advice in respect to access requirements noting that the proposal has been discussed with the Department of Infrastructure and Transport (DIT) and Council.



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Adelaide Planning & Development Solutions Pty Ltd
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e: mark@adelaideplanning.com.au
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w: www.adelaideplanning.com.au
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AMENDED 14 January 2021

The report indicates

- Access for the proposal will be provided via Naughtons Road and the service road. The Naughtons Road access will be an ingress for commercial vehicles servicing the proposed development. The service road access will provide for two-way movements for staff parking and egress movements for commercial vehicles.
- The proposed crossovers will be designed in accordance with Australian/New Zealand Standard, Parking Facilities Part 1: Off-street car parking (AS/NZS 2890.1:2004) and Australian Standard, Parking Facilities Part 2: Off-street commercial vehicle facilities (AS 2890.2:2002). The existing crossover to the site will be closed.
- The largest vehicle used for delivery will be the 19.0 m semi-trailers. Such vehicles will enter the site via Naughtons Road and exit to the service road.
- The proposed access to the service road will bisect the section of Amy Gillet Bikeway adjacent the site. The access design has ensured that there will be adequate sightlines to cyclists and pedestrians on the shared path.
- Parking for the proposed development will be provided within the subject site. The parking area will comprise of 28 parking spaces. Additional parking will be provided along the northern boundary of the site and will be allocated to staff. The design of the parking spaces will generally comply with and will comply with AS/NZS 2890.1:2004.
- The proposal will include one parking space for use by people with disabilities. This space will be 5.4 m long and 2.4 m wide and adjacent to a similarly sized shared space, as per the dimensional requirements of the Australian/New Zealand Standard, Parking Facilities Part 6: Off-street parking for people with disabilities (AS/NZS 2890.6:2009).

With regard to the parking assessment, the Adelaide Hills Council's Development Plan (consolidated 8 August 2019) identifies the following parking provision rate for industry, store, and warehouse type developments:

3.3 spaces per 100 m² total floor area of the office component, plus for the non-office component:

- 2 spaces per 100 square metres up to 200 square metres;
- 1.33 spaces per 100 square metres between 200 and 2000 square metres; and
- 0.67 spaces per 100 square metres over 2000 square metres.

Based on the above rates, the proposal will generate a parking requirement of two spaces for the office component; plus 38 spaces for the non-office component. If Council rates were adopted, therefore, there would be a total requirement for 40 parking spaces



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AMENDED 14 January 2021

The above rates, however, would apply to a facility with a much higher staffing level than the subject proposal. In such circumstances, the most accurate methodology to assess the anticipated parking demand is to use a first principles assessment.

The office designed to accommodate 8-10 staff and processing 6-14 (seasonally dependant) with remaining for (infrequent) visitor parking. For the majority of the time there will be no more than 20 staff onsite.

The proposed facility will be largely automated. The proposal will include two machines that will pre-size and sort the apples and three packing lines. The following staffing levels are anticipated to be required for each operation:

- two staff per machine for the pre-sizing and sorting operations which are less labour intensive and four staff per packing lines.

In addition, there will be a requirement for the office and storage components of the proposal. In regard to the storage area, the Transport for New South Wales (formerly RMS) "Guide to Traffic Generating Development" specifies a rate of one space per 300 m² for warehouses. Based on this rate, the storage area will generate a requirement for two spaces.

It is expected that the proposal would, therefore generate a demand for 20 parking spaces. Such a demand would be readily accommodated for within the proposed parking provision.

It is therefore considered that the proposal has been designed to satisfy the intent of the Development Plan as it relates to Transportation and Access in that the proposed apple sorting facility will be designed to ensure that all vehicles would be able to safely enter and exit the site in a forward direction. Access and parking for the proposal will comply with AS/NZS 2890.1:2004 and AS 2890.2:2018. The proposal has also considered the transport route to and from the site, given the existing substandard intersection of the service road with Onkaparinga Valley Road. Minor modifications to the service road will provide for safe turning movements of semi-trailers associated with the proposal. These modifications will also provide for improved safety for existing users of the service road. Adequate parking spaces will be provided to cater for the anticipated peak parking demand associated with the development and the traffic generated by the proposal will have minimal impact and will not change the nature or function of the adjacent road network.



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e: mark@adelaideplanning.com.au
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4.3.3.9 Council Wide – Waste

The Development Plan seeks to ensure that development suitably manages the impacts from waste so that it does not impact on the natural environment. The storage, treatment, and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.

The proposal will promote waste minimisation and reuse and recycling by providing areas within the building waste storage which will allow for onsite collection and sorting of recycling materials and the storage of fruit. All produce will be stored within the building and were required in refrigerated cool rooms. It is not in the operations commercial interest to let produce rot. There will be no odour sources. The waste area will be appropriately screened and will provide sufficient space for the storage of waste receptacles. Waste will be collected by a private contractor as required.

All outside areas not allocated to landscaping or structures will be sealed. As previously noted, the proposal incorporates high quality perimeter landscaping. No goods will be stored outside of the building. Dust emission from the site is not an issue. The issue will be ensuring dust does not enter the site.

Stormwater is proposed to be captured, stored, and reused on site with overflow diverted to the street water table to Council standards.

During the construction process, where possible all construction waste will be minimised.

On this basis the proposal satisfies the relevant Objectives and Principles of Development Control as they relate to Waste.

4.3.3.10 Council Wide – Regulated /Significant trees

A predevelopment Arboricultural Assessment has been provided by The Adelaide Tree Surgery which has considered the health of the trees contained within the north western corner of the site and on the adjoining site to the north of the subject land.

A total of five trees were identified within the report including the location of two regulated River Red Gum trees to the front of the site, a further two small gum trees and an Elm tree on the adjoining site to the north.

A response from Sharon Gillam from the Native Vegetation Branch, Department for Environment and Water has considered the removal of two gum trees which indicates

The two small gums trees that have been recommended for removal are able to be removed under the Native Vegetation Council's Guidelines for the Management of Roadside Native Vegetation and Regrowth



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AMENDED 14 January 2021

Vegetation, under Native Vegetation Regulation 11(23) (2019), Part 3, Section 5. Clearance for access to adjoining land, where the clearance is required for normal vehicle access and is no more than 5m wide.

In relation to the regulated trees, the report indicates

The two River Red Gums will not be affected by the proposed development which includes the driveway entrance and driveway past the Protected Trees for the proposed Packaging Warehouse. The encroachment into both River Red Gums Tree Protection Zones (TPZ.s) have been calculated at less than 10% each.

The Elm Trees on the adjoining site to the north will be removed as per discussions between the adjoining land owner and the applicant.

Further recommendations in relation to Tree Protection Zones during the development phase and permeable paving are recommended in the report which will ensure that the health of the trees will not be affected.

On this basis, it is considered that the proposal will protect the health of the existing trees and will provide additional landscaping to the site which will offset the removal of the two street trees.

5.0 Conclusion

After careful consideration of the proposed development and having regard to the relevant provisions of the Development Plan, it is my opinion that the proposal is not seriously at variance with the Light Industry (Woodside) Policy Area of the Light Industry Zone and Council Wide provisions of the Adelaide Hills Council Development Plan Consolidated 8 August 2019

The application seeks the construction of fruit packing facility with ancillary offices, parking, retaining walls, boundary fencing and landscaping.

The proposal will result in a form of development which is in keeping with other development in the immediate locality and will complement the mix of uses within the Woodside North Business Park and wider locality and will result in a development which will have an acceptable impact on the amenity of the adjoining properties. Further the proposal is not considered to adversely impact future development of the locality and/or detrimentally impact upon any surrounding development.

The proposed building incorporates a range of architectural elements including of a range of colours and materials, articulation to add visual interest to the streetscape. The proposal will complement the existing Onkaparinga Valley Road environment and sit comfortably within the existing streetscape with appropriate siting and landscaping in keeping with the intent of the Light Industry Zone.



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

AMENDED 14 January 2021

The buildings have been designed in accordance with the advice of MFY Traffic Engineers to ensure buildings on the site are appropriately located to allow for access through the site for both vehicles and pedestrians. The design of the site has been focused on creating a safe, well-designed vehicle and pedestrian environment. The design of the site results in a coordinated appearance that maintains and enhances the visual attractiveness of the locality.

The proposed development will provide parking to adequately accommodate the demand associated with the proposed development in accordance with relevant Australian Standards. The proposed car parking and access arrangements have been designed to ensure vehicles enter and exiting the site in a forward direction. Access arrangements will be maintained for the site, with design modifications to facilitate entry and exit movements of delivery vehicles in a forward direction. Traffic generation associated with the proposed development will not significantly impact on the surrounding road network.

The proposal will be formally referred to DIT through the formal referral process however preliminary considerations with these departments have determined that the proposal will satisfy the standard agency requirements.

Having regard to all the relevant provisions of the Development Plan, for the reasons aforementioned, it is my opinion, that the application represents an appropriate form of development in the context of the Light Industry (Woodside) Policy Area of the Light Industry Zone and Council Wide provisions of the Adelaide Hills Council Development Plan Consolidated 8 August 2019 and the unique circumstances of the subject land and locality.

Accordingly, and on balance, the proposed development is not considered to be seriously at variance with, and sufficiently satisfies, the relevant provisions of the Development Plan and Development Plan Consent is warranted.

We look forward to the support of the proposal in its current form. Should you have any queries or require any further information or clarification with any components of this statement, please do not hesitate to contact by contacting me by email mark@adelaideplanning.com.au.

Yours Sincerely,



Mark Kwiatkowski MPIA CPP

Director + Principal Urban Planner

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ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

AMENDED 14 January 2021

In reply please quote: 2020/00272, Process ID: 654497
Enquiries to: Reece Loughron
Telephone: 08 7109 7876
E-mail: dit.landusecoordination@sa.gov.au



Government of South Australia

Department for Infrastructure
and Transport

**TRANSPORT PLANNING AND
PROGRAM DEVELOPMENT**

Transport Assessment

GPO Box 1533
ADELAIDE SA 5001

ABN 92 366 288 135

15 January 2021

Mr Deryn Atkinson
Adelaide Hills Council
PO Box 44
WOODSIDE SA 5244

Dear Mr Atkinson

SCHEDULE 8 - REFERRAL RESPONSE

Development No.	473/1273/20
Applicant	Joyson Orchards
Location	202-204 Onkaparinga Valley Road (cnr Naughtons Road), Woodside
Proposal	Fruit packing facility with ancillary offices, retaining walls and parking

I refer to the above development application forwarded to the Commissioner of Highways (CoH) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

CONSIDERATION

The subject site abuts Onkaparinga Valley Road and Naughtons Road. Onkaparinga Valley Road is an arterial road under the care, control and management of the CoH. Onkaparinga Valley Road is identified as a Direct Tourist Route and a Major Regional Cycling Route under the Department for Infrastructure and Transport's '*A Functional Hierarchy for South Australia's Land Transport Network*'. The abutting sections of Onkaparinga Valley Road is gazetted for 20 metre PBS Level 1A vehicles. At this location Onkaparinga Valley Road carries approximately 9,300 vehicles per day (9% commercial vehicles) and has a posted speed limit of 60km/h. Naughtons Road has a default speed limit of 50km/h.

Access and Road Safety

DIT had reviewed the APDS Planning Statement (Version 3.0 dated 25 November 2020) and associated MFY Traffic and Parking Report (refer MLM/20-0156 dated 12 November 2020) and the Beyond Ink plan set (Revision A dated 24 November 2020). The site has frontage to Onkaparinga Valley Road through the adjacent service road that also caters for the Amy Gillet Bikeway, as well as frontage to Naughtons Road (refer Beyond Ink, Site Plan, Sheet No. PD001, Revision A dated 24 November 2020).

Delivery Vehicles

The proposed development has been designed to accommodate entry movements of 19 metre semi-trailer via Naughtons Road (right in) and exit movements to the service road and then Onkaparinga Valley Road (left out only). The MFY report has undertaken a thorough review of the current design of the service road connection to Onkaparinga Valley Road and DIT

acknowledges that the existing arrangement does not meet current standards. Therefore, in order to cater for the delivery vehicles undertaking left turns the service road arrangement will be modified to provide for an improved entry angle to maximise driver sightlines. Refer MFY Figure 4 and 5. DIT supports the road works and all costs should be undertaken to Council/DIT requirements with all costs borne by the applicant.

Further, the MFY report indicates that the proposed development will not actually generate a demand for right turning movements for large commercial vehicles onto Onkaparinga Valley Road and that a traffic management plan will be developed by the applicant (operators) that requires all drivers of commercial vehicles to undertake left turns onto the service road and Onkaparinga Valley Road. DIT also supports these measures.

Overall, DIT supports the proposed access arrangements and Council should ensure the service road and Naughtons Road access are appropriately designed and flared to accommodate a 19.0 metre Articulated Vehicle (*AS2890.2:2018*).

In regards to general parking requirements, Council should ensure that the proposed car park design is designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009*. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.

ADVICE

The Department for Infrastructure and Transport supports the proposed development and advises the planning authority to attach the following conditions to any approval:

1. The access points to Onkaparinga Valley Road service road and Naughtons Road shall be constructed as shown on Beyond Ink, Site Plan, Sheet No. PD001, Revision A dated 24 November 2020.
2. The Onkaparinga Valley Road service road shall be modified to accommodate the swept path of a 19 metre Articulated Vehicle in order to permit a suitably angled approach to Onkaparinga Valley Road. The modifications shall be generally consistent with MFY Figure 5 with all costs borne by the applicant.
3. Large commercial vehicles accessing the site shall be restricted to right turn entry only movements to Naughtons Road and left turn exit only movements into the service road and Onkaparinga Valley Road. The movements and traffic flow through the site shall be reinforced with suitable signage and line marking as well as the traffic management plan initiated by the applicant/operator.
4. All vehicles shall enter and exit the site in a forward direction.
5. Any infrastructure within the road reserve that is demolished, altered, removed or damaged during the construction of the project shall be reinstated to the satisfaction of the relevant asset owner, with all costs being borne by the applicant.
6. The car park design shall be designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009*. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
7. Commercial vehicle facilities shall be designed in accordance with *AS2890.2:2018*.

8. Any stormwater run-off shall be collected on-site and disposed of safely without jeopardising the safety of the adjacent roads. Any alterations to the existing road drainage infrastructure as a result of this development shall be at the expense of the applicant.

Yours sincerely



A/MANAGER, TRANSPORT ASSESSMENT
for **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to dit.developmentapplications@sa.gov.au

**CATEGORY 2
PUBLIC NOTIFICATION**

DEVELOPMENT 20/1273/473

Joyson Orchards Pty Ltd

For

Construction of light industry (agricultural industry) building & associated two storey offices, carparking, retaining walls (maximum height 3.5m), acoustic fence (maximum height 2.8m), signage, associated access work, landscaping & associated earthworks

At

**202 & 204 Onkaparinga Valley Road & Lot 21 DP50774 Naughtons Road,
Woodside SA 5244**

COMMENCEMENT DATE: 08 December 2020

CLOSING DATE: 21 December 2020

CONTACT OFFICER: Melanie Scott

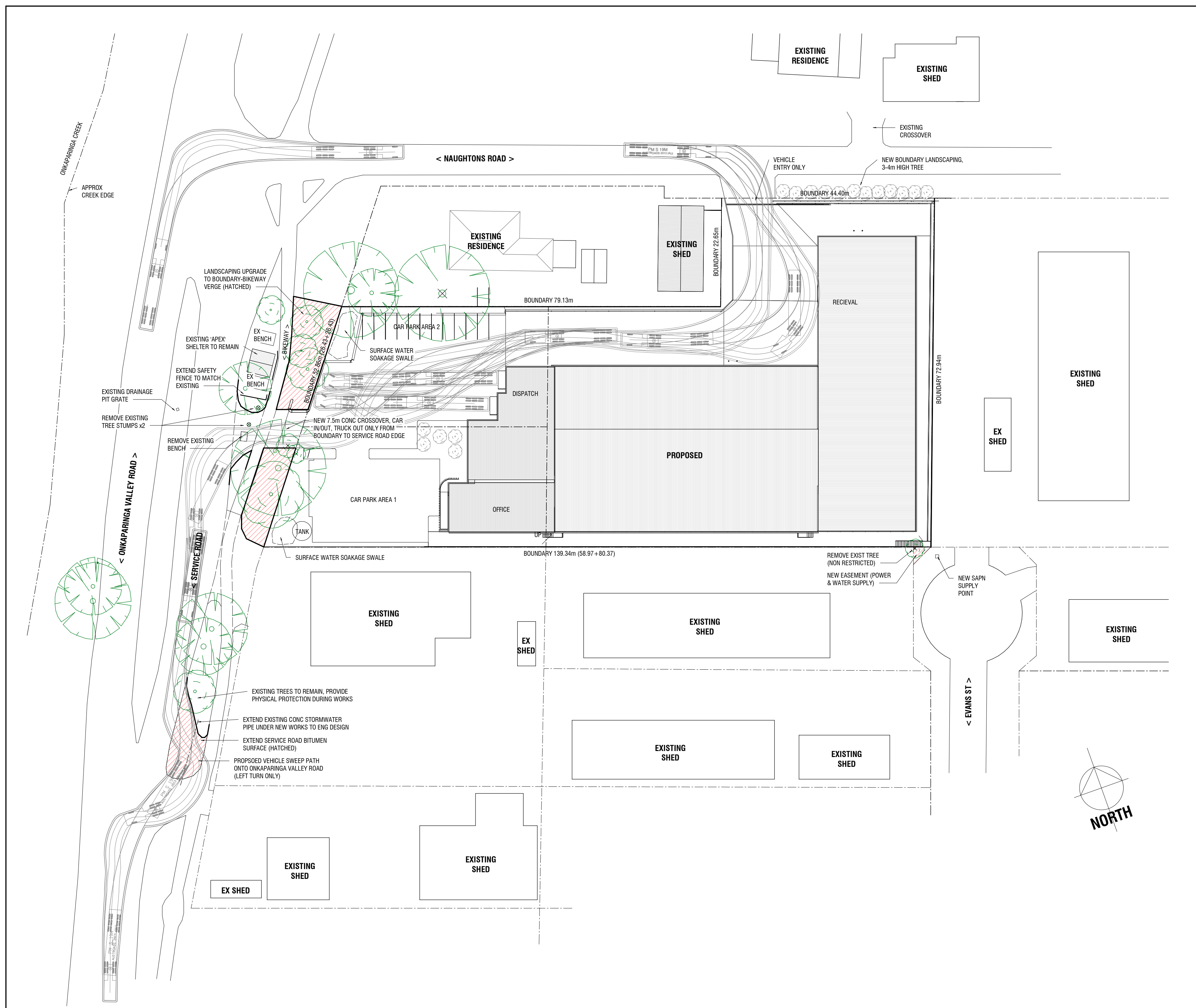
CAT 2 TRIGGER: In accordance with Schedule 9 part 2 clause 19 as the site of the proposed development is adjacent land to land in a zone under the relevant Development Plan which is different to the zone that applies to the site of the development

ZONE: Light Industry Zone - Light Industry (Woodside) Policy Area

FOR PUBLIC DISPLAY ONLY

Date to be displayed: 08 December 2020

Date to be removed: 21 December 2020



SITE PLAN EXTENDED
1 : 500

JOYSON ORCHARDS ADELAIDE HILLS FRESH FRUIT PACKING FACILITY

198-200 ONKAPARKINGA VALLEY ROAD, WOODSIDE SA 5244



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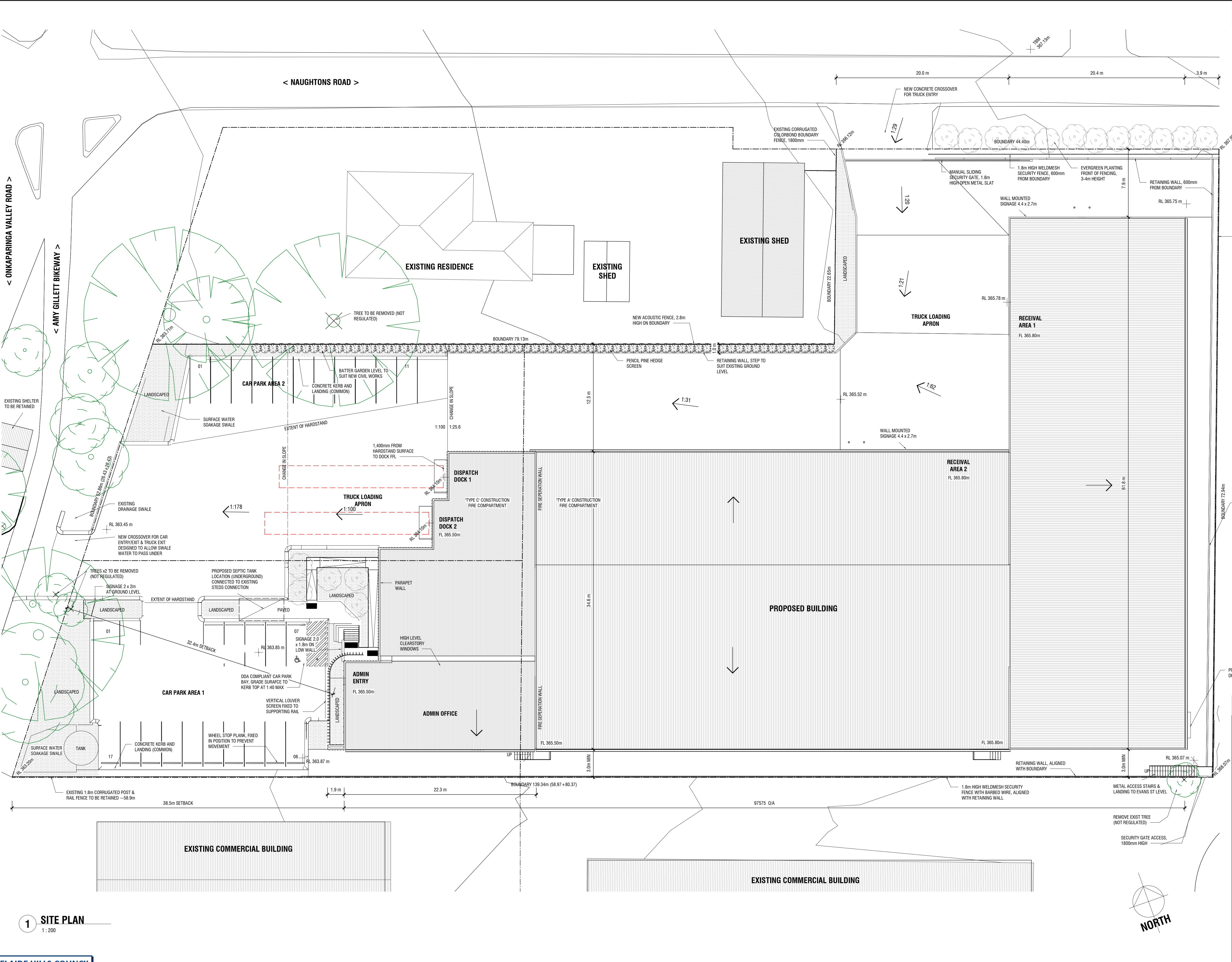
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198-200 Onkaparinga Valley Road, WOODSIDE SA

COVER SHEET & SITE PLAN EXT

PRELIMINARY Revision **A**

Drawn AH Sheet Number
Date 25/11/2020 **PD000**
Scale @A1 size : 1 : 500

ABBREVIATIONS	
ABBREV.	DESCRIPTION



Areas Schedule		
Name	Area	Level
ALLOTMENT (All Titles)	3888.60 m ²	FL-0 (N)
OFFICE (1)	233.51 m ²	FL-1 (N)
OFFICE (G)	239.05 m ²	FL-0 (N)
PROCESSING	3488.72 m ²	FL-0 (N)

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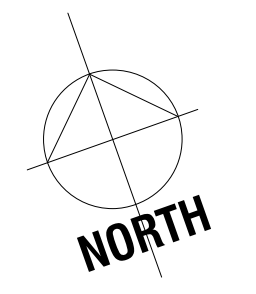
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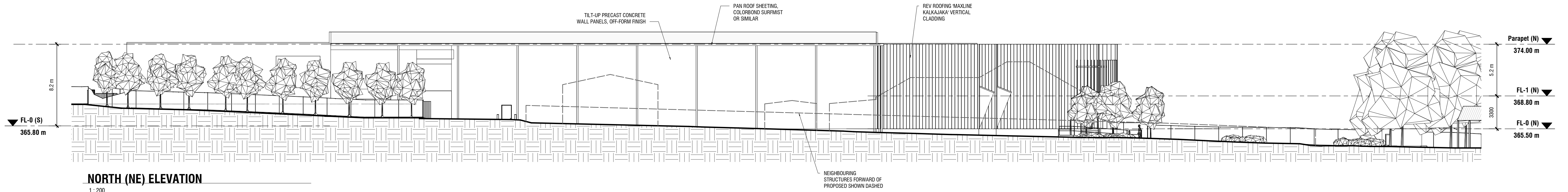
JOYSON ORCHARDS, ADELAIDE HILLS FRESH, FRUIT PACKING FACILITY
 198-200 Onkaparinga Valley Road, WOODSIDE SA

SITE PLAN
PRELIMINARY Revision **A**
 Drawn AH
 Date 25/11/2020
 Scale @A1 size : 1 : 200
 Sheet Number **PD001**

1 SITE PLAN
 1 : 200

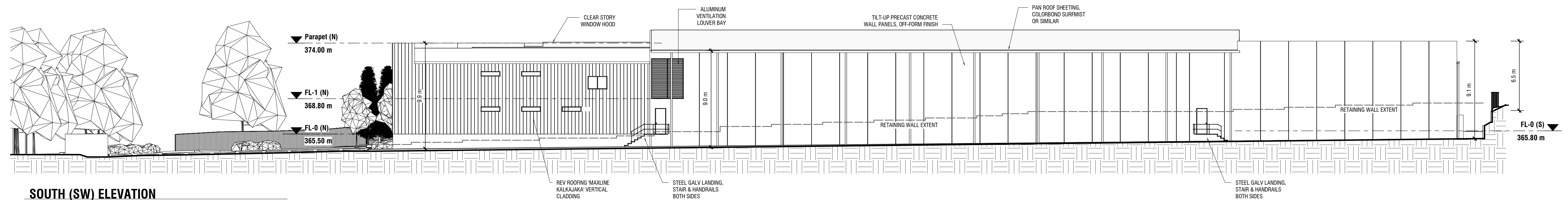
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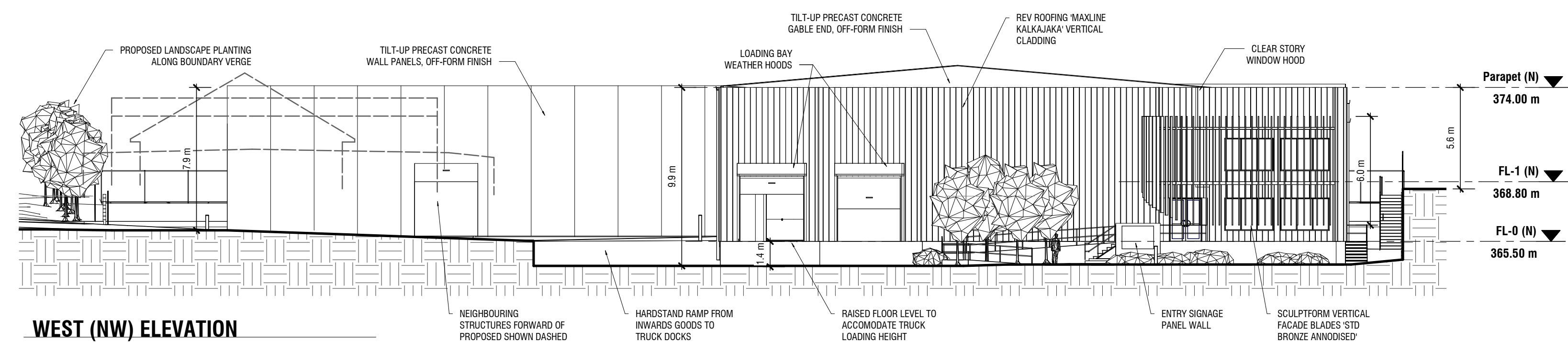
NORTH (NE) ELEVATION

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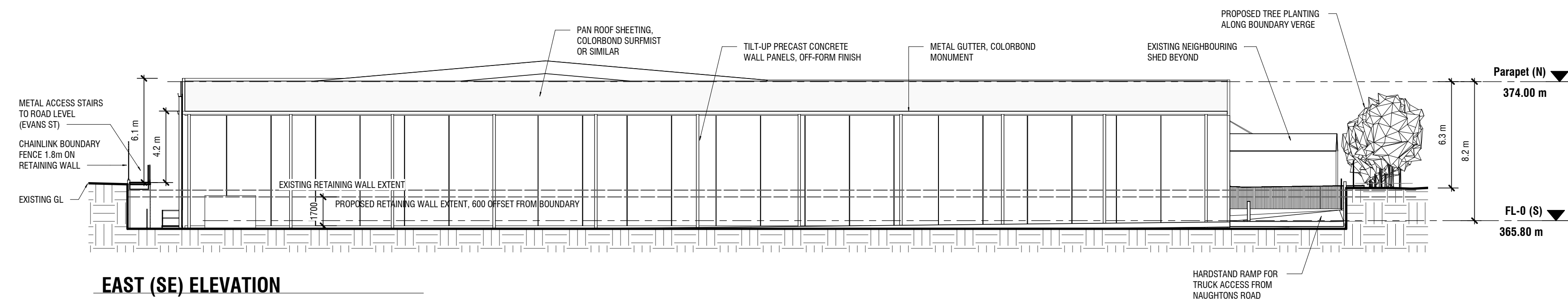
SOUTH (SW) ELEVATION

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WEST (NW) ELEVATION

1 : 200



EAST (SE) ELEVATION

1 : 200

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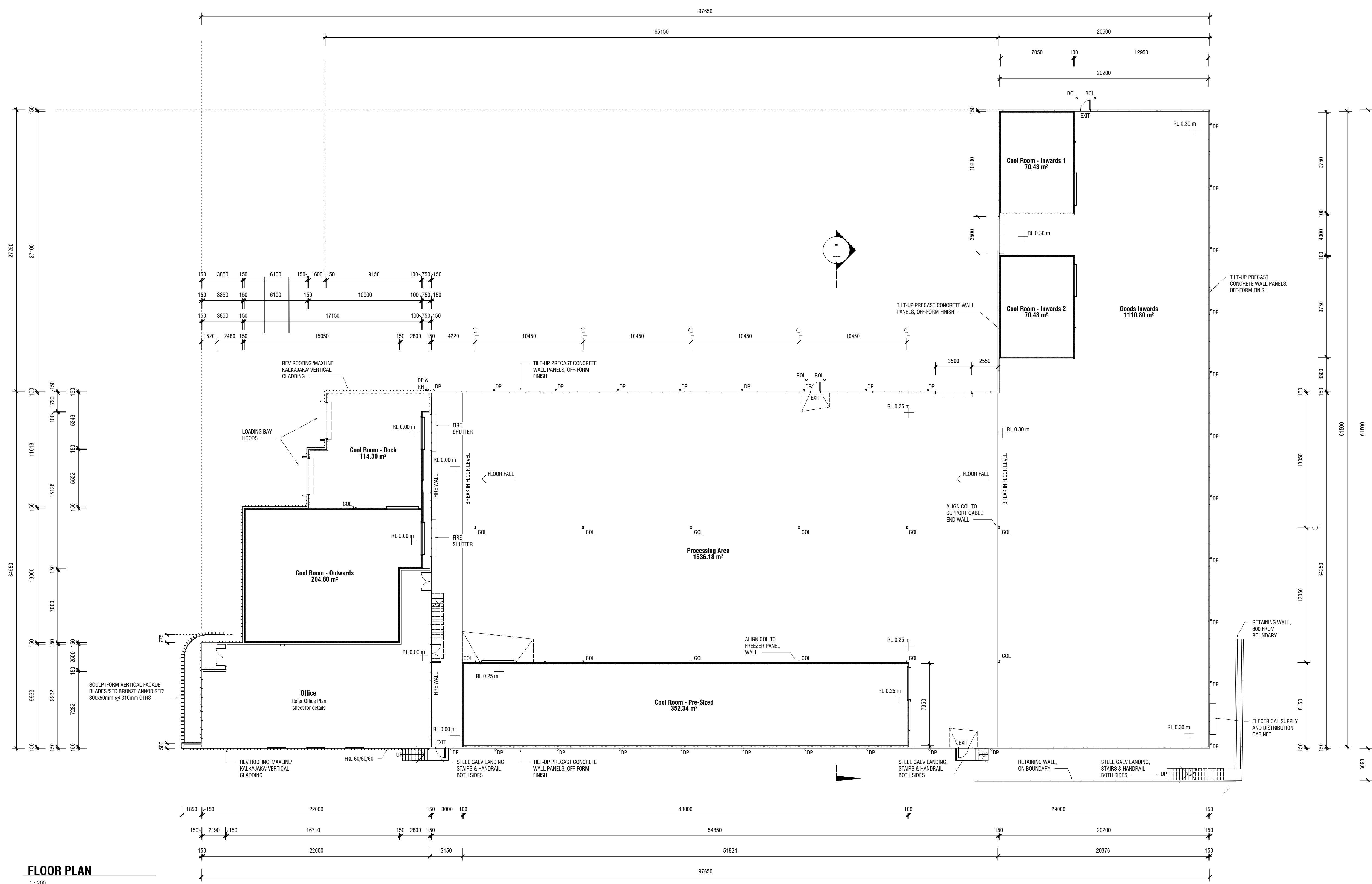
198-200 Onkaparinga Valley Road, WOODSIDE SA

EXTERNAL ELEVATIONS

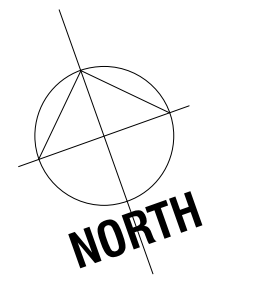
PRELIMINARY Revision **A**

Drawn Author Sheet Number
 Date 24/11/2020 **PD002**
 Scale @A1 size : 1 : 200

ABBREVIATIONS	
ABBREV.	DESCRIPTION



FLOOR PLAN
1 : 200



Rev No	Rev Date	Revision Description
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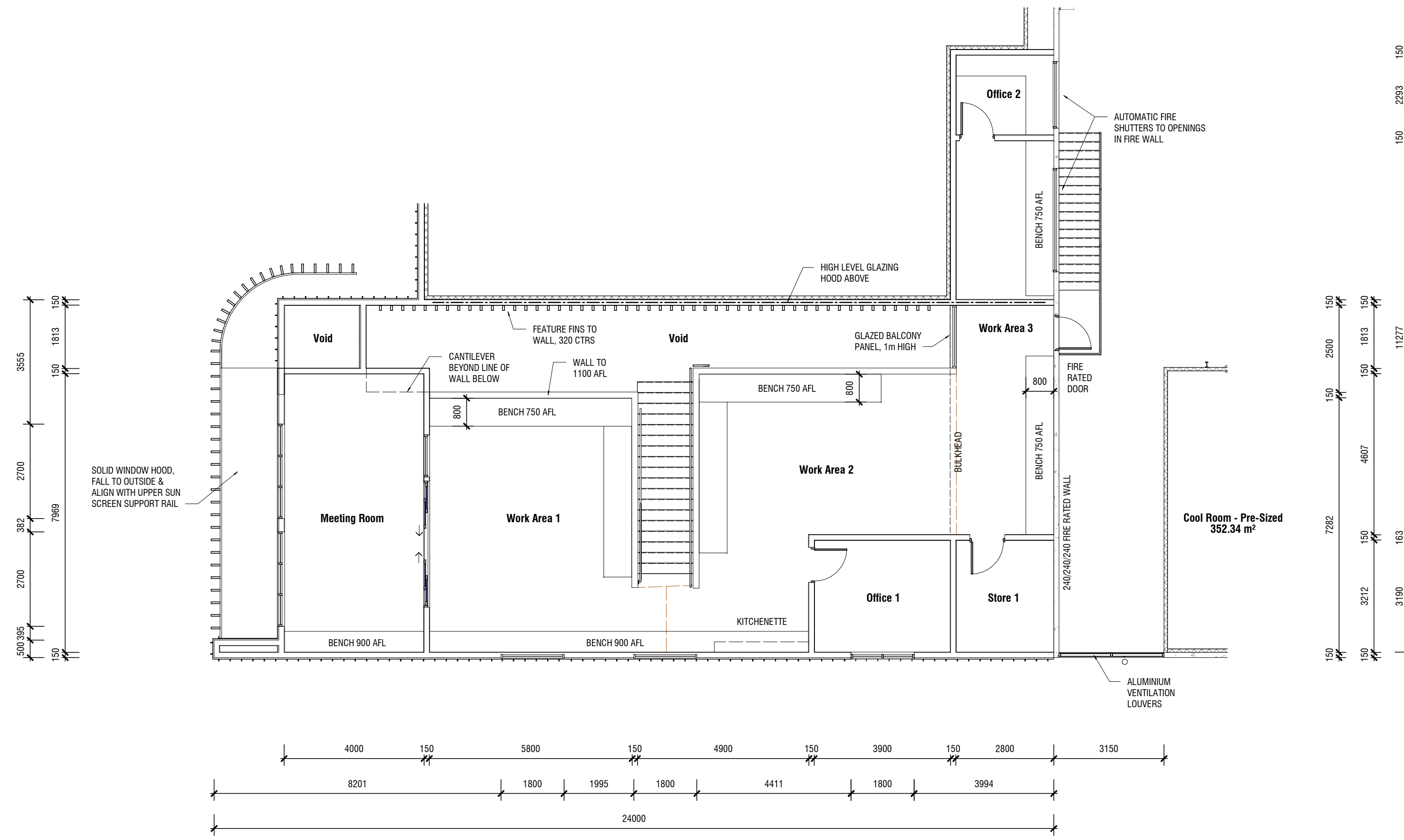
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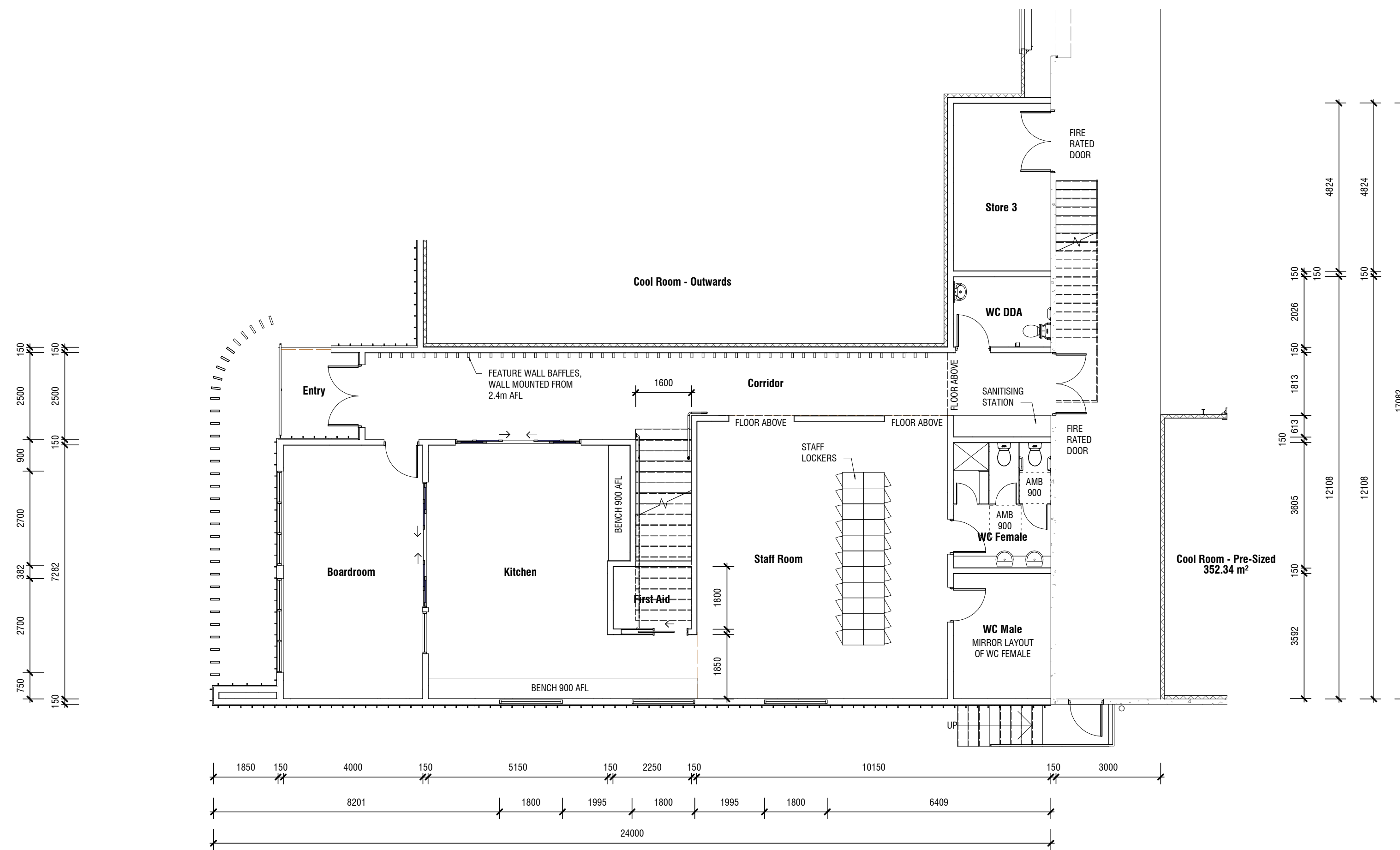
FLOOR PLAN
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Sheet Number **PD101**



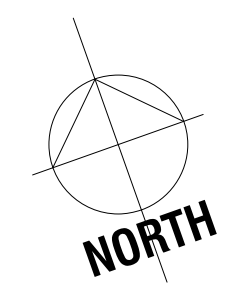
FLOOR PLAN - OFFICE UPPER

1 : 100



FLOOR PLAN - OFFICE LOWER

1 : 100



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**JOYSON ORCHARDS, ADELAIDE HILLS
 FRESH, FRUIT PACKING FACILITY**

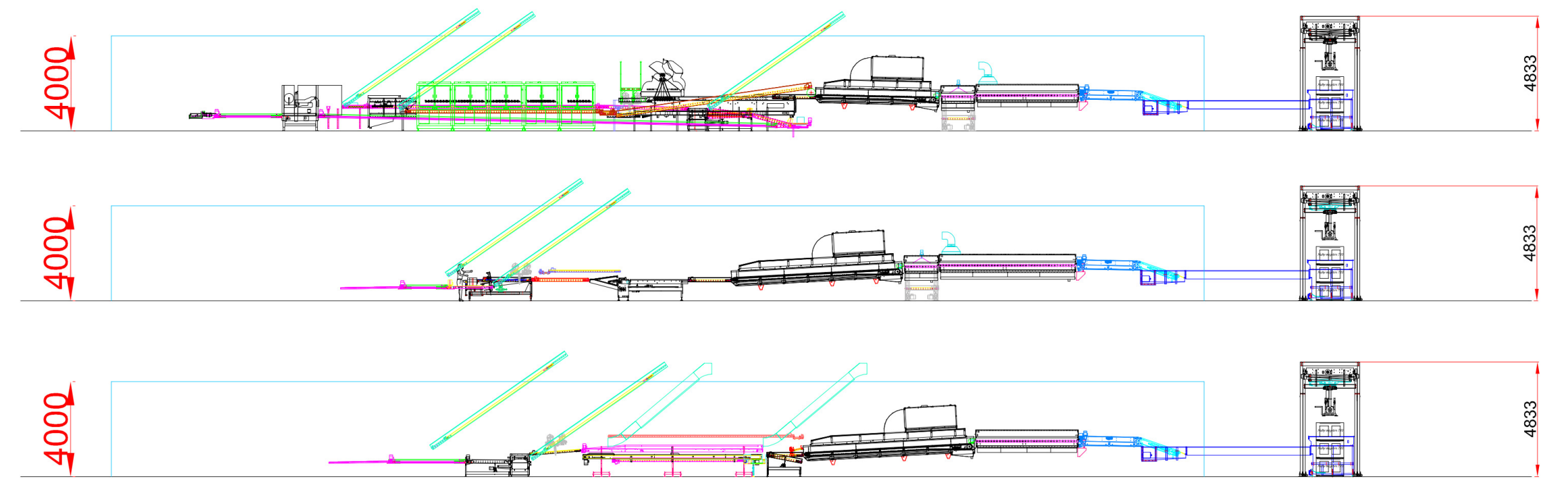
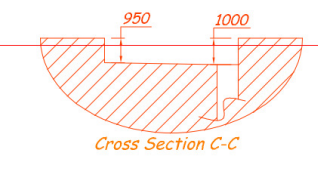
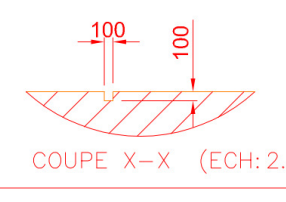
198-200 Onkaparinga Valley Road, WOODSIDE SA

FLOOR PLANS - OFFICE

PRELIMINARY Revision **A**

Drawn AH
 Date 24/11/2020
 Scale @A1 size : As indicated
 Sheet Number **PD102**

- ENGLISH
CIVIL WORK DRAWING
- Same to connect to the water sewage system of the building
 - Same for accessing the pit
 - Load at the bottom of the pit 200kg/m² (external concrete ground)
 - Load in the area of the floor, Pavement, G/H, G/L, Water Surface 200kg/m² (concrete ground)
 - Dimensions in millimeters
 - Water connection point
 - Sewer connection point
 - Compressed air connection point
 - Electrical connection point
 - Drainage line
 - Clearing trap door
 - Steel box grid for Fork Lift Truck crossing
 - point de connection internet



31 APPROVED CUSTOMER: ADELAIDE HILLS FRESH (REVISED) 2019 (Landscape Hills Fresh 43 A200625)

Approved: _____

Product: APPLE

This layout remains the exclusive property of MAF Agrobotic until finalisation of the contract and the final agreement. Any breach may result in legal proceedings.

CUSTOMER: ADELAIDE HILLS FRESH
Address: WOODSIDE SA

Country: AUSTRALIA Phone: _____

MAF Agrobotic
Phone: 08 8364 8100
150012 - 8001 Moultonville
FRANCE
Date: 27/06/2020

Drawn by: IS/VR
Salesman: ALEXANDER

DRAWING N°: 31 A200625

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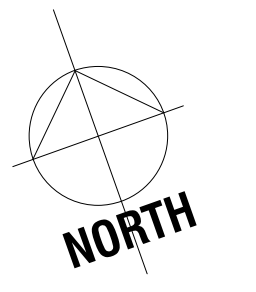
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198-200 Onkaparinga Valley Road, WOODSIDE SA

EQUIPMENT PLAN

PRELIMINARY	Revision	A
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Sheet Name	Front Facade Carpark	
Drawn	AH	Sheet Number
Date	25/11/2020	PP101
Scale	@A3 size	

Revision Number	Revision Date	Revision Description



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Sheet Name	NAUGHTONS ENTRY	
Drawn	AH	Sheet Number
Date	25/11/2020	PP102
Scale	@A3 size	

Revision Number	Revision Date	Revision Description



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Sheet Name	FRONT ELEVATED	
Drawn	AH	Sheet Number
Date	25/11/2020	PP103
Scale	@A3 size	

Revision Number	Revision Date	Revision Description

ALL DIMENSIONS, LEVELS AND BOUNDARY LOCATIONS SHALL BE VERIFIED ON SITE PRIOR TO CONSTRUCTION. ALL DIMENSIONS TO TAKE PRECEDENCE OVER SCALED DRAWINGS. DO NOT SCALE THIS DRAWING. NO WORK CAN COMMENCE ON SITE UNTIL FULL DEVELOPMENT APPROVAL IS GRANTED BY THE LOCAL APPROVAL AUTHORITY.



THE *Adelaide*
TREE SURGERY

A division of *tree aware*

3 Ellemsea Circuit
LONSDALE SA 5160
ABN: 33 099 478 994

Phone: 8371 5955 Mobile: 0408 086 774 Fax: 8297 6885 Email: mark@adelaidetreesurgery.com

Pre-Development Arboricultural Assessment and Report

Prepared for
Joyson Orchards
Attention: Linda MacAuley

Site Address
Lots 202 & 204 Onkaparinga Valley Road
Woodside SA 5244

In Regard to
Various Trees and Proposed Development



Prepared By:
Mark Elliott Consulting Arborist/Diploma Arboriculture



THE *Adelaide*
TREE SURGERY
A division of *tree aware*

3 Ellemsea Circuit
LONSDALE SA 5160
ABN: 33 099 478 994

Phone: 8371 5955 **Mobile:** 0408 086 774 **Fax:** 8297 6885 **Email:** mark@adelaidetreesurgery.com

CONTENTS	Page No
1.0 Introduction	3
2.0 Term of Reference	3
3.0 Caveat Emptor	3
4.0 The Site	3
5.0 The Proposal	4
6.0 The Tree/s	5
7.0 Potential Impacts	6
8.0 Tree Discussion	8
9.0 Legislation	8
10.0 Recommendations	8
Appendix A: References	10
Appendix B: Disclaimer and Limitations	11

1.0 INTRODUCTION

- 1.1 This report has been prepared at the request of **Ms Linda MacAuley** from **Joyson Orchards** and is in relation to five trees and the **Proposed Development** of a **Packaging Facility** at **Lots 202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 1.2 Of the five trees consist of two **Regulated *Eucalyptus camaldulensis* (River Red Gum)** and the **Non Protected *Eucalyptus camaldulensis* (River Red Gum)** which one is a council owned tree.
- 1.3 The subject two **Regulated River Red Gums** have a stem circumference greater than 2 meters and less than 3 meters , when measured at a point 1.0 metre above natural ground level , therefore, these trees can be declared as a **Regulated Trees** as defined in the **SA Development Act 2008**.
- 1.4 The report also uses the **Australian Standards 4970 2009: The Protection of Trees on Development Sites** as the guiding principle in regards to the proposed development of the site and the protection of all trees.

2.0 TERMS OF REFERENCE

- 2.1 Instructions were received is September **2020** via email.
- 2.2 The instructions requested were to undertake a Pre-Development Arborist Report for **2 x Regulated *Eucalyptus camaldulensis* (River Red Gum)** and **2 Non Protected) *Eucalyptus camaldulensis* (River Red Gum)** which are located at **202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 2.3 This report uses the **Australian Standard, 4970-2009: The Protection of Trees on Development Sites** as the guiding principles in regard to the proposed development of the site.

3.0 CAVEAT EMPTOR

- 3.1 This is a stage 1 '**Ground Report**'. The trees were inspected from the ground only.
- 3.2 The report is limited by the time of the inspection.
- 3.3 The report reflects the trees as found on the day of inspection. Any changes to site conditions or surroundings, such as construction works, landscape works or further failures or pruning, may alter the findings of the report.
- 3.4 The report is based on the inspection of the site and the trees along with the supplied plans of the proposed development.
- 3.5 The inspection period to which this report applies is three months from the date of the report.

4.0 THE SITE

- 4.1 The proposed development is located at **Lot 202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 4.2 The **Gum Trees** are located along the front of the property of **202 & 204 Onkaparinga Valley Road, Woodside SA 5244**.
- 4.3 **Woodside** is a rural township which is located within the council boundaries of the **Adelaide Hills Council**.



Figure 1 shows an aerial image of the property at 202 & 203 Onkaparinga Valley Road, Woodside SA 5244.

5.0 THE PROPOSAL:

- 5.1 It is proposed to construct a new **Packing Facility** at the property of **202 & 204 Onkaparinga Valley Road, Woodside SA 5244** (refer Figure 2).
- 5.2 The designated car parking area along the northern side is to be constructed using a permeable product such as granitic sand/gravel
- 5.3 The northern side boundary line fence is to be a 2.8 meter acoustic fence

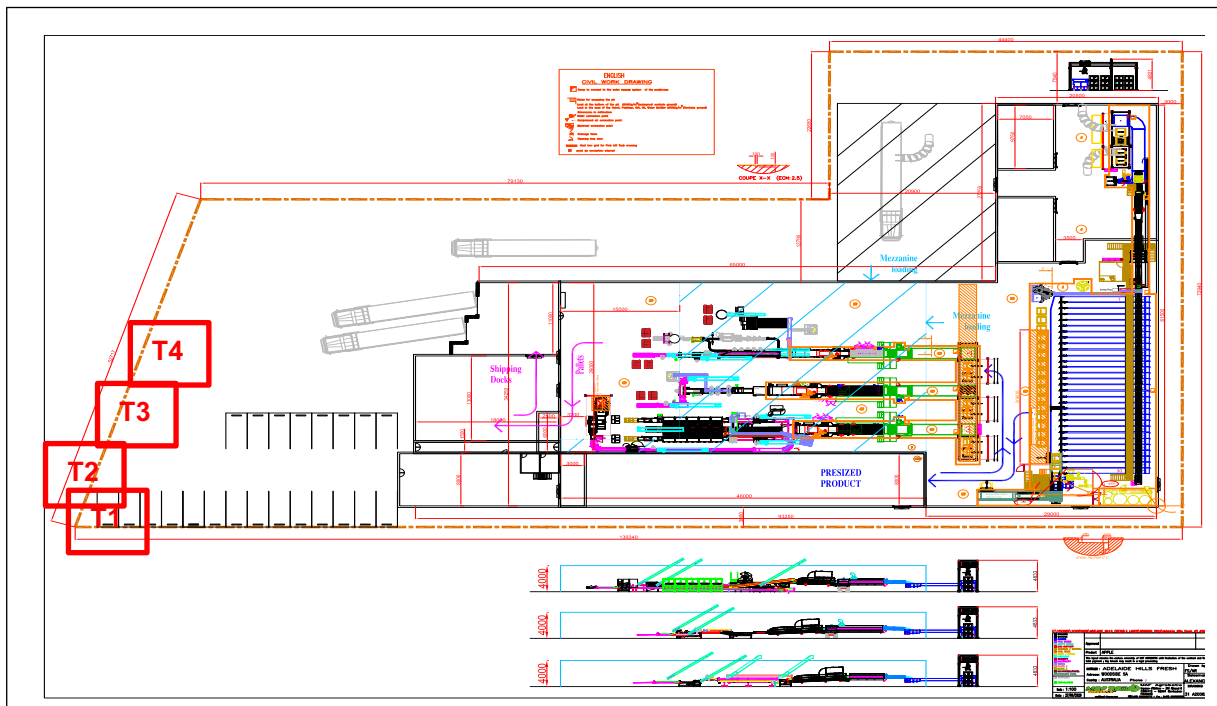


Figure 2 shows the proposed new plans for the Packing Facility of 202 & 204 Onkaparinga Valley Road, Woodside SA 5244.

5 THE TREE/S -


5.1 Following is information of the five subject trees within this **Pre-Development Arbpricultural Report**

Tree Number	Tree Name	Significant and or Regulated	Tree Condition/Information
1	River Red Gum	Regulated	<p>This River Red Gum is showing good health and condition. The tree is located along the front boundary line.</p> <p>The tree is located closest to the side boundary line furthest away from the proposed main driveway</p> <p>There trees foliage had good density and vigour.</p> <p>There was some minor deadwood located throughout the crown.</p> <p>The height of the tree is approximately 14 – 15 meters.</p> <p>The crown of the tree connect with the neighbouring River Red Gum which both crowns for a large broad spreading crown.</p>
2	River Red Gum	Regulated	<p>This River Red Gum is showing good health and condition. The tree is located along the front boundary line.</p> <p>This tree is the second tree from the righthand side boundary line.</p> <p>There trees foliage had good density and vigour.</p> <p>There was some minor deadwood located throughout the crown.</p> <p>The height of the tree is approximately 15 plus meters.</p> <p>The crown of the tree connect with the neighbouring River Red Gum which both crowns for a large broad spreading crown.</p>
3	River Red Gum	Non Protected	<p>This tree is a small tree that is showing good health and condition</p>

			<p>The height of the tree is approximately 5 – 6 meters and the tree has a small crown as a result of being over shadowed by the larger tree.</p> <p>This tree is a council owned tree.</p>
4	River Red Gum	Non Protected	<p>This tree is a small tree that is showing good health and condition</p> <p>The height of the tree is approximately 5 meters and the tree has a small crown as a result of being over shadowed by the larger tree.</p>



Figure 3 shows the four River Red Gums which are located at the front of the property at Lot 202 & 204 Onkaparinga Valley Road, Woodside Sa 5244.



5.2 Tree Measurements (Protected Trees):

Tree Number	Structural Root Zone (SRZ)	Tree Protection Zone (TPZ)
Tree #1	2.55 meters	10.98 meters
Tree #2	2.63 meters	10.68 meters

5.3.1 The measurements within the above table are measured as a radius from the center of the tree base.

6.0 PROPOSED DEVELOPMENT & POTENTIAL IMPACTS

6.1 The aim of this report is to provide guidelines for best practise tree protection measures in accord with ***Australian Standard AS4970-2009 'Protection of Trees on Development Sites'***

6.2 The preamble of the standard provides a brief outline of why it is important to retain and protect trees on development sites and the following Section: The Tree Protection Zone details the zones around a tree that are required to protect it.

6.3 AUSTRALIAN STANDARD: AS4970-2009 'Protection of Trees on Development Sites'

6.3.1 *A living tree is a dynamic organism that needs specific environmental conditions to continue healthy, stable growth. It is rarely possible to repair stressed and injured trees, so substantial injury needs to be avoided during all stages of development and construction.*

6.3.2 *For trees to be retained and their requirements met, procedures must be in place to protect trees at every stage of the development process. This should be taken into account at the earliest planning stage of any outdoor event or design of a development project where trees are involved.*

6.3.3 *Trees and their root systems may occupy a substantial part of any development site and because of their potential size, can have a major influence on planning the use of the site.*

6.3.4 *Existing trees of appropriate species and sound structure can significantly enhance new development by providing immediate benefits such as shade and stormwater reduction as well as complementing new development.*

6.3.5 *Most trees will take many years and possibly decades to establish but can be injured or killed in a very short time, as their vulnerability is commonly not understood. This is especially so in relation to tree root systems which cannot be seen. Irreparable injury frequently occurs in the early stages of site preparation and remedial measures routinely fail.*

6.3.6 *Early identification and protection of important trees on development sites is essential from the outset and will minimise the problems of retaining inappropriate trees.*

6.3.7 *Successful long-term retention of trees on development sites depends on an acceptance and acknowledgement of the constraints and benefits that existing trees generate. Protecting trees in accordance with the Standard may influence design and construction costs and this should be considered in project budgets and contracts. The gains and benefits of retaining trees will accrue if the measures detailed in the Standard are applied.*

6.4 THE TREE PROTECTION ZONE

6.4.1 The **Tree Protection Zone (TPZ)** is the principal means of protecting trees on development sites. The TPZ is a combination of the root area and crown area requiring protection. It is an area isolated from construction disturbance, so that the tree remains viable. The TPZ incorporates the structural root zone (SRZ).

6.4.2 It may be possible to encroach into or make variations to the standard or optimal TPZ. Encroachment includes excavation, compacted fill and machine trenching.

6.4.3 If the proposed encroachment is greater than 10% of the TPZ or inside the SRZ, the project arborist must demonstrate that the tree(s) would remain viable.

6.5 SRZ & TPZ CALCULATIONS

6.5.1 **The SRZ (or CRZ): Structural/Critical Root Zone** is the zone around a tree required to protect the tree's stability. **Generally, no development activities are permitted within this zone unless there are no other suitable options.**

6.5.2 **The TPZ or Optimal Tree Protection Zone is the principal means of protecting the tree and is calculated using the formula TPZ = DBH (diameter @ 1.4 meters above ground level) x 12.**

7.1 POTENTIAL IMPACTS

- 7.1.1 I have provided general comment in relation to the development of the Packaging Warehouse and its potential impact on the two **River Red Gums**.
- 7.1.2 There is potential that some tree roots from the tree closest to the proposed driveway could be exposed during the works.
- 7.1.3 Refer to **Section 10** of this report for recommendations and correct methodologies. It is my opinion that these recommendations and methodologies apply to protected and no protected trees.

7 TREE DISCUSSIONS

- 8.1 I believe the proposed development will not have a detrimental effect of the three trees which are to remain (**Trees 1 and 2**) if the correct methodologies are followed and implemented.
- 8.2 The two **River Red Gums** which are located at the front of the property are located far enough away from the proposed driveway to have minimal impact on the trees health and condition. The encroachment into the Tree Protection Zones (TPZ;s) of the trees is less than 10%.
- 8.3 Unfortunately the two smaller **River Red Gums (Trees 3 and 4)** will be recommended for removal which does allow for better protection of the **Regulated River Red Gums**.
- 8.4 There are no other areas of the proposed development that has any effect on any trees within the property or neighbouring properties.
- 8.5 Refer to the recommended methodologies (**refer section 10**) for this proposed development to ensure that the trees remain in their current good health and condition.

9 LEGISLATE REQUIREMENTS

- 9.1 The two **Regulated Trees** have a single stem greater than 2 meters and less than 3 meters when measured at 1 metre above ground level, therefore fulfilling the criteria contained within the **Development Regulations 2008** and also refers to the **Adelaide Hills Development Plan** consolidated **8th August 2019**.

PRINCIPLES OF DEVELOPMENT CONTROL

Development should have minimal adverse effects on regulated trees. ***Yes – I believe the two River Red Gums will not be affected by the proposed development which includes the driveway entrance and driveway past the Protected Trees for the proposed Packaging Warehouse. The encroachment into both River Red Gums Tree Protection Zones (TPZ.s) have been calculated at less than 10% each.***

10 RECOMMENDATIONS

10.1 Having considered all five trees within this report, I am recommending the following:

10.3 DEVELOPMENT PHASE:

- 10.3.1 All works required within the area of the Tree Protection Zones (TPZ's) must be undertaken by hand, using non-destructive excavation methods or under the supervision of the Project Arborist.
- 10.3.3 All underground services are to avoid the Tree Protection Zone (TPZ) of all trees where possible.

10.4 TREE PROTECTION DURING DEVELOPMENT PHASE:

10.4.1 The Tree Protection Plan for the Regulated Trees is to be installed for duration of the development. This fencing is to be chain mesh panels (1.80 meters high) and clamped together to avoid easy access inside the Tree Protection Zones (TPZ).

10.4.2 Mulch the TPZ using 100mm of course mulch

10.4.3 Irrigate the Tree Protection Zones (TPZ's) periodically as directed by Project Arborist

10.4.4 **NO STORAGE OF ANY BUILDING MATERIAL OR MACHINERY WITHIN THE FENCED TREE PROTECTION ZONE (TPZ) AREAS.**

10.4.5 Protection Zone (TPZ) fencing is to be clearly marked with signage saying – **TREE PROTECTION ZONE, DO NOT ENTER. PROJECT ARBORIST – THE ADELAIDE TREE SURGERY – 08 83715955**

10.5 It is recommended that the two non-protected River Red Gums are to be completely removed as they are located within area of main driveway to the property.

10.5 **Council Approval from the Adelaide Hills Council needs to be granted prior to commencement of any works.**

Mark Elliott
Consultant Arborist/Diploma Arboriculture

APPENDIX A: REFERENCES

The Development Act (1993) South Australian Legislation

The Development Regulations (1993) South Australian Legislation

City of Burnside Development Plan Consolidated 8th August 2017

Australian Standards AS 4970-2009: Protection of Trees on Development Sites

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APPENDIX B: DISCLAIMER AND LIMITATIONS

This report only covers identifiable defects present at the time of inspection. The author accepts no responsibility or can be held liable for any structural defect or unforeseen event/situation that may occur after the time of inspection, unless clearly specified timescales are detailed within the report.

The author cannot guarantee trees contained within this report will be structurally sound under all circumstances, and cannot guarantee that the recommendations made will categorically result in the tree being made safe.

Unless specifically mentioned this report will only be concerned with above ground inspections, that will be undertaken visually from ground level. Trees are living organisms and as such cannot be classified as safe under any circumstances. The recommendations are made on the basis of what can be reasonably identified at the time of inspection therefore the author accepts no liability for any recommendations made.

Care has been taken to obtain all information from reliable sources. All data has been verified insofar as possible; however, the author can neither guarantee nor be responsible for the accuracy of information provided by others.

Planning Statement

CONSTRUCTION OF FRUIT PACKING FACILITY WITH ANCILLARY
OFFICES, PARKING, RETAINING WALLS, BOUNDARY FENCING AND
LANDSCAPING

202 - 204 ONKAPARINGA VALLEY ROAD AND 21 NAUGHTONS
ROAD, WOODSIDE.



Prepared for Joyson Orchards

Report prepared by Adelaide Planning and Development Solutions

Contact Mark Kwiatkowski

Phone 0499933311

Email mark@adelaideplanning.com.au

**ADELAIDE HILLS COUNCIL
RECEIVED 26/11/2020**



PLANNING STATEMENT

Lead Consultant Adelaide Planning and Development Solutions

In association with Beyond Ink

MFY Traffic Engineers

Project Manager

Mark Kwiatkowski

Ph 0499933311

Email mark@adelaideplanning.com.au

Document History and Status

Version	Date	Author
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ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

1.0 Application Overview

Applicant	Joyson Orchards
Property Address	202 - 204 ONKAPARINGA VALLEY ROAD AND 21 NAUGHTONS ROAD, WOODSIDE
Description of land	CTs Volume 5643 Folio 977, Volume 5440 Folio 810, Volume 5440 Folio 920
Site area	Approximately 7650 Square Metres
Development Plan	Adelaide Hills Council Development Plan Consolidated 8 August 2019
Zone	Light Industry Zone
Policy area	Light Industry (Woodside) Policy Area
Development Plan maps	AdHi/18
Existing land use	Vacant land
Development proposal	Construction of fruit packing facility with ancillary offices, parking, retaining walls, boundary fencing and landscaping.
Public notification	To be advised by Council.
Referrals	DIT
Relevant Authority	Adelaide Hills Council
Primary contact person	Mark Kwiatkowski Mark@adelaideplanning.com.au 0499933311



2.0 Introduction and Background

2.1 Introduction

This Planning Statement has been prepared by Adelaide Planning and Development Solutions (APDS) on behalf of Joyson Orchards.

Pursuant to Map AdHi/18, the subject land is located within the Light Industry (Woodside) Policy Area of the Light Industry Zone of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

In preparing this supporting statement, I can confirm that I have reviewed the proposal plans prepared by Beyond Ink, supporting documents along with the relevant provisions of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

I have also inspected the subject land and locality.

2.2 Preliminary discussions with council

A preliminary meeting was held with Council in relation to the project which considered a number of matters including

- Nature of development and proposed built form
- Access and egress arrangements including traffic movements
- Acoustic considerations
- Relocation of the Lions park bench and access to Onkaparinga Valley Road for the subject land over the Amy Gillet Bike Path
- Impacts of the proposal on road network
- Impacts on trees and tree removal
- Stormwater implications of the proposal and removal of easement

On this basis, the applicant proceeded with the development of the proposal in more detail ready for the submission of a formal planning application.

2.3 Discussions with adjoining landowners

As part of the design process, all adjoining landowners were consulted with and allowed to add input to the design process. As a result, all neighbours have provided in principle support of the proposal which has been supplied with the application documentation.



3.0 Subject Land and Locality

3.1 Subject Land



Figure 1 Subject land identified in yellow

The subject site has a street address of 202 - 204 Onkaparinga Valley Road and Naughtons Road, Woodside.

The site is located in the Light Industry (Woodside) Policy Area of the Light Industry Zone of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

The site is an irregular L shaped site consisting of three allotments with a site area of approximately 7650 square metres. The site has frontage of 44.4 metres to Naughtons Road and a frontage of 52.3 metres to Onkaparinga Valley Road.

The property of 21 Naughtons Road Woodside has vehicle access via Naughtons Road, while the properties of 202 and 204 Onkaparinga Valley Road do not have any existing vehicle access.

Onkaparinga Valley Road is an arterial road within the care and control of the Commissioner of Highways. It has an annual daily volume of 9,300 vehicles and a posted speed limit of 60 km/h adjacent the subject site.

Allotment 21 of Deposited Plan 50774 previously had an easement registered on the title for drainage purposes along the allotments north-east boundary for a length of 28.15m. The remaining allotments do not have any easements registered on titles.

The site has a slope from the rear of the site to the front of the site with flat sections towards the front portions of the site.

The subject land has connection to all services (power, water, gas, electricity, telephone).



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

3.2 Locality



Figure 2 Locality Plan

The locality consists of a mix of commercial / light industrial and rural living associated with the different zoning.

The allotments to the south and east of the subject land contain a range of commercial land uses including storage facilities, cabinet makers, light industrial land uses, food stores / agronomy store on larger commercial allotments. Further commercial land uses along Onkaparinga Valley Road include an irrigations supplier, camper trailer repair and sales, and a car yard as part of the Woodside North Business Park.

The allotment abutting the subject land to the north includes a dwelling with an additional outbuilding to the rear which is used as a motorcycle mechanic within the Light Industry Zone.

A dwelling is located on the opposite side of Naughtons Road to the north within the Watershed (Primary Production) zone and a further dwelling with outbuildings is located on the allotment on the opposite side of Onkaparinga Valley Road also within the Watershed (Primary Production) Zone, both which are screened from the subject land with extensive vegetation to the periphery of their sites.

The Amy Gillet cycle path and the Lions Gazebo and Chairs are located within the verge between the subject land and Onkaparinga Valley Road.



4.0 Planning Assessment

4.1 Nature of Development

The application seeks the construction of fruit packing facility with ancillary offices, parking, retaining walls, boundary fencing and landscaping.

The building will be the home of ADELAIDE HILLS FRESH and will also be the administration Head Office for the Joyson Group of companies which includes:

- Joyson Orchards
- Balhannah Nurseries
- Fleurieu Avocados
- Lenswood Estate Wine
- Jak Joy Livestock

Whilst primarily a packing shed for apples, in the future this could change with other fruit being processed by the plant. The plant itself will bring new technology to the region with cost benefits to more efficiently increase production and improve profitability right back to the grower.

The proposed use of the facility involves the storage and handling of primary produce (apples) which will be transported to and from the facility. The volume and nature of produce involved are such that it requires the use of heavy vehicles, and large dry and cool room storage area and receipt and dispatch processing area. The facility must also include elements that facilitate the entry, unloading, loading and exit of these large vehicles.

The proposed facility will involve

- Apples arriving from the orchard will be unloaded at the goods receipt area located at the northern end of the site, adjacent the Naughtons Road ingress
- Deliveries to and from the site will only be approximately four to six trucks per day during non-peak time and six to ten trucks per day during peak picking months.
- Collected fruit of differing size and quantities will be delivered to the site and placed into the processing machine.
- The fruit will be sorted by size and then stored in either the general storage area for short term storage and cool rooms for medium to long term storage.
- The facility will store around 40 to 50 tonnes of produce on the floor at any time.
- The facility will process between 2 - 4 dispatch semi-trailers per day with an average load of 40 to 50 tonnes of produce from the site daily.
- The size and type of produce for dispatch will vary according to season and the orders for the produce. Some produce will be loaded onto the dispatch vehicles directly from the daily incoming collection trucks, with others will be loaded with a combination of stored and daily collected produce.



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
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w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

- Packed apples leaving the facility will be collected from the loading docks at the front of the facility.
- Once packed, drivers of vehicles loaded with boxed apples will travel to the South-Eastern Freeway to distribute the goods
- Waste will be picked up twice per week.

More specifically, the proposal includes

- Construction of a large commercial building with an area of 3727 square metres comprising of
 - Receiving area for fruit (predominately apples)
 - Packing and processing area
 - Office area in the south western portion of the site
- A total of 28 carparking spaces in the south western and north western corners of the subject land.
- The building will be setback 3 metres from the southern boundary, 3.9 metres from the eastern boundary at the rear and 12.9 metres to the internal northern boundary, 7.9 metres to Naughtons Road, and a setback of 38.5 metres to the western boundary (Onkaparinga Valley Road)
- Landscaping to the western boundary and along the northern boundary and within the carparking area.
- One way access proposed through the site from Naughtons Road and exit onto Onkaparinga Valley Road.
- Retaining walls along the eastern, southern, and northern boundaries.
- 2.8 metre high acoustic boundary fencing along the northern boundary on the common boundary of the adjoining residential / commercial property
- One freestanding sign in the northern garden bed, and two fascia signs to the main building.
- Consolidation of the three titles into one title.
- Alterations to the service road (outside of the subject land)
- In terms of operation the facility will operate differently during different periods of the year.

Non-peak (May – January)

Full business operation generally Monday to Friday

Only urgent or occasional supermarket truck deliveries on the weekends or after hours Truck arrivals generally 7.00am to 2.00 pm Mon – Fri departures generally 2.00-4.30 pm Mon-Fri.

During this period, it is likely there will be 4 to 6 truck movements per day.

Peak picking season (February-April)

Full business operation generally Monday-Friday and Saturdays for arrivals to cold room storage.

Trucks arrivals generally 7.00am to 2.00 pm Mon – Fri, departures generally 2.00-6.00 pm Mon-Fri

Some arrivals for cold room storage on Saturday between 7.30-3:30 pm during peak picking time

During this period, it is likely there will be 6 to 10 truck movements per day



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 ph: 0499933311
 w: www.adelaideplanning.com.au
 a: 52A Mount Barker Road, Hahndorf SA 5245

Office Hours of operation (All year round)

Monday to Friday - Staff arrivals from 6.30am to 6.00pm

Staff parking - entry via internal service road with parking in staff car park in south western corner of the site.

We would therefore seek the hours of operation five (5) days per week (Monday to Friday) between the hours of 6:30 am to 7:00 pm and Saturday between 7.00 am to 5:00 pm during peak picking time to enable the delivery and collection of produce from the Adelaide Hills region to the facility, and processing of pallets of produce for distribution to local, interstate and international markets.

- A total of thirty staff will work at the site.
- The only other people on site will be drivers either delivering produce to the facility that has been collected from growers in the region and dispatch drivers for the delivery of the produce.

4.2 Kind of Development

Pursuant to Map AdHi/18, the subject land is located within the Light Industry (Woodside) Policy Area of the Light Industry Zone of the Adelaide Hills Council Development Plan Consolidated 8 August 2019.

The Adelaide Hills Council Development Plan Consolidated 8 August 2019, Light Industry Zone, lists a Fruit Packing Facility as neither a complying or non-complying form of development, therefore pursuant to Regulation 16 of the Development Regulations, the development should be processed via the merit pathway.

In my view, the proposal is acceptable when balanced against all the relevant provisions of the Development Plan, the existing Light Industry Zoning of the site, recent development along in the immediate locality Industry Zone and the intent of the relevant provisions of the Light Industry (Woodside) Policy Area of the Light Industry Zone.

This will be explored within the content of this report.



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
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4.3 Development Plan Assessment

4.3.1 Light Industry Zone

Objective 1 A zone accommodating a range of light industrial, storage and warehouse land uses.

Objective 2 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone will be an intensively developed, high quality, landscaped industrial area. The zone will accommodate a wide range of low-impact industrial activities including manufacturing, warehousing, transport, and distribution. The zone will be protected from the intrusion of residential and other inappropriate uses which will reduce the land resource for industrial uses or create potential for land use conflicts. High impact industrial uses will not be located within the zone.

Development within the zone will achieve generous set-backs from roads and residential development in adjoining zones in order to minimise visual amenity and environmental impacts. The appearance of the zone will be improved by additional tree planting and landscaping which 'break-up' views to buildings and structures from adjoining roads. Where industrial development is proposed adjacent to a more sensitive use or along arterial roads, substantial and effective vegetated buffers will be provided within individual development sites to reduce the visual and environmental impact of the development.

Industrial buildings will be designed to meet the needs of their intended use, however the mass and scale of buildings will be located and designed to minimise their visual impact when viewed from public roads and surrounding properties.

Building mass will be well articulated, using smaller building modules, variation in the facades and varying roof form and pitch. Building material and colour will reduce the apparent bulk of the buildings and will enhance the nature of the surrounding area, particularly for sites that are more publicly visible.

The Mount Lofty Ranges Watershed Area is of importance to Adelaide's public water supply system. The maintenance and enhancement of water quality and prevention of pollution is a priority and given the multi-use nature of the water supply catchments, a balance between best practice watershed protection and development is required.



Light Industry is defined in Schedule 1 (definitions) of the Development Regulations 2008 as

An industry where the process carried on, the materials and machinery used, the transport of materials, goods or commodities to and from the land on or in which (wholly or in part) the industry is conducted and the scale of the industry does not—

(a) detrimentally affect the amenity of the locality or the amenity within the vicinity of the locality by reason of the establishment or the bulk of any building or structure, the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit, oil, spilled light, or otherwise howsoever; or

(b) directly or indirectly, cause dangerous or congested traffic conditions in any nearby road;

With regard to the above Objectives and Desired Character statement of the Light Industry Zone, it is considered that the proposal satisfies the intent of the Development Plan in that:

- The proposal will result in a well-designed and articulated commercial building which provides a range of articulation and design elements to Onkaparinga Valley Road frontage with generous setbacks from front and side boundaries allowing for landscaping and a form which sits comfortably on the subject land.
- The proposal results in a light industrial land use as per the definition above as defined by Schedule 1 (definitions) of the Development Regulations 2008. The land use as a fruit packing facility with ancillary offices, parking, retaining walls and landscaping is in keeping with the intent of the Light Industry zone which allows for transport and distribution, warehousing and storage and other land uses which will not have a high impact on the amenity of adjoining land uses.
- The proposal has been designed to ensure no impact to Adelaide's public water supply system through an appropriately designed stormwater and waste system connecting to the existing infrastructure afforded to the subject land and wider locality.
- Appropriate landscaping is proposed to the periphery of the site which will aid in enhancing the appearance of the subject land whilst retaining trees to the periphery of the site and not impacting on adjoining trees.
- Further, the building will complement the appearance of the adjoining commercial buildings in the Woodside Business Park and will provide additional landscaping on the site.

Principle of Development Control 1 The following forms of development are envisaged in the zone:

- *light industry*
- *office in association with and ancillary to industry*



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

Principle of Development Control 3 Development should not be undertaken unless it is consistent with the desired character for the zone.

The proposal is considered to be a suitable light industrial land use within the Light Industry Zone and provides a primary land use which falls under the broad spectrum of a light industrial land use and is suitable as per the above considerations in relation to the Objectives and Desired Character statement of the Light Industry Zone.

Principle of Development Control 4 Development should be set back at least 8 metres from any road frontage, except where fronting an arterial road in which case it should be set back at least 20 metres.

The design of the building is setback 38.5 metres from the front property boundary which results in a built form which satisfies the intent of the above Principle of Development Control.

Principle of Development Control 5 Advertisements and advertising hoardings should not include any of the following:

- (a) flashing or animated signs*
- (b) bunting, streamers, flags, wind vanes and similar*
- (c) roof-mounted advertisements projected above the roofline*
- (d) parapet-mounted advertisements projecting above the top of the parapet.*

The proposed advertising in the form of a small front pylon sign, and fascia signage to the building will result in advertising / signage which will not be animated or project above the roofline of the proposed built form. A further and more detailed assessment of the suitability of the signage will be contained within the Council Wide – Advertisements assessment, but it is considered that the signage proposed is suitable.

4.3.2 Light Industry (Woodside) Policy Area

Objective 1 Provision for the development of light industrial activities.

Objective 2 The preservation of the rural character especially as viewed from Onkaparinga Valley Road.

Objective 3 Development that contributes to the desired character of the Policy Area.

DESIRED CHARACTER

The Light Industry (Woodside) Policy Area forms part of the travellers' approach to Woodside from the north. As a gateway to the town it is important that development in this area enhances the area's visual appeal.



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
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e: mark@adelaideplanning.com.au
ph: 0499933311
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Views from the Onkaparinga Valley Road will reflect a vigorous local economy and developments with a form that enhances the overall views and rural character. Large buildings will be articulated to avoid blank walls, and landscaping will be used to break up the visual appearance of large roofs and structures.

Management of vehicles and access will avoid conflict with road users on the Onkaparinga Valley Road.

Signage will enhance the appearance of the policy area and views across the town by grouping occupant signs near the main road, and by locating smaller individual business identification signs above entry doors viewable from internal roadways.

With regard to the above Objectives and Desired Character statement of the Light Industry (Woodside) Policy Area, it is considered that the proposal satisfies the intent of the Development Plan in that:

- The proposal will result in a well-designed and articulated commercial building which provides a range of articulation and design elements to Onkaparinga Valley Road with generous setbacks from front and side boundaries allowing for landscaping and a form which sits comfortably on the subject land resulting in a development which enhances the area's visual appeal. .
- The light industrial land use as a fruit packing facility with ancillary offices, parking, retaining walls and landscaping is in keeping with the intent of the zone which allows for transport and distribution, warehousing and storage and other land uses which will not have a high impact on the amenity of adjoining land uses.
- Appropriate landscaping is proposed to the periphery of the site which will aid in enhancing the appearance of the subject land whilst retaining trees to the periphery of the site and not impacting on adjoining trees.
- Signage proposed has been designed in keeping with the Council Wide provisions as they relate to Advertisements and will result in a freestanding pylon sign and fascia signage to the building will result in advertising / signage which will not be animated or project above the roofline of the proposed built form.
- Appropriate access will result in avoiding conflict with road users on the Onkaparinga Valley Road as outlined in the traffic assessment by MFY Traffic Consultants.

Principle of Development Control 1 The following forms of development are envisaged in the Policy Area:

- *light industry*
- *office in association with and ancillary to industry*
- *store*
- *warehouse.*



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

Form and Character

Principle of Development Control 2 Development should not be undertaken unless it is consistent with the desired character for the Policy Area.

The proposal is considered to be a suitable land use within the Light Industry (Woodside) Policy Area and provides a primary land use which falls under the broad spectrum of a light industrial land use and is suitable as per the above considerations in relation to the Objectives and Desired Character statement of the Light Industry Zone.

Principle of Development Control 3 Industrial development should only be undertaken if adequate measures are taken to ensure that all waste and effluent is removed from the site or treated to prevent pollution of the water catchment area, and all premises are connected to the sewer.

An appropriately designed waste system has been designed which will ensure that all waste and effluent is removed from the site or treated to prevent pollution of the water catchment area, and the premises will be connected to the sewer in keeping with the intent of the above Principle of Development Control.

Principle of Development Control 4 Development fronting Onkaparinga Valley Road should be suitably landscaped along that road frontage to minimize the visual impact of that development from the road.

The proposal includes two large garden beds along the Onkaparinga Valley Road frontage which will provide a suitable area for the provision of landscaping which will aid in minimising the visual impact of the proposed built form noting that development in the immediate locality within the Light Industry (Woodside) Policy Area.

Principle of Development Control 5 Buildings should be sited to have minimal interference with existing trees.

A predevelopment Arboricultural Assessment has been provided by The Adelaide Tree Surgery which has considered the health of the trees contained within the north western corner of the site and on the adjoining site to the north of the subject land.

A total of five trees were identified within the report including the location of two regulated River Red Gum trees to the front of the site, a further two small gum trees and an Elm tree on the adjoining site to the north.

A response from Sharon Gillam from the Native Vegetation Branch, Department for Environment and Water has considered the removal of two gum trees which indicates



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The two small gums trees that have been recommended for removal are able to be removed under the Native Vegetation Council's Guidelines for the Management of Roadside Native Vegetation and Regrowth Vegetation, under Native Vegetation Regulation 11(23) (2019), Part 3, Section 5. Clearance for access to adjoining land, where the clearance is required for normal vehicle access and is no more than 5m wide.

In relation to the regulated trees, the report indicates

The two River Red Gums will not be affected by the proposed development which includes the driveway entrance and driveway past the Protected Trees for the proposed Packaging Warehouse. The encroachment into both River Red Gums Tree Protection Zones (TPZ.s) have been calculated at less than 10% each.

The Elm Trees on the adjoining site to the north will be removed as per discussions between the adjoining land owner and the applicant.

Further recommendations in relation to Tree Protection Zones during the development phase and permeable paving are recommended in the report which will ensure that the health of the trees will not be affected.

On this basis, it is considered that the proposal will protect the health of the existing trees and will provide additional landscaping to the site which will offset the removal of the two street trees.

4.3.3 Council Wide Section

4.3.3.1 Council Wide – Advertisements

The relevant Council Wide provisions seek advertisements which enhance the appearance of the building and locality, will not cause hazards and provide signage which will ensure that the urban landscape is not disfigured by advertisements and/or advertising hoardings.

With regard to the relevant Council Wide provisions, the proposed signage on the fascia's of the buildings and the proposed freestanding sign are considered to satisfy the intent of the Development Plan in that the signs will not disfigure the urban landscape, will not create a hazard and will enhance the appearance of the buildings and the immediate locality. The proposed signage will result in a coordinated appearance over the site and will complement the architectural form of the proposed built form. The fascia signage associated with the proposed built form will not result in clutter, disorder, or untidiness on the buildings and will sit comfortably on the site and will identify the proposed land use and will also reflect the signage in the wider commercial locality.



Further, the signage will not cause discomfort to approaching drivers, or to create difficulty in the driver's perception of the road, will be easily interpreted, will not distract drivers, and will not obstruct views of pedestrians or the road activities

On this basis it is considered that the signage is in keeping with the intent of the signage provisions envisaged in the Development Plan.

4.3.3.2 Council Wide – Design and Appearance

The Council Wide - Design and Appearance Objectives and Principles of Development Control seek well-designed buildings which provide a high standard of design using a range of colours and materials complimenting the built form and rural / hills face environment.

The application proposes a well-designed industrial building incorporating a range of architectural elements which integrate of a range of colours and materials and articulation to add visual interest. The proposal will complement the existing industrial environment and sit comfortably on the site with appropriate siting and landscaping in keeping with the intent of the Light Industry Zone and more specifically the Light Industry (Woodside) Policy Area.

The proposal will provide a built form on the site with appropriate setbacks from boundaries and proposes a formalised parking area to the front portion of the building and allows for suitable traffic movements through the site. The proposal will result in acceptable impacts on adjoining properties and the articulated built form will not result in an inappropriate visual impact when viewed from the immediate or wider locality.

The massing of the building is carefully managed, firstly through the size which presents a built form to what would reasonably be expected within the Light Industry Zone with a commercial appearance towards the rear of the site matching adjoining large shed structures in association with the processing areas and an articulated built form to the front of the site as it relates to Onkaparinga Valley Road associated with the office component.

The proposed building has a form that is consistent with the building height of adjoining properties and provides the use of a range of materials providing variation in wall and roof lines proposed, wall materials and colours. The development as a whole including driveways and parking area has been designed to ensure the structures are appropriately located to allow for access through the site for pedestrians and vehicles whilst being functional. The design of the site has been focused on creating a safe, well-designed pedestrian environment and a functional space.



The proposal has been designed with facades which avoid extensive areas of uninterrupted walling facing areas exposed to public view. The external walls and roofs of the additions proposed buildings will not incorporate highly reflective materials and will not result in glare to neighbouring properties, drivers, or cyclists. The building has been designed to complement and harmonise with the character of the existing approved buildings in the immediate locality.

It is considered that the proposed building has been designed to satisfy the requirements of the Development Plan in relation to design and appearance and will result in an appropriate form which will sit comfortably within the Woodside North Business Park and Onkaparinga Valley Road streetscape complementing the existing built form on adjoining sites.

4.3.3.3 Council Wide – Bulk Handling and storage facilities

The Council Wide - Bulk Handling and Storage Facilities Objectives and Principles of Development Control seek developments which have been designed to minimise adverse impacts on the landscape and on and from surrounding land uses whilst being located within appropriate zones.

The proposal results in a development which is located within the Light Industry Zone and has been designed and sited to minimise risks of contamination to the environment and adverse impacts on nearby sensitive land uses and from surrounding land uses.

The proposal has been designed to allow for suitable access and manoeuvring of vehicles through the site providing appropriate vehicle circulation between activity areas through the site, offers a sealed internal driveway and parking areas (parking areas will be gravel or granitic sand or other) to control dust emissions from the site. Vehicles will enter the site from Naughtons Road and exit the site via Onkaparinga Valley Road ensuring vehicles enter and exit in a forward manner.

The proposal provides appropriate landscaping to the periphery of the site for the purpose of providing shade and shelter assisting with screening and dust filtration to adjoining properties and the street. Further, appropriate fencing is proposed which will aid in providing adequate acoustic buffering in addition to the security of the perimeter of the site.



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
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w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

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4.3.3.4 Council Wide – Crime Prevention

With regard to the Council Wide section – Crime Prevention Objectives and Principles of Development Control relating to Crime prevention, the design of the proposal provides a fruit packing facility with ancillary offices which will allow increased casual surveillance from users and traders on the site.

The location and design of the main building and open nature of the building layout on the site allows for clear sightlines through the site with passive surveillance over the car park and internal driveway from the building and surrounds.

The materials proposed will provide a robust environment that is resistant to vandalism and graffiti and will withstand normal use. The design building and additions minimises areas for vandalism and graffiti. In any event, should the buildings be vandalised, this would be immediately removed from the site by the tenants.

Whilst the final detail of the lighting to the carparking area has not been finalised, all lighting of the site will be of a low level and designed in a manner to ensure that the subject land is appropriately lit whilst not impacting on the amenity of adjoining land uses. Any lighting used at the site will comply with AS 4282-1997-Control of the Obtrusive Effects of Outdoor Lighting, and AS 1158.1 Public Lighting Code for the illumination level of the car parking area

Access to the buildings from the carpark and internal driveways will not result in pedestrian entrapment spots and the open nature of the parking area will allow for casual surveillance of the site.

It is considered that the proposal will result in a safe, secure, crime resistant environment which satisfies the intent of the Development Plan in relation to Crime Prevention.

4.3.3.5 Council Wide – Hazards

With regard to the Council Wide section – Hazards, the proposal should be designed to ensure the maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk, whilst being located and designed to minimise the risks to safety and property from flooding.

A catchment Analysis has been provided by Mlei Consulting Engineers. The report indicates

MLEI Consulting Engineers were engaged to do a catchment analysis for the network contributing to a discharge location at the corner of Naughtons and Onkaparinga Valley Roads. The purpose of the investigation is to determine the change in peak flows as a result of the increased runoff from the development of the proposed apple facility and any increased burden on the existing stormwater infrastructure.



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e: mark@adelaideplanning.com.au
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Stormwater peak flow rates have been determined for both the existing site and the proposed development. There is expected to be an increase in the total peak flow for the proposed development when compared to the existing site. Looking at the overall catchment at the point of discharge, the site is unlikely to have a major impact on overall peak flow based on the proximity to outlet and the upstream catchment's time of concentrations.

The volumes are computed by using DRAINS modelling. The calculations are based on the Horton/ILSAX method using the 1 in 20 year (5% AEP) peak storm to determine the runoff of stormwater from the site.

The results of the analysis find a negligible increase (0.23%) in peak flow from the catchment in the post-development scenario of the Woodside Apple Facility, in comparison to the current rural state of the site for the 5% AEP storm event.

Based on the findings of the analysis and the location being within several metres from the Onkaparinga River and no directly downstream properties, it would seem appropriate for unrestricted discharge of stormwater from the development, providing safe stormwater management practices are in place for major events. The expectation from the results is that the development will not create a sizeable increased burden to the existing network.

On this basis, it is considered that the proposal will satisfy and meet the requirements of the Development Plan in relation to the impacts on flooding and will not adversely affect flood levels or create a sizeable increased burden to the existing network.

4.3.3.6 Council Wide – Interface between land uses

The Council Wide - Interface between Land Uses seeks development that does not cause unreasonable interference, particularly referencing the emission of effluent, odour, smoke, fumes, and dust or other airborne pollutants, noise, hours of operation and traffic impacts.

Further the development plan seeks development to be designed and sited to minimise negative impact on existing and potential future land uses considered appropriate in the locality and be designed, constructed, and sited to minimise negative impacts of noise and to avoid unreasonable interference.

The proposed hours of operation of the packing facility will generally be from 6:30 am until 7:00 pm Monday to Friday with additional hours during peak season from 7:00 am to 3:30 pm on Saturdays as outlined in section 4.1 of the report.



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The proposed nature of the proposal within the Light Industrial Zone and proposed hours of operation will have an acceptable impact on the adjoining locality and is consistent with the hours of operation of other non-residential land uses in the immediate and wider locality and the hours envisaged by the Environment Protection (Noise) Policy 2007.

Within areas of the Light Industry Zone where there are commercial activities, it is envisaged that a range of impacts will occur to adjoining properties including noise, traffic, hours of operation and will be more significant when compared with a traditional residential area during certain periods of the day. When considering the impacts of the proposal, it should be noted that two residential allotments are located in close proximity to the subject land, one of which abuts the site to the north and one on the opposite side of Naughtons Road. There is also a residential allotment on the opposite side of Onkaparinga Valley Road.

A preliminary review of the proposal has been undertaken by Sonus to determine existing operational noise levels from the existing facility at Monarto and collection of noise from the Woodside site to inform the influence of Onkaparinga Valley Road on the existing environment. The preliminary review indicated

The site is located in a Light Industry Zone of the Adelaide Hills Council Development Plan.

The nearest residence, being the one adjacent to the site to the north, is also in the Light Industry Zone. The next nearest residence on the opposite side of Naughtons Road is in a Watershed (Primary Production) Zone.

Based on this zoning, the Environment Protection (Noise) Policy 2007 (the Policy) provides noise level criteria to be achieved at the nearby residences. The criteria are influenced by different factors, such as the zoning, the development status of the project and the character of the noise and compliance with the criteria ensures compliance with the Development Plan provisions relating to noise.

The goal noise levels to be achieved by operations at the site when considered at the nearest dwellings are:

- 47 dB(A) to be achieved during daytime operation, from 7am to 10pm
- 40 dB(A) to be achieved during nighttime operation, before 7am and after 10pm.

The Policy enables the contribution of other noise sources such as Onkaparinga Valley Road to be taken into account. Monitoring was conducted at the site between 2 September and 16 September. The lowest background noise level (the noise level during lulls in traffic) measured during the week, i.e. Monday to Friday, between 6:30am and 7:00am was 43 dB(A).

In this circumstance, the Policy enables the criteria for the night period to be increased to 43 dB(A) (during the week).



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e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

The proposal provides a 2.8 metre high acoustic fence to the northern boundary and additional landscaping in the form of compact pencil pines which will result in a suitable acoustic screen providing a suitable acoustic barrier to the adjoining residential property.

From an operational perspective, electric forklifts only, removing forklift beeps and replace with low flashing lights and fitted with white noise muffler filters and trucks and truck refrigeration units be turned off during unloading and loading.

The majority of the activities associated with the fruit packing facility will be contained within the proposed building with the exception of the movement of vehicles for loading and unloading. The proposed activities and hours of operation will ensure there will be limited noise or light spill impacts to the adjoining allotments and will have an acceptable impact in keeping with the EPA Noise Policy.

It is considered that the noise levels at dwellings in the wider locality will not be impacted by the proposal and the noise when measures from any dwellings from activity at the site is predicted to achieve the conservatively applied requirements of the Environment Protection (Noise) Policy 2007.

The proposal will have an acceptable impact on the existing road network which is consistent with other light industrial land uses in the immediate and wider locality. The proposed access point from Naughtons Road and egress to Onkaparinga Valley Road will not result in additional detrimental impacts from traffic movements to adjoining properties and will be consistent with the current existing traffic movements along Naughtons Road and Onkaparinga Valley Road. Further the formalisation of the parking area will result in safe and convenient access and parking through the site.

With regard to lighting, whilst the final detail of the internal lighting to the site have not been finalised, it is likely that the site will be lit with low level lighting surrounding the building which will be designed in a manner to ensure that the subject land is appropriately lit whilst not impacting on the amenity of adjoining land uses including along pedestrian pathways and main access routes in accordance with Australian Standard AS 4282-1997 Control of the obtrusive effects of outdoor lighting.

With regard to the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants, vibration, electrical interference and glare, the proposal will have an acceptable impact given distances to adjoining properties.

On balance, It is considered that the proposal will have an acceptable impact on the amenity of the adjoining properties in accordance with the Council Wide requirements of the Development Plan as it relates to Interface between Land Uses.



4.3.3.7 Council Wide – Landscaping, Fences and Walls

The Council Wide – Landscaping, fences and walls seeks that the amenity of land and development being enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible as well as functional fences and walls that enhance the attractiveness of development

As aforementioned, the proposal will result in the construction of a 2.8 metre high acoustic fence to the northern boundary abutting the adjoining residential property and further 1.8 metre high security fencing to the periphery of the site to maintain privacy and security without adversely affecting the visual amenity or access to sunlight of adjoining land. The fencing height to the adjoining boundary with the residential property was at the request of the landowner and will result in an appropriate acoustic screen between the two properties. Naughton Road entry will also be secured with an automatic sliding gate which will be remotely operated and remain closed during no operational hours.

Provision for suitable landscaping areas have been provided which identifies the areas of landscaping and provides for a range of species to be planted including trees, shrubs and ground covers which will provide adequate screening and designed to not create concealment points. The landscaping will aid in complementing the built form and reducing the visual impact of the buildings whilst screening service yards, loading areas, outdoor storage areas. Appropriate species will be selected which require minimal maintenance requirements and watering requirements, promote water and biodiversity conservation, and will result in appropriate clearance from power lines and other infrastructure.

On this basis, it is considered that the proposal has been designed to satisfy the relevant provisions of the Development Plan as they relate to Landscaping, Fences and Walls.

4.3.3.8 Council Wide – Orderly and Sustainable Development

The proposal on the site is considered appropriate and orderly and consistent with the relevant provisions of the Development Plan in that

- The proposal expands the economic base of the region in a sustainable manner and value adds to range of land uses within the Light Industry Zone on the Woodside North Business Park.
- The proposal which will ensure a safe convenient and pleasant environment for uses and adjoining landowners and provides significant investment to the locality.
- The proposal does not prejudice the zone for its intended purpose as outlined in the above assessment against the Zone provisions and is consistent with the intended of the site and adjoining land uses within the Light Industry Zone and adjoining Watershed (Primary Production) Zone.
- Will not jeopardise the mix of land uses in the surrounding allotments,



- The site will utilise the existing facilities and services to the site (power, access to roads etc.) and will not jeopardise the continuance of the adjoining land uses within the Light Industry Zone and adjoining Watershed (Primary Production) Zone
- The proposed development does not undermine the objectives of the zone and protects the character and amenity of the light industrial character of the locality.
- Provides safe and convenient traffic movements and sufficient on-site car parking.
- Results in a development which retains and protects the natural resources and environment.
- Provides an environmentally sustainable and innovative tourism development that does not adversely affect the use of the land or adjoining properties.

On this basis it is considered that the proposal satisfies the above provisions in relation to orderly and sustainable development.

4.3.3.8 Council Wide – Transportation, access, and parking

The relevant transportation and access objectives and principles promote development that provides for the safe and efficient movement of all motorised and non-motorised transport modes. Given the small-scale nature of the amendments to the development and the hours of operation proposed, the proposal will not result in unacceptable traffic movements to and from the site.

A review of the proposal has been undertaken by MFY Traffic Consultants in relation to the traffic and parking aspects of the proposal and provided advice in respect to access requirements noting that the proposal has been discussed with the Department of Infrastructure and Transport (DIT) and Council.

The report indicates

- Access for the proposal will be provided via Naughtons Road and the service road. The Naughtons Road access will be an ingress for commercial vehicles servicing the proposed development. The service road access will provide for two-way movements for staff parking and egress movements for commercial vehicles.
- The proposed crossovers will be designed in accordance with Australian/New Zealand Standard, Parking Facilities Part 1: Off-street car parking (AS/NZS 2890.1:2004) and Australian Standard, Parking Facilities Part 2: Off-street commercial vehicle facilities (AS 2890.2:2002). The existing crossover to the site will be closed.
- The largest vehicle used for delivery will be the 19.0 m semi-trailers. Such vehicles will enter the site via Naughtons Road and exit to the service road.
- The proposed access to the service road will bisect the section of Amy Gillet Bikeway adjacent the site. The access design has ensured that there will be adequate sightlines to cyclists and pedestrians on the shared path.



ABN 55 289 434 618
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 Town Planning Specialists and Planning Private Certifiers
 e: mark@adelaideplanning.com.au
 ph: 0499933311
 w: www.adelaideplanning.com.au
 a: 52A Mount Barker Road, Hahndorf SA 5245

- Parking for the proposed development will be provided within the subject site. The parking area will comprise of 28 parking spaces. Additional parking will be provided along the northern boundary of the site and will be allocated to staff. The design of the parking spaces will generally comply with and will comply with AS/NZS 2890.1:2004.
- The proposal will include one parking space for use by people with disabilities. This space will be 5.4 m long and 2.4 m wide and adjacent to a similarly sized shared space, as per the dimensional requirements of the Australian/New Zealand Standard, Parking Facilities Part 6: Off-street parking for people with disabilities (AS/NZS 2890.6:2009).

With regard to the parking assessment, the Adelaide Hills Council's Development Plan (consolidated 8 August 2019) identifies the following parking provision rate for industry, store, and warehouse type developments:

3.3 spaces per 100 m² total floor area of the office component, plus for the non-office component:

- 2 spaces per 100 square metres up to 200 square metres;
- 1.33 spaces per 100 square metres between 200 and 2000 square metres; and
- 0.67 spaces per 100 square metres over 2000 square metres.

Based on the above rates, the proposal will generate a parking requirement of two spaces for the office component; plus 38 spaces for the non-office component. If Council rates were adopted, therefore, there would be a total requirement for 40 parking spaces

The above rates, however, would apply to a facility with a much higher staffing level than the subject proposal. In such circumstances, the most accurate methodology to assess the anticipated parking demand is to use a first principles assessment.

The proposed facility will be largely automated. The proposal will include two machines that will pre-size and sort the apples and three packing lines. The following staffing levels are anticipated to be required for each operation:

- two staff per machine for the pre-sizing and sorting operations which are less labour intensive and four staff per packing lines.

In addition, there will be a requirement for the office and storage components of the proposal. In regard to the storage area, the Transport for New South Wales (formerly RMS) "Guide to Traffic Generating Development" specifies a rate of one space per 300 m² for warehouses. Based on this rate, the storage area will generate a requirement for two spaces.



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 e: mark@adelaideplanning.com.au
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It is expected that the proposal would, therefore generate a demand for 20 parking spaces. Such a demand would be readily accommodated for within the proposed parking provision.

It is therefore considered that the proposal has been designed to satisfy the intent of the Development Plan as it relates to Transportation and Access in that the proposed apple sorting facility will be designed to ensure that all vehicles would be able to safely enter and exit the site in a forward direction. Access and parking for the proposal will comply with AS/NZS 2890.1:2004 and AS 2890.2:2018. The proposal has also considered the transport route to and from the site, given the existing substandard intersection of the service road with Onkaparinga Valley Road. Minor modifications to the service road will provide for safe turning movements of semi-trailers associated with the proposal. These modifications will also provide for improved safety for existing users of the service road. Adequate parking spaces will be provided to cater for the anticipated peak parking demand associated with the development and the traffic generated by the proposal will have minimal impact and will not change the nature or function of the adjacent road network.

4.3.3.9 Council Wide – Waste

The Development Plan seeks to ensure that development suitably manages the impacts from waste so that it does not impacts on the natural environment. The storage, treatment, and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.

The proposal will promote waste minimisation and reuse and recycling by providing areas within the building waste storage which will allow for onsite collection and sorting of recycling materials and the storage of fruit. All produce will be stored within the building and were required in refrigerated cool rooms. It is not in the operations commercial interest to let produce rot. There will be no odour sources. The waste area will be appropriately screened and will provide sufficient space for the storage of waste receptacles. Waste will be collected by a private contractor as required.

All outside areas not allocated to landscaping or structures will be sealed. As previously noted, the proposal incorporates high quality perimeter landscaping. No goods will be stored outside of the building. Dust emission from the site is not an issue. The issue will be ensuring dust does not enter the site.

Stormwater is proposed to be captured, stored, and reused on site with overflow diverted to the street water table to Council standards.

During the construction process, where possible all construction waste will be minimised.

On this basis the proposal satisfies the relevant Objectives and Principles of Development Control as they relate to Waste.



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e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

4.3.3.10 Council Wide – Regulated /Significant trees

A predevelopment Arboricultural Assessment has been provided by The Adelaide Tree Surgery which has considered the health of the trees contained within the north western corner of the site and on the adjoining site to the north of the subject land.

A total of five trees were identified within the report including the location of two regulated River Red Gum trees to the front of the site, a further two small gum trees and an Elm tree on the adjoining site to the north.

A response from Sharon Gillam from the Native Vegetation Branch, Department for Environment and Water has considered the removal of two gum trees which indicates

The two small gums trees that have been recommended for removal are able to be removed under the Native Vegetation Council's Guidelines for the Management of Roadside Native Vegetation and Regrowth Vegetation, under Native Vegetation Regulation 11(23) (2019), Part 3, Section 5. Clearance for access to adjoining land, where the clearance is required for normal vehicle access and is no more than 5m wide.

In relation to the regulated trees, the report indicates

The two River Red Gums will not be affected by the proposed development which includes the driveway entrance and driveway past the Protected Trees for the proposed Packaging Warehouse. The encroachment into both River Red Gums Tree Protection Zones (TPZ.s) have been calculated at less than 10% each.

The Elm Trees on the adjoining site to the north will be removed as per discussions between the adjoining land owner and the applicant.

Further recommendations in relation to Tree Protection Zones during the development phase and permeable paving are recommended in the report which will ensure that the health of the trees will not be affected.

On this basis, it is considered that the proposal will protect the health of the existing trees and will provide additional landscaping to the site which will offset the removal of the two street trees.



5.0 Conclusion

After careful consideration of the proposed development and having regard to the relevant provisions of the Development Plan, it is my opinion that the proposal is not seriously at variance with the Light Industry (Woodside) Policy Area of the Light Industry Zone and Council Wide provisions of the Adelaide Hills Council Development Plan Consolidated 8 August 2019

The application seeks the construction of fruit packing facility with ancillary offices, parking, retaining walls, boundary fencing and landscaping.

The proposal will result in a form of development which is in keeping with other development in the immediate locality and will complement the mix of uses within the Woodside North Business Park and wider locality and will result in a development which will have an acceptable impact on the amenity of the adjoining properties. Further the proposal is not considered to adversely impact future development of the locality and/or detrimentally impact upon any surrounding development.

The proposed building incorporates a range of architectural elements including of a range of colours and materials, articulation to add visual interest to the streetscape. The proposal will complement the existing Onkaparinga Valley Road environment and sit comfortably within the existing streetscape with appropriate siting and landscaping in keeping with the intent of the Light Industry Zone.

The buildings have been designed in accordance with the advice of MFY Traffic Engineers to ensure buildings on the site are appropriately located to allow for access through the site for both vehicles and pedestrians. The design of the site has been focused on creating a safe, well-designed vehicle and pedestrian environment. The design of the site results in a coordinated appearance that maintains and enhances the visual attractiveness of the locality.

The proposed development will provide parking to adequately accommodate the demand associated with the proposed development in accordance with relevant Australian Standards. The proposed car parking and access arrangements have been designed to ensure vehicles enter and exiting the site in a forward direction. Access arrangements will be maintained for the site, with design modifications to facilitate entry and exit movements of delivery vehicles in a forward direction. Traffic generation associated with the proposed development will not significantly impact on the surrounding road network.

The proposal will be formally referred to DIT through the formal referral process however preliminary considerations with these departments have determined that the proposal will satisfy the standard agency requirements.

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a: 52A Mount Barker Road, Hahndorf SA 5245

Having regard to all the relevant provisions of the Development Plan, for the reasons aforementioned, it is my opinion, that the application represents an appropriate form of development in the context of the Light Industry (Woodside) Policy Area of the Light Industry Zone and Council Wide provisions of the Adelaide Hills Council Development Plan Consolidated 8 August 2019 and the unique circumstances of the subject land and locality.

Accordingly, and on balance, the proposed development is not considered to be seriously at variance with, and sufficiently satisfies, the relevant provisions of the Development Plan and Development Plan Consent is warranted.

We look forward to the support of the proposal in its current form. Should you have any queries or require any further information or clarification with any components of this statement, please do not hesitate to contact by contacting me by email mark@adelaideplanning.com.au.

Yours Sincerely,



Mark Kwiatkowski MPIA CPP

Director + Principal Urban Planner

Adelaide Planning & Development Solutions - Town Planning Specialists | Planning Private Certifiers



ABN 55 289 434 618
Adelaide Planning & Development Solutions Pty Ltd
Town Planning Specialists and Planning Private Certifiers
e: mark@adelaideplanning.com.au
ph: 0499933311
w: www.adelaideplanning.com.au
a: 52A Mount Barker Road, Hahndorf SA 5245

ADELAIDE HILLS COUNCIL
RECEIVED 26/11/2020

MLM/20-0156



12 November 2020

Traffic • Parking • Transport

Unit 6, 224 Glen Osmond Road
FULLARTON SA 5063

T: +61 8 8338 8888

F: +61 8 8338 8880

E: mfy@mfy.com.au

W: mfy.com.au

MFY Pty Ltd

ABN 79 102 630 759

Mr Mark Kwiatkowski
Adelaide Planning and Development Solutions
52A Mount Barker Road
HAHNDORF SA 5245

Dear Mark,

PROPOSED APPLE SORTING FACILITY, 198-200 ONKAPARINGA VALLEY ROAD, WOODSIDE

I refer to the proposed apple sorting facility to be constructed at the above site. As requested, I have reviewed the traffic and parking aspects of the proposal and provided advice in respect to access requirements. In undertaking the assessment, I have liaised with the Department of Infrastructure and Transport (DIT) and Council.

The assessment has been based on Beyond Ink Drawing No. PD001 dated 11 November 2020.

1 EXISTING SITUATION

The subject site has frontages to Naughtons Road and a service road on the eastern side of Onkaparinga Valley Road. Access to the site is provided via Naughtons Road.

Onkaparinga Valley Road is an arterial road within the care and control of the Commissioner of Highways. It has an annual daily volume of 9,300 vehicles and a posted speed limit of 60 km/h adjacent the subject site.

The service road provides access to adjacent allotments, on which a number include commercial developments and the balance are undeveloped. The service road intersects with Onkaparinga Valley Road at its southern end and at the Onkaparinga Valley Road/Naughtons Road intersection.

The existing intersections with the service road appear to have been created without having specific regard to recommended design criteria (as is historically the case with much of the road network in the Adelaide Hills) as there are a number of alignment and safety deficiencies. The following aspects of the design are particularly relevant to note:

- the angle of the southern approach of the service road with Onkaparinga Valley Road is currently approximately 12 degrees which is significantly more acute than the recommended

minimum angle of 70° (reference Austroads “*Guide to Road Design – Part 4A: Unsignalised and Signalised Intersections*”). This compromises sight lines for drivers exiting the intersection. Figure 1 illustrates the angle of the existing intersection;



Figure 1: Angle of existing intersection

- Drivers of large vehicles exiting the service road are not able to turn right to Onkaparinga Valley Road from the southern intersection. While the intent of the service road may have been to provide for right turn movements at the northern end, there is no traffic control prohibiting right turn movements at the southern intersection. Figure 2 illustrates a right turn movement by a semi-trailer at the intersection; and

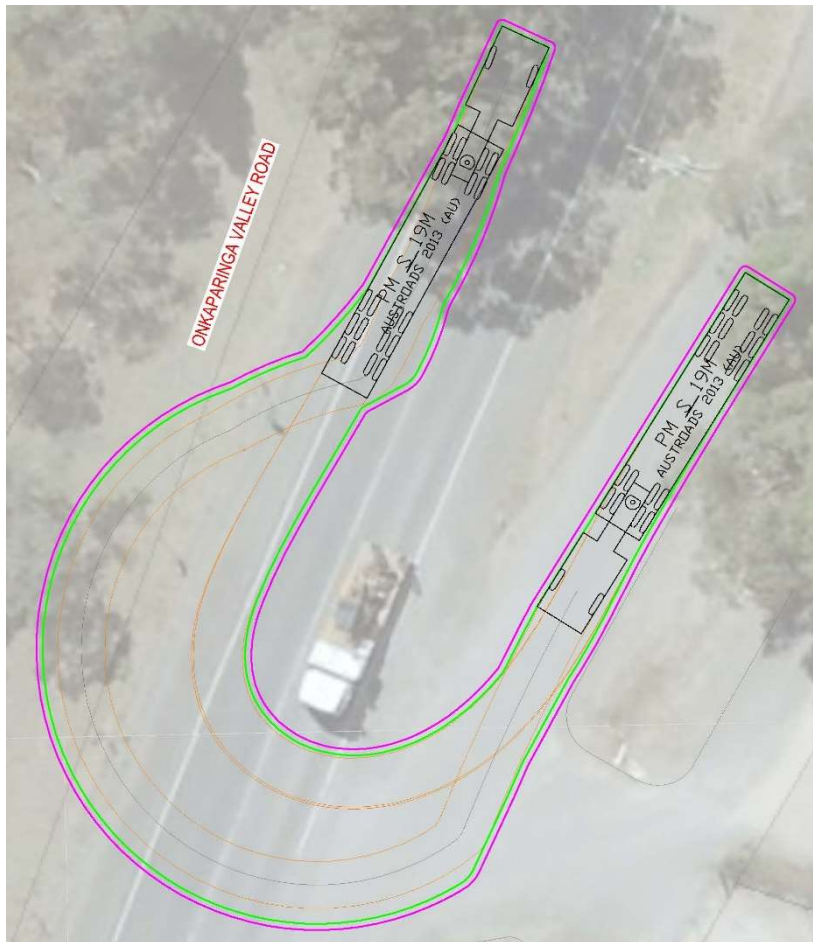


Figure 2: right turn movement by semi-trailer restricted at intersection

- the above two issues are compounded by the open drain adjacent the service road, which limits the flexibility for turning vehicles.

Naughtons Road is a local road within the care and control of Council. It is located along the northern extent of the township of Woodside, with Commercial development along the southern side and rural properties along the northern side. The road is sealed between Onkaparinga Valley Road and Henry Road. East of Henry Road, Naughtons Road is unsealed.

Naughtons Road forms an intersection with Onkaparinga Valley Road and the service road. All movements are permitted at the intersection.

The Amy Gillet Bikeway is located along the western boundary of the site. It forms an intersection with Naughtons Road to the east of the Onkaparinga Valley Road/Naughtons Road intersection.

2 PROPOSAL

The proposal is for the development of an apple sorting and packing facility. The development will include a building with a floor area of approximately 3,885 m². The uses within the building will comprise of the following components:

- approximately 145 m² of office area plus ancillary areas;
- approximately 810 m² of storage area;
- approximately 2,650 m² of processing area; and
- 30 parking spaces.

Apples which will be sorted and packed at the facility will be farmed at an apple orchard located to the south of the site and will be then distributed for sale.

2.1 ACCESS

Access for the proposal will be provided via Naughtons Road and the service road. The Naughtons Road access will be an ingress for commercial vehicles servicing the proposed development. The service road access will provide for two-way movements for staff parking and egress movements for commercial vehicles.

The proposed crossovers will be designed in accordance with Australian/New Zealand Standard, *Parking Facilities Part 1: Off-street car parking (AS/NZS 2890.1:2004)* and Australian Standard, *Parking Facilities Part 2: Off-street commercial vehicle facilities (AS 2890.2:2002)*. The existing crossover to the site will be closed.

The largest vehicle used for delivery will be the 19.0 m semi-trailers. Such vehicles will enter the site via Naughtons Road and exit to the service road. Figure 3 illustrates that a semi-trailer will be able to enter and exit the proposed development.



Figure 3: Semi-trailer entering and exiting the subject site

The proposed access to the service road will bisect the section of Amy Gillet Bikeway adjacent the site. The access design has ensured that there will be adequate sightlines to cyclists and pedestrians on the shared path.

The levels of the site will necessitate that a ramp be provided at the ingress on Naughtons Road and within the circulation road along the northern boundary of the site. The ramp will comply with Australian Standard, *Parking Facilities Part 2: Off-street commercial vehicle facilities (AS 2890.2:2018)* and will include appropriate transitions to provide for semi-trailer access.

2.2 SERVICE ROAD

In developing the access solution for the site, consideration was not only given to the direct access arrangements for the site but also to the accessibility to the broader road network, given that the proposal will increase the volume of semi-trailers using the service road. The following options were considered:

- A direct access being provided across the service road to Onkaparinga Valley Road; and
- Utilising the service road to then gain access to Onkaparinga Valley Road.

While a direct access could be provided to Onkaparinga Valley Road, the clearance between the two roads is insufficient to provide for queuing at the access. This could result in conflict between a vehicle stored waiting to turn to Onkaparinga Valley Road and a vehicle using the service road. Accordingly, it was considered safer to utilise the service road as the exit route.

Notwithstanding the above, it has been previously identified that the angle of the southern intersection of the service road and Onkaparinga Valley Road is deficient in respect to its intersecting angle. While this is an existing situation, it would have two ramifications for drivers exiting the subject development, namely:

- drivers of delivery vehicles would have restricted line of sight; and
- drivers of larger vehicles would not be able to turn right to Onkaparinga Valley Road.

A review of options to improve these deficiencies have therefore been considered as part of the development. In regard to the angle of approach of the intersection, minor widening of the service road on the approach to Onkaparinga Valley Road will enable the angle of approach to be increased the 67°. While this angle is still less than the recommended 70°, it is a substantial improvement when compared with the existing scenario. Further, the angle of sight lines for a truck driver will meet the 50° angle where sight lines are available, as illustrated in Figure 4

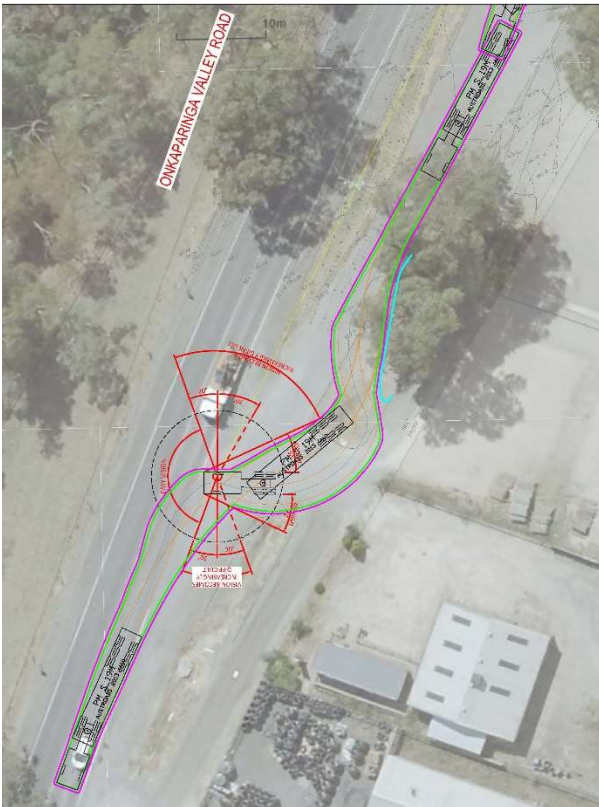


Figure 4: Sight line angle which would be achieved at modified intersection

The widening of the service road would only be required south of the proposed access and could be achieved by installing a pipe within the existing open drain adjacent the carriageway. There would be no impact on trees and no further works required at the actual intersection with Onkaparinga Valley Road.



Figure 5: Proposed minor widening of the service road

In regard to the right turn constraint at the intersection, the proposal will not generate a demand for right turning movements of large commercial vehicles. The orchard is located to the south of the site and hence apple delivery vehicles will enter via a right turn to Naughtons Road and exit via a left turn to the service road and then Onkaparinga Valley Road.

Once packed, drivers of vehicles loaded with boxed apples will travel to the South-Eastern Freeway to distribute the goods. This will require a left turn to the service road and Onkaparinga Valley Road.

Notwithstanding the above, it is proposed that a traffic management plan will be developed that requires all drivers of commercial vehicles to turn left to both the service road and Onkaparinga Valley Road when exiting the site. This will be supplemented with an All Traffic Left sign at the exit from the site.

In the unlikely event that a driver of a commercial vehicle wished to travel north, an alternate travel route is available via Charles Street, Henry Street and Naughtons Road. The intersections of these roads are designed to accommodate semi-trailer movements and drivers will be able to safely navigate this route to then proceed north.

Accordingly, there will be no requirement for drivers of commercial vehicles associated with the site to turn right to Onkaparinga Valley Road at the southern service road intersection. That said, the proposed works to the service road will improve the ability for such a turn to be executed. Accordingly, while such a movement is not proposed in association with the subject development, the modifications to the service road will at least mean that this turn can be executed within the formed section of Onkaparinga Valley Road (inclusive of the shoulder), as illustrated in Figure 6.

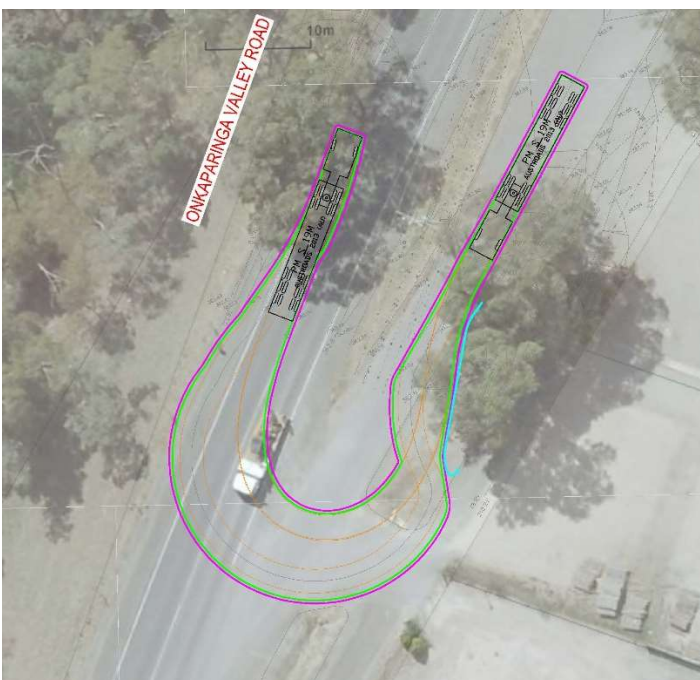


Figure 6: Right-turn movement to Onkaparinga Valley Road via the service road

The proposal will, therefore, also effect an improvement to the safety of existing movements at the intersection, should such a right turn be executed.

2.3 LOADING

Apples arriving from the orchard will be unloaded at the goods receive area located at the northern end of the site, adjacent the Naughtons Road ingress. Figure 7 illustrates a 19.0 m semi-trailer accessing the unloading area.

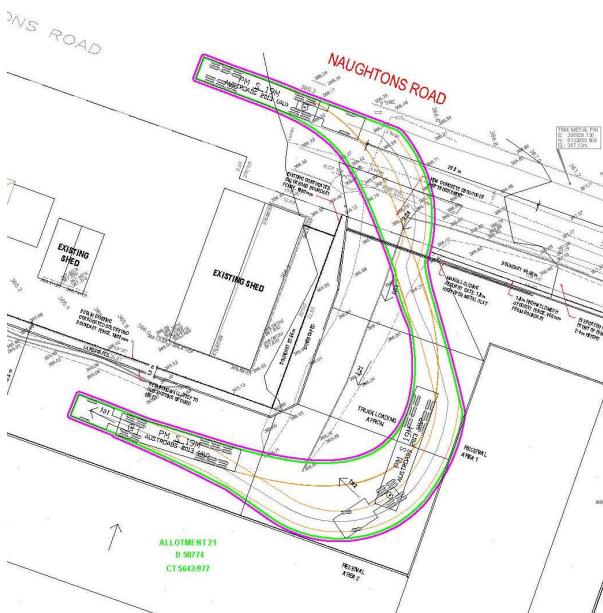


Figure 7: 19.0 m semi-trailer accessing goods receive area

Packed apples leaving the facility will be collected from the loading docks at the western end of the facility. Figure 8 illustrates a semi-trailer accessing the loading docks.

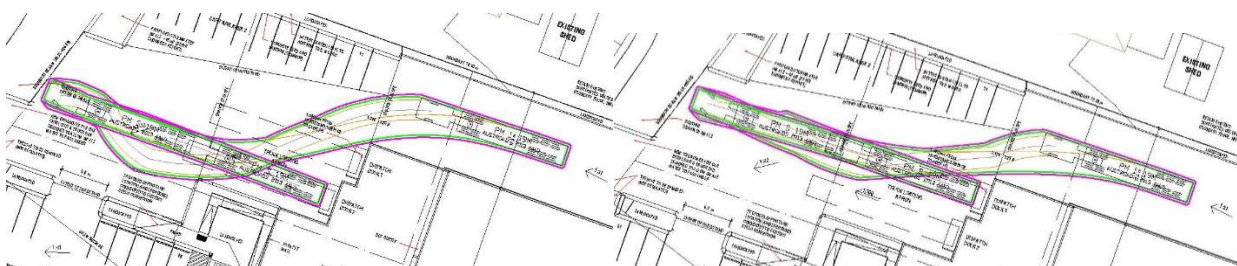


Figure 8: 19.0 m semi-trailers accessing the western loading area

Note the proposal does not seek independent access to the loading docks for semi-trailers but rather flexibility to load at either location. Two docks will not be used simultaneously. Importantly all delivery vehicles will be able to manoeuvre safely on the site and will enter and exit the site in a forward direction.

2.4 PARKING

Parking for the proposed development will be provided within the subject site. The parking area will comprise of 20 parking spaces. Additional parking will be provided along the northern boundary of the site and will be allocated to staff. The design of the parking spaces will generally comply with and will comply with *AS/NZS 2890.1:2004*.

The proposal will include one parking space for use by people with disabilities. This space will be 5.4 m long and 2.4 m wide and adjacent to a similarly sized shared space, as per the dimensional requirements of the Australian/New Zealand Standard, *Parking Facilities Part 6: Off-street parking for people with disabilities (AS/NZS 2890.6:2009)*.

3 PARKING ASSESSMENT

Adelaide Hills Council's Development Plan (consolidated 8 August 2019) identifies the following parking provision rate for industry, store and warehouse type developments:

- 3.3 spaces per 100 m² total floor area of the office component, plus for the non-office component:
 - 2 spaces per 100 square metres up to 200 square metres;
 - 1.33 spaces per 100 square metres between 200 and 2000 square metres; and
 - 0.67 spaces per 100 square metres over 2000 square metres.

Based on the above rates, the proposal will generate a parking requirement of:

- five spaces for the office component; plus
- 32 spaces for the non-office component.

If Council rates were adopted, therefore, there would be a total requirement for 37 parking spaces.

The above rates, however, would apply to a facility with a much higher staffing level than the subject proposal. In such circumstances, the most accurate methodology to assess the anticipated parking demand is to use a first principles assessment.

The proposed facility will be largely automated. The proposal will include two machines that will pre-size and sort the apples and three packing lines. The following staffing levels are anticipated to be required for each operation:

- two staff per machine for the pre-sizing and sorting operations which are less labour intensive; and
- four staff per packing lines.

Accordingly, there will be a demand for 16 staff when the facility is used to full capacity. If all staff were to drive to the site, there would be a requirement for 16 spaces.



In addition, there will be a requirement for the office and storage components of the proposal. In regard to the storage area, the Transport for New South Wales (formerly RMS) "Guide to Traffic Generating Development" specifies a rate of one space per 300 m² for warehouses. Based on this rate, the storage area will generate a requirement for three spaces.

It is expected that the proposal would, therefore generate a demand for 24 parking spaces. Such a demand would be readily accommodated for within the proposed parking provision.

4 TRAFFIC ASSESSMENT

The RMS guide specifies a traffic generation rate of 1 trip per 100 m² for a factory during the peak hour. Based on this rate, the proposal will generate approximately 40 trips during the peak hour periods.

In reality, the proposed development would not be expected to generate such a volume. Even if all staff were to arrive and/or depart during the peak hour, this would only equate to 24 trips. Deliveries to and from the site will only be approximately two to four trucks per day. Even if one arriving truck and one departing truck were to occur during a peak hour (which is not likely), there would still only be a forecast volume of 26 vehicles per hour (vph) during the peak period.

Notwithstanding the above, should a peak hour volume of 40 trips be realised, such a volume is still low and will have negligible impact on the adjacent road network.

5 SUMMARY

The proposed apple sorting facility will be designed to ensure that all vehicles would be able to safely enter and exit the site in a forward direction. Access and parking for the proposal will comply with AS/NZS 2890.1:2004 and AS 2890.2:2018.

The proposal has also considered the transport route to and from the site, given the existing substandard intersection of the service road with Onkaparinga Valley Road. Minor modifications to the service road will provide for safe turning movements of semi-trailers associated with the proposal. These modifications will also provide for improved safety for existing users of the service road.

Adequate parking spaces will be provided to cater for the anticipated peak parking demand associated with the development and the traffic generated by the proposal will have minimal impact and will not change the nature or function of the adjacent road network.

Yours sincerely,
MFY PTY LTD

MELISSA MELLEN
Director



2010 NATIONAL WINNER
2010 TELSTRA SOUTH AUSTRALIAN
BUSINESS WOMAN OF THE YEAR

In reply please quote: 2020/00272, Process ID: 654497
Enquiries to: Reece Loughron
Telephone: 08 7109 7876
E-mail: dit.landusecoordination@sa.gov.au



Government of South Australia

Department for Infrastructure
and Transport

**TRANSPORT PLANNING AND
PROGRAM DEVELOPMENT**

Transport Assessment

GPO Box 1533
ADELAIDE SA 5001

ABN 92 366 288 135

15 January 2021

Mr Deryn Atkinson
Adelaide Hills Council
PO Box 44
WOODSIDE SA 5244

Dear Mr Atkinson

SCHEDULE 8 - REFERRAL RESPONSE

Development No.	473/1273/20
Applicant	Joyson Orchards
Location	202-204 Onkaparinga Valley Road (cnr Naughtons Road), Woodside
Proposal	Fruit packing facility with ancillary offices, retaining walls and parking

I refer to the above development application forwarded to the Commissioner of Highways (CoH) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

CONSIDERATION

The subject site abuts Onkaparinga Valley Road and Naughtons Road. Onkaparinga Valley Road is an arterial road under the care, control and management of the CoH. Onkaparinga Valley Road is identified as a Direct Tourist Route and a Major Regional Cycling Route under the Department for Infrastructure and Transport's '*A Functional Hierarchy for South Australia's Land Transport Network*'. The abutting sections of Onkaparinga Valley Road is gazetted for 20 metre PBS Level 1A vehicles. At this location Onkaparinga Valley Road carries approximately 9,300 vehicles per day (9% commercial vehicles) and has a posted speed limit of 60km/h. Naughtons Road has a default speed limit of 50km/h.

Access and Road Safety

DIT had reviewed the APDS Planning Statement (Version 3.0 dated 25 November 2020) and associated MFY Traffic and Parking Report (refer MLM/20-0156 dated 12 November 2020) and the Beyond Ink plan set (Revision A dated 24 November 2020). The site has frontage to Onkaparinga Valley Road through the adjacent service road that also caters for the Amy Gillet Bikeway, as well as frontage to Naughtons Road (refer Beyond Ink, Site Plan, Sheet No. PD001, Revision A dated 24 November 2020).

Delivery Vehicles

The proposed development has been designed to accommodate entry movements of 19 metre semi-trailer via Naughtons Road (right in) and exit movements to the service road and then Onkaparinga Valley Road (left out only). The MFY report has undertaken a thorough review of the current design of the service road connection to Onkaparinga Valley Road and DIT

acknowledges that the existing arrangement does not meet current standards. Therefore, in order to cater for the delivery vehicles undertaking left turns the service road arrangement will be modified to provide for an improved entry angle to maximise driver sightlines. Refer MFY Figure 4 and 5. DIT supports the road works and all costs should be undertaken to Council/DIT requirements with all costs borne by the applicant.

Further, the MFY report indicates that the proposed development will not actually generate a demand for right turning movements for large commercial vehicles onto Onkaparinga Valley Road and that a traffic management plan will be developed by the applicant (operators) that requires all drivers of commercial vehicles to undertake left turns onto the service road and Onkaparinga Valley Road. DIT also supports these measures.

Overall, DIT supports the proposed access arrangements and Council should ensure the service road and Naughtons Road access are appropriately designed and flared to accommodate a 19.0 metre Articulated Vehicle (*AS2890.2:2018*).

In regards to general parking requirements, Council should ensure that the proposed car park design is designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009*. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.

ADVICE

The Department for Infrastructure and Transport supports the proposed development and advises the planning authority to attach the following conditions to any approval:

1. The access points to Onkaparinga Valley Road service road and Naughtons Road shall be constructed as shown on Beyond Ink, Site Plan, Sheet No. PD001, Revision A dated 24 November 2020.
2. The Onkaparinga Valley Road service road shall be modified to accommodate the swept path of a 19 metre Articulated Vehicle in order to permit a suitably angled approach to Onkaparinga Valley Road. The modifications shall be generally consistent with MFY Figure 5 with all costs borne by the applicant.
3. Large commercial vehicles accessing the site shall be restricted to right turn entry only movements to Naughtons Road and left turn exit only movements into the service road and Onkaparinga Valley Road. The movements and traffic flow through the site shall be reinforced with suitable signage and line marking as well as the traffic management plan initiated by the applicant/operator.
4. All vehicles shall enter and exit the site in a forward direction.
5. Any infrastructure within the road reserve that is demolished, altered, removed or damaged during the construction of the project shall be reinstated to the satisfaction of the relevant asset owner, with all costs being borne by the applicant.
6. The car park design shall be designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009*. Additionally, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
7. Commercial vehicle facilities shall be designed in accordance with *AS2890.2:2018*.

8. Any stormwater run-off shall be collected on-site and disposed of safely without jeopardising the safety of the adjacent roads. Any alterations to the existing road drainage infrastructure as a result of this development shall be at the expense of the applicant.

Yours sincerely



A/MANAGER, TRANSPORT ASSESSMENT
for **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to dit.developmentapplications@sa.gov.au

From: Gillam, Sharon (DEW) Sharon.Gillam@sa.gov.au
Subject: FW: Native Vegetation Enquiry : NVCCE00022 [SEC=UNOFFICIAL]
Date: 22 October 2020 at 12:54 pm
To: linda@einstein.com.au
Cc: markj@joyson.com.au

SG

Dear Linda,

Firstly, please accept my sincere apologies for the late reply on this enquiry.

The two small gums trees that have been recommended for removal are able to be removed under the Native Vegetation Council's *Guidelines for the Management of Roadside Native Vegetation and Regrowth Vegetation, under Native Vegetation Regulation 11(23) (2019)*, Part 3, Section 5. Clearance for access to adjoining land, where the clearance is required for normal vehicle access and is no more than 5m wide.

I am satisfied that the two small trees, as indicated in the photographs provided, meet the requirements for removal under these Guidelines.

If you have any further questions, please contact me.

Kind regards,
Sharon

Sharon Gillam
Assessment Officer

Native Vegetation Branch | Environment, Heritage & Sustainability Division (Mon-Thurs)
Department for Environment and Water
E sharon.gillam@sa.gov.au | P (08) 8463 6927 | M 0408 852 766
Level 4, 81-95 Waymouth Street, Adelaide SA 5000
GPO Box 1047, Adelaide, SA 5001

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Unofficial
Classified by Cerise.Curnow@sa.gov.au

From: DEW:NVC <DEWNVC@sa.gov.au>
Sent: Thursday, 22 October, 2020 11:51 AM
To: Gillam, Sharon (DEW) <Sharon.Gillam@sa.gov.au>
Subject: FW: Native Vegetation Enquiry : NVCCE00022 [SEC=UNOFFICIAL]

From: Linda MacAuley <linda@einstein.com.au>
Sent: Wednesday, 21 October, 2020 5:23 PM
To: DEW:NVC <DEWNVC@sa.gov.au>
Cc: Mark Joyce <markj@joyson.com.au>
Subject: Re: Native Vegetation Enquiry : NVCCE00022

Hello Native Veg

This e-mail is for Linda MacAuley. If you are not the intended recipient, please do not disseminate, distribute or copy this e-mail. If you have received this e-mail by mistake, please notify the sender immediately by e-mail. If you are not the intended recipient, you should not disseminate, distribute or copy this e-mail. Please notify the sender immediately by e-mail if you have received this e-mail by mistake and delete this e-mail from your system. If you are not the intended recipient you should not disseminate, distribute or copy this e-mail. If you are not the intended recipient you should not disseminate, distribute or copy this e-mail. Please notify the sender immediately by e-mail if you have received this e-mail by mistake and delete this e-mail from your system. If you are not the intended recipient you should not disseminate, distribute or copy this e-mail. Please notify the sender immediately by e-mail if you have received this e-mail by mistake and delete this e-mail from your system.

1 Naughton Road
Woodside
South Australia 5244

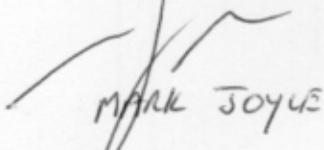
Dear Mark

AMENDED AGREEMENT

I confirm that we will undertake the following actions to address your concerns that you mentioned.

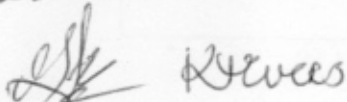
1. Electric forklifts only, removing forklift beeps and replace with low flashing lights and fitted with white noise muffler filters.
2. Trucks and truck refrigeration units be turned off during unloading and loading.
3. No spillage low-level lighting to be installed.
4. Installing a hedge along a section of your southern boundary, at no cost to you and,
5. Install a new 2.8m fence on your southern boundary to buffer truck noise at no cost to you.
6. Landscaping and tree screenings to the Naughton Road and waterwise planting to Onkaparinga Valley Road frontage to enhance the area at no cost to you.
7. Remove your existing Golden Elm tree including stump on your southern boundary due to the ongoing tree litter so close to your home at no cost to you.

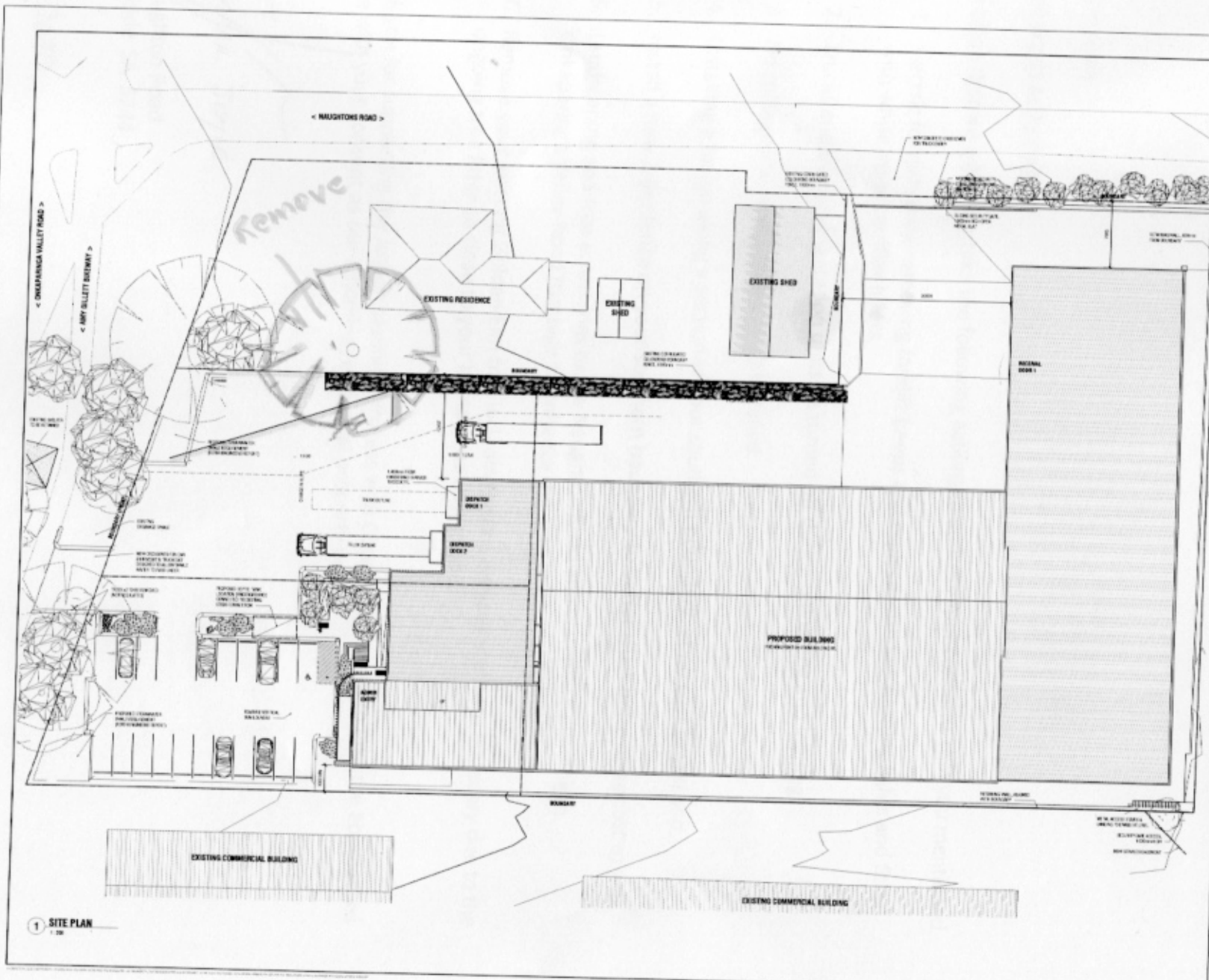
Thank you for supporting the Joyson planning process with Council. We agree to take the approaches above with your consent as our adjoining residential property owner.


MARK JOYCE

21 Naughton Road
Woodside SA 5244

23/11/2020


Mark Joyce



1 SITE PLAN
1:200

ABBREVIATIONS	
ABBRV	DESCRIPTION

BEYONDDINK
 DESIGN + ARCHITECTURE + BUILDING
 150-200 Champagne Valley Road, VIC 3088
 www.beyonddink.com.au

PROPOSED NEW FRUIT PROCESSING FACILITY & OFFICE
 150-200 Champagne Valley Road, VIC 3088 SA

PRELIMINARY
 DATE: 01/12/20
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 PROJECT NO: WD002